

Halton Hills #4 WWPS Municipal Class Environmental Assessment

Indigenous Communities, Agency, and Public Comments Log

Agency Name or Public	Contact Name	Email	Phone Number	Date Received	Summary of Comment	Action	Summary of Response to Stakeholder	Date Returned	Follow up Action	Follow Up Contact Point	Follow up action completed?
A. Comments Received for Notice of Commencement											
1 Trans-Northern Pipelines Inc.	Sandrine Exibard-Edgar	seedgar@tnpi.ca	289-475-5366	24-Jul-24	Trans-Northern Pipelines has no infrastructure in the area.	RVA updated the Stakeholders List.	Acknowledged receipt of their email.	19-Aug-24	N/A	N/A	N/A
2 Ministry of Natural Resources	Brianna Tombs	SB.Planning@ontario.ca	705-761-5633	25-Jul-24	Asked RVA to review the MNRF Areas of Interest package. If the project will not impact any MNRF interests there is no need to circulate any subsequent notices to the office.	RVA reviewed the MNRF Areas of Interest package and determined that the project will not impact any MNRF interests.	Acknowledged receipt of their email and updated the stakeholders list.	25-Jul-24	N/A	N/A	N/A
3 Conservation Halton	Ben Davis	bdevis@hnce.on.ca	905.336.1158 ext.2278	29-Jul-24	Asked for clarification regarding study area.	RVA replied and confirmed study area.	Study area to be East of Tenth Line, North of Steeles Ave.	01-Aug-24	N/A	N/A	N/A
4 Halton District School Board	Laureen Choi	choil@hdsb.ca	905-749-2184 ext.2201	25-Jul-24	The Halton District School Board has no comment on the study at this time. Please continue to circulate us on further updates of the study.	RVA kept the contact in the Stakeholders list.	Acknowledged receipt of their email.	19-Aug-24	N/A	N/A	N/A
5 Hydro One		Department.SecondaryLandUse@hydroone.com		31-Jul-24	Confirmed that there are no existing Hydro One Transmission assets in the subject area. Asked to be contacted if plans for undertaking change or the study area expands.	RVA updated the Stakeholders List.	N/A	31-Jul-24	N/A	N/A	N/A
6 Six Nations	Peter Graham	LRCs@sixnations.ca		25-Jul-24	Asked to be informed as the study progresses.	RVA updated the Stakeholders List and Comments Log.	Acknowledged receipt of their email and confirmed that they will be updated on the project's progress.	26-Jul-24	N/A	N/A	N/A
7 Atura Power	Stephen Smith	Stephen.Smith@aturapower.com	289-259-2377	07-Aug-24	Please add email to contact list.	Added to EA Contact list.	Stated that they had been added to the contact list.	08-Aug-24	N/A	N/A	N/A
8 Nation Huronne-Wendat	Benjamin Labbe	Benjamin.Labbe@wendake.ca	418 843-3767	14-Aug-24	Hurone-Wendat Nation is interested in getting involved in the review of all archaeology studies in the region and would like to have a copy of the updated Stage 1 report to review when the draft is completed.	RVA/the Region to share the updated Stage 1 report.	Acknowledge receipt of their email and will send them the updated Stage 1 report when available.	02-Oct-24	Halton sent the updated Stage 1 report on Oct 2, 2024.	N/A	N/A
9 Ontario Ministry of the Environment, Conservation and Parks	Chunmei Liu	mei.liu@ontario.ca	437-249-3102	22-Aug-24	Provided ministry acknowledge letter and relevant supporting information for the project.	RVA to review the letter and supporting information.	Acknowledge receipt of their email and will send questions if there are any.	04-Sep-24	N/A	N/A	N/A
10 Ministry of Citizenship and Multiculturalism	Dan Minkin	dan.minkin@ontario.ca	416-786-7553	26-Aug-24	Provided comments and asked RVA to keep consulting MCM throughout the EA process. MCM requested the following: -The licensed archaeologist should submit the report for the initiated Stage 1 AA (PIF# P051-0292-2023 and P051-0322-2024) to MCM prior to the issuance of the notice of completion. -Approval authorities and/or proponents should wait to receive the MCM's written confirmation that the archaeological assessment report(s) has been entered into the Register before issuing a decision or proceeding with any ground disturbing activities. -The Ministry's Criteria for Evaluating Potential for Built Heritage Resources and Cultural Heritage Landscapes should be completed to help determine whether this EA project may impact known or potential heritage resources and/or cultural heritage landscapes. -Advise MCM whether any technical cultural heritage studies will be completed, and provide them to MCM before issuing a Notice of Completion or commencing any work on the site. If screening has identified no known or potential cultural heritage resources, or no impacts to these resources, please include the completed checklists and supporting documentation in the EA report or file.	RVA to review the response letter.	Acknowledge receipt of their email and sent questions.	11-Sep-24	MCM responded to questions on Oct 16,2024	N/A	N/A
11 Haudenosaunee Development Institute	Emma Kameka	emma@hdi.to.com		20-Aug-24	Sent a standard monitoring agreement to be signed prior to commencement of the work, requested to be provided with all relevant archaeology reports for the project, and asked to be provided with the following information: •Project Name: •Project Address: •Project Contact Details: •Billing Contact: •Billing Contact Details: •Proponent Name: •Proponent Contact Details:	RVA to review and respond.	Acknowledge receipt of their email and stated that RVA understands that Halton Region is in active discussion with HDI regarding agreements related to archaeological work. Once an agreement has been reached, we can discuss scheduling a meeting. RVA also noted that while no field work is anticipated for this project, any persons attending a project site must have an executed Agreement with The Regional Municipality of Halton.	29-Oct-24	N/A	N/A	N/A
12 Credit Valley Conservation	Tyler Slaght	tyler.slaght@cvc.ca	905-670-1615 ext. 4060	03-Sep-24	Stated that the study area does not contain any features of interest to CVC and request that appropriate SWM controls are implemented and that existing drainage patterns are maintained.	Acknowledged receipt of their email.	Acknowledged receipt of their email.	28-Oct-24	N/A	N/A	N/A
13 Halton Regional Police Service	Keith Moore	keith.moore@haltonpolice.ca	905-825-4830	03-Sep-24	Asked RVA to send the Notice of Commencement to them.	RVA sent the Notice of Commencement.	Acknowledge receipt of their email and sent the Notice of Commencement.	03-Sep-24	N/A	N/A	N/A
Halton Regional Police Service	Keith Moore	keith.moore@haltonpolice.ca	905-825-4830	04-Sep-24	Noted that the Halton Regional Police do not need to receive the Class EA Notices for this project.	Acknowledged receipt of their email.	Acknowledged receipt of their email and updated stakeholders list.	04-Sep-24	N/A	N/A	N/A
				19-Sep-24	Expressed concerns regarding new direct access in close proximity to the existing interchange at Steeles Ave. and Winston Churchill Blvd. does not represent careful control of the location, type and design of driveways and intersections.	RVA to consult traffic team and respond.	The Region and RVA have and will continue to consider site access during the alternatives review, with site access being finalized as part of the detailed design of the project. All aspects of site access will be considered in greater detail through the detailed design process to provide safe travel for those using Steeles Avenue, Halton Region staff, and residents.	27-Sep-24	N/A	N/A	N/A
				02-Oct-24	Mentioned that in the Access Management Guideline it is stated that "spacing guideline of 250 to 400 meters between direct access points and potentially further in proximity to signalized intersections as supported by detailed studies". Requested that the Regional requirements be reviewed within the EA process.	RVA to consult transportation team and respond.	Acknowledge receipt of their email and stated the following information. -Traffic analysis is being completed as require by the Region's Access Management Guidelines, -The Region's Transportation Development Review group determined that, given the site specifics and that the proposed facility is remotely managed, it is not expected to be a significant traffic-generating site and that a full Traffic Impact Study is not required, -It is proposed that access to the wastewater pumping station will be restricted to right-in/right-out movements and is permitted through the Assess Management Guidelines, -A transportation letter will be submitted as part of the Schedule B MECA study. The analysis will be documented in the Project File for the MCEA study, and the requirements of the access will be finalized through the project's detailed design to support safe access for all users.	28-Oct-24	N/A	N/A	N/A
				10-Jan-25	Followed up regarding email sent on October 2, 2024 and stated that the property under study does not have independent access and the contractors had assumed that they had access from their property, without requesting authorization and have resulted in property damage.	Region to review and respond.	First response on January 16, 2025: Acknowledge receipt of their email and stated that the Region is currently reviewing the matters and have reminded the contractor that access to the site is not permitted until review is complete. Reminded that a response regarding access planning for the pumping station was provided end of October, 2024. Second response on January 22, 2025: Apologized on behalf of contractors and informed them that they are not authorized to enter the property moving forward. Confirmed that at this time no additional field work involving heavy equipment is anticipated but site visits might be required to conduct survey work. Access through the property or parking on the driveway will not be permitted during these visits. Stated that the project team would like to meet to review the damage to the landscaping as well as to develop an acceptable access plan for future site visits.	22-Jan-25	Region met with local residents, and impacted areas were restored to original condition	Region	Completed, June 2, 2025

Agency Name or Public	Contact Name	Email	Phone Number	Date Received	Summary of Comment	Action	Summary of Response to Stakeholder	Date Returned	Follow up Action	Follow Up Contact Point	Follow up action completed?
				31-Jan-25	Responded to the email sent on January 22, 2025 and provided contact to coordinate the site visit with Vivek Nemani.	Region to review and respond.	<p>Region responded on February 28, 2025:</p> <p>Regarding access requirements, Region confirms that guidance from Region Transportation Development Review team is reflective of both existing land uses, traffic regimes, and the approval of the Premier Gateway Node at the intersection of the two major arterial roads. Given that the proposed WWPS is remotely operated with infrequent access required, access from Steeles Avenue is considered appropriate in accordance with the Region's Access Guidelines. The proposed spacing from the intersection of right-in/right-out access is 250m which meets the required 115m minimum spacing identified and considers access needs for future development in the area which would be accessed through transportation impact studies required through the development application process.</p> <p>Regarding land use compatibility, and the visual identity of the area, Region have confirmed with the Town of Halton Hills that their bylaws allow public use for municipal infrastructure in all Zones of the municipality, whether explicitly stated or not. And the Town of Halton Hills continues to be consulted on the means to deliver required water and wastewater infrastructure for approved growth, while minimizing impacts on the existing community.</p> <p>Region stated that these measures will be documented through the completion of the Schedule B MCEA Study and look forward to continuing to work closely as the project progresses.</p>	28-Feb-25	N/A	N/A	N/A
				09-Jun-25	<p>A report was provided regarding 16863 Steeles Avenue, compiling historical planning documents, consultation records, and discussions from the May 15, 2025 meeting. Concerns raised include:</p> <ul style="list-style-type: none"> - The location of the proposed Wastewater Pumping Station (WWPS) versus other potential sites. - Traffic and safety concerns related to the new entrance to the WWPS from Steeles Avenue. - The possibility of separating the WWPS from the adjacent property via a local road. - Potential adverse impacts of the WWPS on the adjacent groundwater well system. 	Region to review and respond.	<p>The Region responded in a letter addressing the following:</p> <ul style="list-style-type: none"> - Site Selection Constraints: The WWPS location was limited to the north side of Steeles Avenue due to Highway 413 planning constraints. A gravity sewer option was considered but dismissed due to its inability to meet required in-service timelines. - Traffic and Access: The WWPS will be an unmanned facility with minimal traffic, limited to intermittent visits by Regional staff for maintenance. A bulk water station is not planned. While the approved Premier Gateway Phase 2B Secondary Plan does not include a local road in the area, the Region will continue coordinating with the Town to explore future access opportunities via a local right-of-way. - Facility Placement and Odour Control: The Region is prepared to site the facility on the west side of the property to maximize distance from 16863 Steeles Avenue. Landscaping screens will be considered, and an emissions dispersion model will be completed during detailed design to determine odour control requirements. - Groundwater Protection: The Region will conduct pre- and post-construction surveys of the well at 16863 Steeles Avenue and other nearby private wells. If construction impacts the water supply, a replacement will be provided, either through a new well or connection to the existing municipal watermain supplied under the Halton-Peel Cross-Boundary Water Servicing Agreement. 	24-Jul-25	N/A	N/A	N/A
15 Ministry of Citizenship and Multiculturalism	Dan Minkin	dan.minkin@ontario.ca	416-786-7553	16-Oct-24	Responded to the questions submitted and stated that RVA can carry out geotechnical and hydrogeological investigations without waiting for the results of the Stage 1 AA. Regarding built heritage checklist, if the Secondary Plan did not find known or potential built heritage resources and cultural heritage landscapes answer "yes" to question 2 of the checklist and not complete the rest.	RVA to review and respond.	Acknowledge receipt of their email.	22-Oct-24	N/A	N/A	N/A
B. Comments Received during PIC											
17 Halton District School Board	Laureen Choi	Kausi.Vick@hll.com choil@hdsb.ca	905-749-2184 ext.2201	23-Oct-24	<p>Asked about the anticipated timeline to complete the wastewater pumping station after the minimum 30 days of the public and agency review period (in respect to development timeline for the lands between Ninth Line and Winston Churchill, north of Steeles).</p> <p>Noted that the closest Halton District School Board existing facility is Pineview Public School located at 13074 5 Side Rd. Please continue to circulate us on further updates of the study.</p>	<p>RVA to consult the Region and respond.</p> <p>RVA kept the contact in the Stakeholders list.</p>	<p>Acknowledge receipt of their email and noted that the timeline to complete the construction of the wastewater pumping station is late 2027 or early 2028.</p> <p>Acknowledge receipt of their email.</p>	<p>06-Nov-24</p> <p>23-Oct-24</p>	N/A	N/A	N/A
18 Halton Hills Fire Department	Jason Blackmere	jblackmere@haltonhills.ca	416-523-8923	23-Oct-24	Stated that the Fire Chief would like more information regarding the study and if any fire department input is needed.	RVA to respond.	Acknowledge receipt of their email and shared available project information. Noted that once in the design phase the fire department might be involved during the building permit process through the Town of Halton Hills.	06-Nov-24	N/A	N/A	N/A
19 Gagnon Walker Domes Ltd.	Marc De Nardis	mdenardis@gwdplanners.com	905-796-5790 ext. 257	07-Nov-24	Please add email to contact list.	RVA updated the Stakeholders List.	Acknowledged receipt of their email and updated stakeholders list.	13-Nov-24	N/A	N/A	N/A
20 Nation Huronne-Wendat	Benjamin Labbe	Benjamin.Labbe@wendake.ca	418 843-3767	14-Nov-24	Stated that the archaeologist has completed the review, but they are still waiting for signature of the response letter.	N/A	N/A	N/A	N/A	N/A	N/A
21 Nation Huronne-Wendat	Benjamin Labbe	Benjamin.Labbe@wendake.ca	418 843-3767	18-Nov-24	Attached a letter with Huron-Wendat Nation's comments on the revised draft Stage 1 AA.	Region to review and respond.	Acknowledged receipt of their email and shared 2021 Stage 1 and 2 Archaeological Assessment for their review. And asked any additional comments or feedback to be sent back by December 2nd, 2024.	25-Nov-24	N/A	N/A	N/A
22 Métis Nation of Ontario	Laura Desaulniers	LauraD@metisnation.org	807-375-0208	19-Nov-24	Acknowledged receipt of their email and stated that there are no comments at this time.	Region to review and respond.	N/A	N/A	N/A	N/A	N/A
				29-Oct-24	Asked if the pump station can be moved to a better catchment area within their area and not on the outskirts. Asked to be added to the mailing list.	Region to review and respond.	<p>Updated stakeholders list.</p> <p>Region responded on April 25, 2025: Acknowledge receipt of their comment and provided information on study area and why it was chosen. Provided information on project file report and notice of study completion. Notified the resident that they have been added to the project mailing list.</p>	24-Apr-25	N/A	N/A	N/A
				31-Oct-24	<p>Asked to be added to the mailing list. And asked the following questions:</p> <ul style="list-style-type: none"> - How will the new building be powered? Is it green or sustainable? Will it help to get HH to their net 30 goals? If only natural gas will be used as a source why were other sources not considered? Will solar panel be added to the roof design? - What is the plan for addressing the monarch habitat area? Will you build a habitat area somewhere nearby? Work with local butterfly gardenway groups to do this? 	Region to review and respond.	<p>Updated stakeholders list.</p> <p>Region responded on May 12, 2025: Acknowledge receipt of their comment and provided information on topics including operation of the new wastewater pumping station, climate change mitigation, and monarch butterfly habitat. Provided information on project file report and notice of study completion. Notified the resident that they have been added to the project mailing list.</p>	12-May-25	N/A	N/A	N/A
				31-Oct-24	<p>Asked to be added to the mailing list. And asked the following questions:</p> <ul style="list-style-type: none"> - How is the treated water to be released into the environment? Exactly where will the treated water be released? - Has the Region contacted the nearby property owners to ensure they are all aware of this? 	Region to review and respond.	<p>Updated stakeholders list.</p> <p>Region responded on April 25, 2025: Acknowledge receipt of their comment and provided information on operation of the new wastewater pumping station and how the nearby property owners was informed about the project. Provided information on project file report and notice of study completion. Notified the resident that they have been added to the project mailing list.</p>	24-Apr-25	N/A	N/A	N/A

Agency Name or Public		Contact Name	Email	Phone Number	Date Received	Summary of Comment	Action	Summary of Response to Stakeholder	Date Returned	Follow up Action	Follow Up Contact Point	Follow up action completed?
26	Mississaugas of the Credit First Nation (MCFN)	Matthew LaForme	Matthew.LaForme@mcfn.ca	905-229-9483	14-Nov-24	Acknowledged that the Stage 1 AA, Halton Premier Gateway 2B, Potential Pump Station Alternative 2 and 3 has been received and reviewed. And noted that they agree with the recommendations made and there are no further comments or concerns at this time.	RVA to draft response.	Updated stakeholders list.		N/A	N/A	N/A
					13-Nov-24	Asked to be added to the mailing list, and asked if the project team had contacted residents in the area regarding the work.	Region to review and respond.	Updated stakeholders list. Region responded on April 25, 2025: Acknowledge receipt of their comment and provided information on how the nearby property owners was informed about the project. Provided information on project file report and notice of study completion. Notified the resident that they have been added to the project mailing list.	24-Apr-25	N/A	N/A	N/A
					17-Nov-24	Asked for the link to the PIC video and why is it not mentioned before the survey and asked to be added to the mailing list.	Region to review and respond.	Updated stakeholders list. Region responded on April 25, 2025: Acknowledge receipt of their comment and provided information on the location of the PIC video. Provided information on project file report and notice of study completion. Notified the resident that they have been added to the project mailing list.	24-Apr-25	N/A	N/A	N/A
					19-Nov-24	Asked if homes on Tenth Line will be connected to this system.	Region to review and respond.	Updated stakeholders list. Region responded on April 29, 2025: Acknowledge receipt of their comment and provided information on whether homes on Tenth Line will be connected to the new Halton Hills #4 Wastewater Pumping Station. Provided information on project file report and notice of study completion. Notified the resident that they have been added to the project mailing list.	29-Apr-25	N/A	N/A	N/A
30	Nation Huronne-Wendat	Benjamin Labbe	Benjamin.Labbe@wendake.ca	418 843-3767	13-Dec-24	Stated that the review of the 2021 Stage 1 and 2 Archaeological Assessment report are completed and are satisfied with the 2024 draft stage 1 report's conclusion. Asked for the Region to provide billing information to prepare the invoice.	Region acknowledged receipt of the email.			N/A	N/A	N/A
31	GWD Planners	Marc De Nardis	mdenardis@gwdplanners.com	905-796-5790 ext. 257	03-Feb-25	Requested an update regarding the project, and if the Notice of Completion had been issued yet.	Project team to respond to the question.	Project team responded to email, provided updated contact information for Region staff, and noted that the Notice of Completion would be issued sometime in Spring 2025.	11-Feb-25	N/A	N/A	N/A
C. Comments Received during draft Project File Report review												
32		Trevor Bell				Provided a letter from the MECP, Environmental Assessment Branch, regarding the draft Project File Report. Comments included: 1. Updating references to the 2024 Provincial Policy Statement. 2. Comments on Air and Noise modelling for consideration during the design. 3. Formatting revisions	Project team to provide response letter.	Project team acknowledged the letter, and provided responses to the comments. Project team updated the PFR to suit.	26-Jun-25	N/A		
33	Ministry of Citizenship and Multiculturalism	Dan Minkin	dan.minkin@ontario.ca	416-786-7553	13-Jun-25	Provided a letter from the MCM, regarding the draft Project File Report. Comments included: 1. Updating terminology related to cultural heritage resources	Project team to provide response letter.	Project team acknowledged the letter, and provided responses to the comments. Project team updated the PFR to suit.	26-Jun-25	N/A	N/A	N/A

From: Carol Derrick
Sent: Wednesday, July 24, 2024 6:55 PM
To: Freya Wu
Cc: Dania Chehab
Subject: FW: R236814-20240724-Halton Hills #4 WWPS - Premier Gateway Employment Area Class EA Notice of Commencement

FYI – see below.

From: Land ROW East <landroweast@tnpi.ca>
Sent: Wednesday, July 24, 2024 5:17 PM
To: Carol Derrick <cderrick@rvanderson.com>
Cc: info <info@tnpi.ca>; Sandrine Exibard-Edgar <seedgar@tnpi.ca>
Subject: RE: R236814-20240724-Halton Hills #4 WWPS - Premier Gateway Employment Area Class EA Notice of Commencement

[CAUTION EXTERNAL EMAIL] Make Sure that it is legitimate **before** Replying or Clicking on any links

Thank you for your notice, but Trans-Northern Pipelines has no infrastructure in the area

Sandrine Exibard-Edgar

Team Lead, Property Administration/ Chef d'Equipe, Administration Foncière
Trans-Northern Pipelines Inc. | 45 Vogell Road, Suite 310, Richmond Hill, ON L4B 3P6 |
(289) 475-5366
seedgar@tnpi.ca



From: Carol Derrick <cderrick@rvanderson.com>
Sent: Wednesday, July 24, 2024 8:20 AM
Cc: Freya Wu <mwu@rvanderson.com>
Subject: R236814-20240724-Halton Hills #4 WWPS - Premier Gateway Employment Area Class EA Notice of Commencement

You don't often get email from cderrick@rvanderson.com. [Learn why this is important](#)

CAUTION: This email originated from outside of the TNPI organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Dear Sir/Madam:

Please see attached the Notice of Commencement for the Halton Hills #4 Wastewater Pumping Station – Premier Gateway Employment Area Municipal Class Environmental Assessment (Class EA).

The Regional Municipality of Halton is undertaking this Class EA to identify a preferred site for the Halton Hills #4 Wastewater Pumping Station based on current industry standards and due to growing development flows in the Premier Gateway Employment Area.

This notice is sent to your attention as it was deemed that you may be an interested stakeholder.

If you would like more information about the Halton Hills #4 Wastewater Pumping Station – Premier Gateway Employment Area or would like to be removed from the study's contact list, please contact the undersigned.

Yours very truly,

Dania Chehab
Consultant Project Manager
R.V. Anderson Associates Limited
2001 Sheppard Avenue East, Suite 300
Toronto, ON M2J 4Z8

Phone: 416-497-8600 ext. 1456
Email: dchehab@rvanderson.com

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From: Dania Chehab
Sent: Monday, July 29, 2024 9:44 AM
To: Alonzo Moya, Carlos
Cc: Jonathan Rudyk; Freya Wu
Subject: FW: R236814-20240724-Halton Hills #4 WWPS - Premier Gateway Employment Area Class EA Notice of Commencement
Attachments: 25_07_2024_MNRresponse.docx

FYI

From: Southern Region Planning Inbox (MNR) <SR.Planning@ontario.ca>
Sent: Thursday, July 25, 2024 4:27 PM
To: Dania Chehab <dchehab@rvanderson.com>
Cc: Tombs, Brianna (MNR) <Brianna.Tombs@ontario.ca>
Subject: RE: R236814-20240724-Halton Hills #4 WWPS - Premier Gateway Employment Area Class EA Notice of Commencement

[CAUTION EXTERNAL EMAIL] Make Sure that it is legitimate **before** Replying or Clicking on any links

Dear Dania Chehab,

The Ministry of Natural Resources and Forestry (MNRF) received the Notice of Commencement for Halton Hills #4 Wastewater Pumping Station, Premier Gateway- Steeles Avenue between Ninth Line and Winston Churchill Boulevard (R236814-20240724) on July 24, 2024. Thank you for circulating this to our office. Please note the ministry has not completed a screening of natural heritage, natural hazards, other natural resource values, or applicable ministry permits or approvals at this time.

This response provides information to guide you in identifying ministry interests and engaging the ministry for advice as needed. MNRF has prepared the attached MNRF Areas of Interest package to provide proponents an overview of MNRF mandated interests and the ministry's commenting role in respect of external requests for comment for projects subject to the *Environmental Assessment Act*.

Please note it is the proponent's responsibility to be aware of, and comply with, all relevant federal or provincial legislation, municipal by-laws and/or other agency approvals.

After reviewing the information provided, if you have identified that your project would not impact any MNRF interests there is no need to circulate any subsequent notices to our office. If you have identified MNRF interests and/or may require permit(s) or further technical advice, please direct your specific questions to the undersigned.

If you have any questions or concerns, please feel free to contact me.

Best Regards,

Brianna

Brianna Tombs

Planning Intern | Land Use Planning and Strategic Issues Section
Southern Region | Regional Operations Division
Ministry of Natural Resources | Ontario Public Service
705-761-5633 | Brianna.tombs@ontario.ca



Taking pride in strengthening Ontario, its places and its people

From: Scientific Collection Permits Aurora (MNR) <scp.aurora@ontario.ca>
Sent: Wednesday, July 24, 2024 10:29 AM
To: Southern Region Planning Inbox (MNR) <SR.Planning@ontario.ca>
Subject: FW: R236814-20240724-Halton Hills #4 WWPS - Premier Gateway Employment Area Class EA Notice of Commencement

From: Carol Derrick <cderrick@rvanderson.com>
Sent: Wednesday, July 24, 2024 10:18 AM
Cc: Freya Wu <mwu@rvanderson.com>
Subject: R236814-20240724-Halton Hills #4 WWPS - Premier Gateway Employment Area Class EA Notice of Commencement

CAUTION -- EXTERNAL E-MAIL - Do not click links or open attachments unless you recognize the sender.

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If you would like more information about the Halton Hills #4 Wastewater Pumping Station – Premier Gateway Employment Area or would like to be removed from the study's contact list, please contact the undersigned.

Yours very truly,

Dania Chehab
Consultant Project Manager
R.V. Anderson Associates Limited
2001 Sheppard Avenue East, Suite 300
Toronto, ON M2J 4Z8

Phone: 416-497-8600 ext. 1456
Email: dchehab@rvanderson.com

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Ministry of Natural Resources

Land Use Planning and Strategic Issues
Section
Southern Region

Regional Operations Division
300 Water Street
Peterborough, ON K9J 3C7
Tél.: 705 761-4839

Ministère des Richesses Naturelles

Section de l'aménagement du territoire et des
questions stratégiques
Région du Sud

Division des opérations régionales
300, rue Water
Peterborough (ON) K9J 3C7
Tél. : 705 761-4839



July 25, 2024

Dania Chehab
Project Manager, R.V. Anderson Associates Limited
2001 Sheppard Avenue East, Suite 300
Toronto, ON M2J 4Z8

**SUBJECT: Halton Hills #4 Wastewater Pumping Station, Premier Gateway-
Steeles Avenue between Ninth Line and Winston Churchill Boulevard (R236814-
20240724)**

The Ministry of Natural Resources (MNR) received the Notice of Study Commencement by email on July 24, 2024. Thank you for circulating this to our office. Please note that we have not completed a screening of natural heritage or other resource values for the project at this time. This response, however, does provide information to guide you in identifying and assessing natural features and resources as required by applicable policies and legislation, as well as engaging with the Ministry for advice as needed.

Please also note that it is the proponent's responsibility to be aware of, and comply with, all relevant federal or provincial legislation, municipal by-laws or other agency approvals.

Natural Heritage

MNR's natural heritage and natural resources GIS data layers can be obtained through the Ministry's [Land Information Ontario \(LIO\)](#) website. You may also view natural heritage information online (e.g., Provincially Significant Wetlands, ANSI's, woodlands, etc.) using the [Make a Map: Natural Heritage Areas](#) tool.

We recommend that you use the above-noted sources of information during the review of your project proposal.

Natural Hazards

A series of natural hazard technical guides developed by MNR are available to support municipalities and conservation authorities implement the natural hazard policies in the Provincial Policy Statement (PPS). For example, standards to address flood risks and the potential impacts and costs from riverine flooding are addressed in the *Technical Guide River and Stream Systems: Flooding Hazard Limit (2002)*. We recommend that you consider these technical guides as you assess specific improvement projects that can be undertaken to reduce the risk of flooding.

Petroleum Wells & Oil, Gas and Salt Resources Act

There may be petroleum wells within the proposed project area. Please consult the Ontario Oil, Gas and Salt Resources Library website (www.ogsrlibrary.com) for the best-known data on any wells recorded by MNR. Please reference the 'Definitions and Terminology Guide' listed in the publications on the library website to better understand the well information available. Any oil and gas wells in your project area are regulated by the *Oil, Gas and Salt Resource Act*, and the supporting regulations and operating standards. If any unanticipated wells are encountered during development of the project, or if the proponent has questions regarding petroleum operations, the proponent should contact the Petroleum Operations Section at POSRecords@ontario.ca or 519-873-4634.

Fish and Wildlife Conservation Act

Please note, that should the project require:

- The relocation of fish outside of the work area, a Licence to Collect Fish for Scientific Purposes under the *Fish and Wildlife Conservation Act* will be required.
- The relocation of wildlife outside of the work area (including amphibians, reptiles, and small mammals), a Wildlife Collector's Authorization under the *Fish and Wildlife Conservation Act* will be required.

Public Lands Act & Lakes and Rivers Improvement Act

Some Project may be subject to the provisions of the *Public Lands Act* or *Lakes and River Improvement Act*. Please review the information on MNR's web pages provided below regarding when an approval is, or is not, required. Please note, *Lakes and Rivers Improvement Act* approval from the Ministry is not required for certain activities within the area of jurisdiction of a Conservation Authority. Please see the *Lakes and Rivers Improvement Act* administrative guide for more information and contact your local Conservation Authority where unsure if work is subject to regulation under the *Conservation Authorities Act*.

- For more information about the *Public Lands Act*: <https://www.ontario.ca/page/crown-land-work-permits>
- For more information about the *Lakes and Rivers Improvement Act*: <https://www.ontario.ca/page/lakes-and-rivers-improvement-act-administrative-guide>

After reviewing the information provided, if you have not identified any of MNR's interests stated above, there is no need to circulate any subsequent notices to our office. If you have identified any of MNR's interests and/or may require permit(s) or further technical advice, please direct your specific questions to the undersigned.

If you have any questions or concerns, please feel free to contact me.

Best Regards,

Brianna Tombs
Brianna.tombs@ontario.ca (705) 761-5633

From: Carol Derrick
Sent: Monday, July 29, 2024 12:40 PM
To: Freya Wu
Cc: Dania Chehab
Subject: FW: [EXTERNAL]R236814-20240724-Halton Hills #4 WWPS - Premier Gateway Employment Area Class EA Notice of Commencement

Categories: Filed by Newforma

See below email.

Carol

From: Ben Davis <bdavis@hrca.on.ca>
Sent: Monday, July 29, 2024 12:33 PM
To: Carol Derrick <cderrick@rvanderson.com>
Subject: RE: [EXTERNAL]R236814-20240724-Halton Hills #4 WWPS - Premier Gateway Employment Area Class EA Notice of Commencement

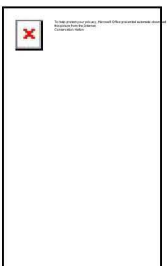
[CAUTION EXTERNAL EMAIL] Make Sure that it is legitimate **before** Replying or Clicking on any links

Hi Carol,

Thank you for providing the Notice of Study Commencement for this Wastewater Pumping Station (Halton Hills #4). The Notice outlines that the study area is between Ninth Line and Winston Churchill Blvd however the graphic only shows the study area to be East of Tenth Line, North of Steeles Ave. Would you be able to confirm the location?

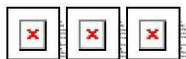
Additionally, the study area also appears to be within Credit Valley Conservation jurisdiction as well as Conservation Halton. Please be sure to send the Notice of Study Commencement to Credit Valley Conservation also.

Thank you,
Ben



Ben Davis, CAN-CISEC
Regional Infrastructure Team Lead

2596 Britannia Road West, Burlington, ON L7P 0G3
905.336.1158 ext.2278 | bdavis@hrca.on.ca
conservationhalton.ca





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From: Carol Derrick <cderrick@rvanderson.com>

Sent: Wednesday, July 24, 2024 10:20 AM

Cc: Freya Wu <mwu@rvanderson.com>

Subject: [EXTERNAL]R236814-20240724-Halton Hills #4 WWPS - Premier Gateway Employment Area Class EA Notice of Commencement

You don't often get email from cderrick@rvanderson.com. [Learn why this is important](#)

Dear Sir/Madam:

Please see attached the Notice of Commencement for the Halton Hills #4 Wastewater Pumping Station – Premier Gateway Employment Area Municipal Class Environmental Assessment (Class EA).

The Regional Municipality of Halton is undertaking this Class EA to identify a preferred site for the Halton Hills #4 Wastewater Pumping Station based on current industry standards and due to growing development flows in the Premier Gateway Employment Area.

This notice is sent to your attention as it was deemed that you may be an interested stakeholder.

If you would like more information about the Halton Hills #4 Wastewater Pumping Station – Premier Gateway Employment Area or would like to be removed from the study's contact list, please contact the undersigned.

Yours very truly,

Dania Chehab
Consultant Project Manager
R.V. Anderson Associates Limited
2001 Sheppard Avenue East, Suite 300
Toronto, ON M2J 4Z8

Phone: 416-497-8600 ext. 1456

Email: dchehab@rvanderson.com

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From: Carol Derrick
Sent: Monday, July 29, 2024 12:40 PM
To: Freya Wu
Cc: Dania Chehab
Subject: FW: R236814-20240724-Halton Hills #4 WWPS - Premier Gateway Employment Area Class EA Notice of Commencement

Categories: Filed by Newforma

FYI, see below.

From: Laureen Choi [Staff] <choil@hdsb.ca>
Sent: Thursday, July 25, 2024 4:37 PM
To: Carol Derrick <cderrick@rvanderson.com>
Cc: carlos.alonzomoya@halton.ca; Thibeault, Frederick <thibeaultf@hdsb.ca>
Subject: Re: R236814-20240724-Halton Hills #4 WWPS - Premier Gateway Employment Area Class EA Notice of Commencement

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Hi Carol. Thank you for the circulation.

The HDSB has no comment on the study at this time. Please continue to circulate us on future updates of the study.

Laureen Choi | Specialist - Planning
Halton District School Board
J.W. Singleton Education Centre
2050 Guelph Line, Burlington, ON L7R 3Z2
choil@hdsb.ca | ext. 2201
905-749-2184

On Wed, Jul 24, 2024 at 10:19 AM Carol Derrick <cderrick@rvanderson.com> wrote:

Dear Sir/Madam:

Please see attached the Notice of Commencement for the Halton Hills #4 Wastewater Pumping Station – Premier Gateway Employment Area Municipal Class Environmental Assessment (Class EA).

The Regional Municipality of Halton is undertaking this Class EA to identify a preferred site for the Halton Hills #4 Wastewater Pumping Station based on current industry standards and due to growing development flows in the Premier Gateway Employment Area.

This notice is sent to your attention as it was deemed that you may be an interested stakeholder.

If you would like more information about the Halton Hills #4 Wastewater Pumping Station – Premier Gateway Employment Area or would like to be removed from the study's contact list, please contact the undersigned.

Yours very truly,

Dania Chehab

Consultant Project Manager

R.V. Anderson Associates Limited

2001 Sheppard Avenue East, Suite 300

Toronto, ON M2J 4Z8

Phone: 416-497-8600 ext. 1456

Email: dchehab@rvanderson.com

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From: Alonzo Moya, Carlos <Carlos.AlonzoMoya@halton.ca>
Sent: Wednesday, July 31, 2024 3:03 PM
To: Dania Chehab
Cc: Freya Wu
Subject: FW: Hydro One Response: 20240731-NoticeOfCommence-Halton Hills 4 Wastewater Pumping Station
Attachments: 20240731-NoticeOfCommence-Halton Hills 4 Wastewater Pumping Station.pdf

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Hi Dania,

I received this email from Hydro One. Could you please update the contact information for Hydro One?

Thanks!
Carlos

Carlos Alonzo Moya, M.A.Sc., P.Eng.
Project Manager II
Infrastructure Planning & Policy
Halton Region
905-825-6000, ext. 7426 | 1-866-442-5866

-----Original Message-----

From: SUN Hongxia <Susan.SUN@HydroOne.com> On Behalf Of SECONDARY LAND USE Department
Sent: Wednesday, July 31, 2024 11:37 AM
To: Alonzo Moya, Carlos <Carlos.AlonzoMoya@halton.ca>
Cc: SECONDARY LAND USE Department <Department.SecondaryLandUse@hydroone.com>
Subject: Hydro One Response: 20240731-NoticeOfCommence-Halton Hills 4 Wastewater Pumping Station

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Please see the attached for Hydro One's Response.

Hydro One Networks Inc

SecondaryLandUse@HydroOne.com



Hydro One Networks Inc.

483 Bay Street
8th Floor South Tower
Toronto, Ontario M5G 2P5

HydroOne.com

July 31, 2024

Re: Halton Hills 4 Wastewater Pumping Station

Attention:
Carlos Alonzo Moya
Project Manager
Halton Region

Thank you for sending us notification regarding Halton Hills 4 Wastewater Pumping Station. In our preliminary assessment, we confirm there are no existing Hydro One Transmission assets in the subject area. Please be advised that this is only a preliminary assessment based on current information.

If plans for the undertaking change or the study area expands beyond that shown, please contact Hydro One to assess impacts of existing or future planned electricity infrastructure.

Any future communications are sent to Secondarylanduse@hydroone.com.

Be advised that any changes to lot grading and/or drainage within proximity to Hydro One transmission corridor lands must be controlled and directed away from the transmission corridor.

Sent on behalf of,

**Secondary Land Use
Asset Optimization
Strategy & Integrated Planning
Hydro One Networks Inc.**

From: Dania Chehab
Sent: Thursday, August 8, 2024 6:26 PM
To: Stephen Smith; carlos.alonzomoya@halton.ca
Cc: Freya Wu; Jonathan Rudyk
Subject: RE: Halton Hills #4 Wastewater Pumping Station NoC

Categories: Filed by Newforma

Hello Stephen,
Thank you for your email and interest in this project.
We acknowledge your request and have added you to the mailing list.

All the best,
Dania

Dania Chehab, P.Eng., M.Eng. (she/her)

Project Manager, Water & Wastewater



R.V. Anderson Associates Limited
2001 Sheppard Avenue East, Suite 300, Toronto ON M2J 4Z8
t 416 497 8600 ext. 1456

[LinkedIn](#) | [Facebook](#) | [Website](#)



From: Stephen Smith <Stephen.Smith@aturapower.com>
Sent: Friday, August 2, 2024 8:43 AM
To: carlos.alonzomoya@halton.ca
Cc: Dania Chehab <dchehab@rvanderson.com>
Subject: Halton Hills #4 Wastewater Pumping Station NoC

[CAUTION EXTERNAL EMAIL] Make Sure that it is legitimate **before** Replying or Clicking on any links

Good Morning Carlos,

I am Stephen Smith the Environmental Specialist for the Halton Hills Generating Station down the road from your EA project at Sixth & Steeles. I would like to be included in your project mailing list.

Thank you,

Stephen Smith
Environmental Specialist | **Atura Power**
289-259-2377 | Stephen.smith@aturapower.com

From: Alonzo Moya, Carlos <Carlos.AlonzoMoya@halton.ca>
Sent: Wednesday, August 7, 2024 11:44 AM
To: Freya Wu
Cc: Dania Chehab; Jonathan Rudyk
Subject: FW: Halton Hills #4 Wastewater Pumping Station NoC

Categories: Filed by Newforma

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Hi Freya,

Can you please confirm if your team is replying to these emails? We just need to acknowledge the email and add them to the stakeholder list.

Best,
Carlos

Carlos Alonzo Moya (he/him/his), M.A.Sc., P.Eng.

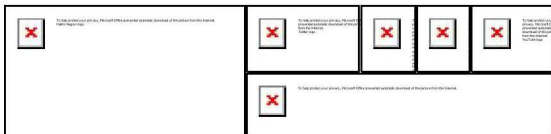
Project Manager II

Infrastructure Planning & Policy

Public Works

Halton Region

905-825-6000, ext. 7426 | 1-866-442-5866



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From: Stephen Smith <Stephen.Smith@aturapower.com>
Sent: Friday, August 2, 2024 8:43 AM
To: Alonzo Moya, Carlos <Carlos.AlonzoMoya@halton.ca>
Cc: dchehab@rvanderson.com
Subject: Halton Hills #4 Wastewater Pumping Station NoC

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Good Morning Carlos,

I am Stephen Smith the Environmental Specialist for the Halton Hills Generating Station down the road from your EA project at Sixth & Steeles. I would like to be included in your project mailing list.

Thank you,

Stephen Smith

Environmental Specialist | **Atura Power**

289-259-2377 | Stephen.smith@aturapower.com

From: EA Notices to CRegion (MECP) <eanotification.cregion@ontario.ca>
Sent: Thursday, August 22, 2024 5:03 PM
To: Freya Wu; carlos.alonzomoya@halton.ca; Jonathan Rudyk; Dania Chehab
Cc: EA Notices to CRegion (MECP); Mazzuca, Marco (MECP); Dufresne, Tina (MECP); Hoy, Debbie (MECP)
Subject: RE: Halton Region, Schedule B Municipal Class EA, Halton Hills #4 Wastewater Pumping Station - Premier Gateway Employment Area
Attachments: MECP Ack of NOC-HH#4WPS-PremierGatewayEmploymentAreaHR.pdf; Supporting Attachment - Proponent's Intro to Delegation of Procedural Aspects of Consultation with Aboriginal Communities.pdf; Supporting Attachment - Species at Risk Proponents Guide to Preliminary Screening (May 2019).pdf; Municipal Class EA Feb 2024.pdf

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Dear Project Team, hope you're all doing well 🌑 🌕 .. 😊

Thank you for following up with us about this project. Attached please find the ministry acknowledge letter and relevant supporting information for this project. Please ensure that the most recent versions of ministry documents are used and referenced. A copy of the February 2024 MCEA is attached for reference.

If you have any questions regarding the information shared, please feel free to contact me for further discussion.

Warm regards,

Chunmei Liu (she/her) | Regional Environmental Planner

Environmental Assessments Branch, Ontario Ministry of the Environment, Conservation and Parks | 7th Flr, 135 St Clair Ave W, Toronto, ON M4V 1P5 | Chunmei.Liu@ontario.ca | 437-249-3102

From: Freya Wu <mwu@rvanderson.com>
Sent: Tuesday, August 20, 2024 11:18 AM
To: EA Notices to CRegion (MECP) <eanotification.cregion@ontario.ca>
Cc: carlos.alonzomoya@halton.ca; Jonathan Rudyk <JRudyk@rvanderson.com>
Subject: RE: Halton Region, Schedule B Municipal Class EA, Halton Hills #4 Wastewater Pumping Station - Premier Gateway Employment Area

CAUTION -- EXTERNAL E-MAIL - Do not click links or open attachments unless you recognize the sender.

Good morning,

Just following up regarding the copy of project notice and completed project information form we submitted on July 24th, 2024.

Please let us know if you have any comments or questions.

Thank you,

Freya (Menghong) Wu, M.A.Sc. (she/her)

Process Designer



R.V. Anderson Associates Limited
2001 Sheppard Avenue East, Suite 300, Toronto ON M2J 4Z8
t 416 497 8600 ext. 1377

[LinkedIn](#) | [Facebook](#) | [Website](#)



From: Freya Wu

Sent: Wednesday, July 24, 2024 11:02 AM

To: eanotification.cregion@ontario.ca

Cc: carlos.alonzomoya@halton.ca; Dania Chehab <dchehab@rvanderson.com>; Jonathan Rudyk <JRudyk@rvanderson.com>

Subject: Halton Region, Schedule B Municipal Class EA, Halton Hills #4 Wastewater Pumping Station - Premier Gateway Employment Area

Good morning,

Please find a copy of project notice and completed project information form attached.

Best Regards,

Freya (Menghong) Wu, M.A.Sc. (she/her)

Process Designer



R.V. Anderson Associates Limited
2001 Sheppard Avenue East, Suite 300, Toronto ON M2J 4Z8
t 416 497 8600 ext. 1377

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Ministry of the Environment,
Conservation and Parks

Ministère de l'Environnement, de la
Protection de la nature et des Parcs



Environmental Assessment
Branch

Direction des évaluations
environnementales

7th Floor
135 St. Clair Avenue W
Toronto ON M4V 1P5
Tel.: 416 314-8001
Fax.: 416 314-8452

7ème étage
135, avenue St. Clair Ouest
Toronto ON M4V 1P5
Tél. : 416 314-8001
Téléc. : 416 314-8452

August 22, 2024

Carlos Alonzo Moya
Project Manager II
Halton Region
carlos.alonzomoya@halton.ca
***BY EMAIL ONLY

Re: **Halton Hills #4 Wastewater Pumping Station - Premier Gateway Employment Area
Regional Municipality of Halton
Municipal Class Environmental Assessment, Schedule B
Acknowledgement of Notice of Commencement**

Dear Carlos Moya,

This letter is in response to the Notice of Commencement for the above noted project. The Ministry of the Environment, Conservation and Parks (MECP) acknowledges that the Regional Municipality of Halton (proponent) has indicated that the study is following the approved environmental planning process for a Schedule B project under the Municipal Class Environmental Assessment (Class EA).

The **updated (August 2022)** attached "Areas of Interest" document provides guidance regarding the ministry's interests with respect to the Class EA process. Please address all areas of interest in the EA documentation at an appropriate level for the EA study. Proponents who address all the applicable areas of interest can minimize potential delays to the project schedule. **Further information is provided at the end of the Areas of Interest document relating to recent changes to the Environmental Assessment Act through Bill 197, Covid-19 Economic Recovery Act 2020.**

The Crown has a legal duty to consult Aboriginal communities when it has knowledge, real or constructive, of the existence or potential existence of an Aboriginal or treaty right and contemplates conduct that may adversely impact that right. Before authorizing this project, the Crown must ensure that its duty to consult has been fulfilled, where such a duty is triggered.

Although the duty to consult with Aboriginal peoples is a duty of the Crown, the Crown may delegate procedural aspects of this duty to project proponents while retaining oversight of the consultation process.

The proposed project may have the potential to affect Aboriginal or treaty rights protected under Section 35 of Canada's *Constitution Act* 1982. Where the Crown's duty to consult is triggered in relation to the proposed project, **the MECP is delegating the procedural aspects of rights-based consultation to the proponent through this letter.** The Crown intends to rely on the delegated consultation process in discharging its duty to consult and maintains the right to participate in the consultation process as it sees fit.

The project area is located in Treaty 13a, 1805w/Mississaugas, within the traditional territory and claim of the Mississaugas of the Credit and, within the 1701 Nanfan Deed which allows for the harvesting activities of the Six Nations. Based on information provided to date and the Crown's preliminary assessment the proponent is required to consult with the following communities who have been identified as potentially affected by the proposed project:

- **Mississaugas of the Credit First Nation**
- **Six Nations of the Grand River (elected council and traditional council-HCCC)**

Steps that the proponent may need to take in relation to Aboriginal consultation for the proposed project are outlined in the ["Code of Practice for Consultation in Ontario's Environmental Assessment Process"](#). Additional information related to Ontario's Environmental Assessment Act is available online at: www.ontario.ca/environmentalassessments.

Please also refer to the attached document "A Proponent's Introduction to the Delegation of Procedural Aspects of consultation with Aboriginal Communities" for further information, including the MECP's expectations for EA report documentation related to consultation with communities.

The proponent must contact the Director of Environmental Assessment Branch (EABDirector@ontario.ca) under the following circumstances after initial discussions with the communities identified by the MECP:

- Aboriginal or treaty rights impacts are identified to you by the communities;
- You have reason to believe that your proposed project may adversely affect an Aboriginal or treaty right;
- Consultation with Indigenous communities or other stakeholders has reached an impasse; or
- A Section 16 Order request is expected based on impacts to Aboriginal or treaty rights

The MECP will then assess the extent of any Crown duty to consult for the circumstances and will consider whether additional steps should be taken, including what role you will be asked to play should additional steps and activities be required.

Please ensure a copy of the final notice and the Project File report is sent to the ministry's Central Region EA notification email account (eanotification.cregion@ontario.ca) for filing.

Should you or any members of your project team have any questions regarding the material above, please contact me at chunmei.liu@ontario.ca.

Sincerely,



Chunmei Liu
Regional Environmental Planner –Central Region
Project Review Unit, Environmental Assessment Branch

Cc: Marco Mazzuca, Supervisor, Project Review Unit, MECP
Tina Dufresne, Manager, Halton-Peel District Office, MECP
Debbie Hoy, Water Compliance Officer, Halton-Peel District Office, MECP
Diana Chehab, Project Manager, R.V Anderson Associates Limited,
dchehab@rvanderson.com

Enclosed: Areas of Interest

Attached: Client's Guide to Preliminary Screening for Species at Risk
A Proponent's Introduction to the Delegation of Procedural Aspects of Consultation
with Aboriginal Communities

AREAS OF INTEREST (v. August 2022)

It is suggested that you check off each section after you have considered / addressed it.

Planning and Policy

- Applicable plans and policies should be identified in the report, and the proponent should describe how the proposed project adheres to the relevant policies in these plans.
 - Projects located in MECP Central, Eastern or West Central Region may be subject to [A Place to Grow: Growth Plan for the Greater Golden Horseshoe \(2020\)](#).
 - Projects located in MECP Central or Eastern Region may be subject to the [Oak Ridges Moraine Conservation Plan \(2017\)](#) or the [Lake Simcoe Protection Plan \(2014\)](#).
 - Projects located in MECP Central, Southwest or West Central Region may be subject to the [Niagara Escarpment Plan \(2017\)](#).
 - Projects located in MECP Central, Eastern, Southwest or West Central Region may be subject to the [Greenbelt Plan \(2017\)](#).
 - Projects located in MECP Northern Region may be subject to the [Growth Plan for Northern Ontario \(2011\)](#).
- The [Provincial Policy Statement \(2020\)](#) contains policies that protect Ontario's natural heritage and water resources. Applicable policies should be referenced in the report, and the proponent should describe how the proposed project is consistent with these policies.
- In addition to the provincial planning and policy level, the report should also discuss the planning context at the municipal and federal levels, as appropriate.

Source Water Protection

The *Clean Water Act*, 2006 (CWA) aims to protect existing and future sources of drinking water. To achieve this, several types of vulnerable areas have been delineated around surface water intakes and wellheads for every municipal residential drinking water system that is located in a source protection area. These vulnerable areas are known as a Wellhead Protection Areas (WHPAs) and surface water Intake Protection Zones (IPZs). Other vulnerable areas that have been delineated under the CWA include Highly Vulnerable Aquifers (HVAs), Significant Groundwater Recharge Areas (SGRAs), Event-based modelling areas (EBAs), and Issues Contributing Areas (ICAs). Source protection plans have been developed that include policies to address existing and future risks to sources of municipal drinking water within these vulnerable areas.

Projects that are subject to the Environmental Assessment Act that fall under a Class EA, or one of the Regulations, have the potential to impact sources of drinking water if they occur in designated vulnerable areas or in the vicinity of other at-risk drinking water systems (i.e. systems that are not municipal residential systems). MEA Class EA projects may include

activities that, if located in a vulnerable area, could be a threat to sources of drinking water (i.e. have the potential to adversely affect the quality or quantity of drinking water sources) and the activity could therefore be subject to policies in a source protection plan. Where an activity poses a risk to drinking water, policies in the local source protection plan may impact how or where that activity is undertaken. Policies may prohibit certain activities, or they may require risk management measures for these activities. Municipal Official Plans, planning decisions, Class EA projects (where the project includes an activity that is a threat to drinking water) and prescribed instruments must conform with policies that address significant risks to drinking water and must have regard for policies that address moderate or low risks.

- In October 2015, the MEA Parent Class EA document was amended to include reference to the Clean Water Act (Section A.2.10.6) and indicates that proponents undertaking a Municipal Class EA project must identify early in their process whether a project is or could potentially be occurring with a vulnerable area. **Given this requirement, please include a section in the report on source water protection.**
 - The proponent should identify the source protection area and should clearly document how the proximity of the project to sources of drinking water (municipal or other) and any delineated vulnerable areas was considered and assessed. Specifically, the report should discuss whether or not the project is located in a vulnerable area and provide applicable details about the area.
 - If located in a vulnerable area, proponents should document whether any project activities are prescribed drinking water threats and thus pose a risk to drinking water (this should be consulted on with the appropriate Source Protection Authority). Where an activity poses a risk to drinking water, the proponent must document and discuss in the report how the project adheres to or has regard to applicable policies in the local source protection plan. This section should then be used to inform and be reflected in other sections of the report, such as the identification of net positive/negative effects of alternatives, mitigation measures, evaluation of alternatives etc.
- While most source protection plans focused on including policies for significant drinking water threats in the WHPAs and IPZs it should be noted that even though source protection plan policies may not apply in HVAs, these are areas where aquifers are sensitive and at risk to impacts and within these areas, activities may impact the quality of sources of drinking water for systems other than municipal residential systems.
- In order to determine if this project is occurring within a vulnerable area, proponents can use [Source Protection Information Atlas](#), which is an online mapping tool available to the public. Note that various layers (including WHPAs, WHPA-Q1 and WHPA-Q2, IPZs, HVAs, SGRAs, EBAs, ICAs) can be turned on through the “Map Legend” bar on the left. The mapping tool will also provide a link to the appropriate source protection plan in order to identify what policies may be applicable in the vulnerable area.

- For further information on the maps or source protection plan policies which may relate to their project, proponents must contact the appropriate source protection authority. **Please consult with the local source protection authority to discuss potential impacts on drinking water. Please document the results of that consultation within the report and include all communication documents/correspondence.**

More Information

For more information on the *Clean Water Act*, source protection areas and plans, including specific information on the vulnerable areas and drinking water threats, please refer to [Conservation Ontario's website](#) where you will also find links to the local source protection plan/assessment report.

A list of the prescribed drinking water threats can be found in [section 1.1 of Ontario Regulation 287/07](#) made under the *Clean Water Act*. In addition to prescribed drinking water threats, some source protection plans may include policies to address additional "local" threat activities, as approved by the MECP.

Climate Change

The document "[Considering Climate Change in the Environmental Assessment Process](#)" (Guide) is now a part of the Environmental Assessment program's Guides and Codes of Practice. The Guide sets out the MECP's expectation for considering climate change in the preparation, execution and documentation of environmental assessment studies and processes. The guide provides examples, approaches, resources, and references to assist proponents with consideration of climate change in EA. Proponents should review this Guide in detail.

- **The MECP expects proponents of Class EA projects to:**
 1. Consider during the assessment of alternative solutions and alternative designs, the following:
 - a. the project's expected production of greenhouse gas emissions and impacts on carbon sinks (climate change mitigation); and
 - b. resilience or vulnerability of the undertaking to changing climatic conditions (climate change adaptation).
 2. Include a discrete section in the report detailing how climate change was considered in the EA.

How climate change is considered can be qualitative or quantitative in nature and should be scaled to the project's level of environmental effect. In all instances, both a project's impacts on climate change (mitigation) and impacts of climate change on a project (adaptation) should be considered.

- The MECP has also prepared another guide to support provincial land use planning direction related to the completion of energy and emission plans. The "[Community Emissions Reduction Planning: A Guide for Municipalities](#)" document is designed to educate stakeholders on the municipal opportunities to reduce energy and greenhouse gas emissions, and to provide guidance on methods and techniques to incorporate consideration of energy and greenhouse gas emissions into municipal activities of all types. We encourage you to review the Guide for information.

Air Quality, Dust and Noise

- If there are sensitive receptors in the surrounding area of this project, a quantitative air quality/odour impact assessment will be useful to evaluate alternatives, determine impacts and identify appropriate mitigation measures. The scope of the assessment can be determined based on the potential effects of the proposed alternatives, and typically includes source and receptor characterization and a quantification of local air quality impacts on the sensitive receptors and the environment in the study area. The assessment will compare to all applicable standards or guidelines for all contaminants of concern. **Please contact this office for further consultation on the level of Air Quality Impact Assessment required for this project if not already advised.**
- If a quantitative Air Quality Impact Assessment is not required for the project, the MECP expects that the report contain a qualitative assessment which includes:
 - A discussion of local air quality including existing activities/sources that significantly impact local air quality and how the project may impact existing conditions;
 - A discussion of the nearby sensitive receptors and the project's potential air quality impacts on present and future sensitive receptors;
 - A discussion of local air quality impacts that could arise from this project during both construction and operation; and
 - A discussion of potential mitigation measures.
- As a common practice, "air quality" should be used as an evaluation criterion for all road projects.
- Dust and noise control measures should be addressed and included in the construction plans to ensure that nearby residential and other sensitive land uses within the study area are not adversely affected during construction activities.
- The MECP recommends that non-chloride dust-suppressants be applied. For a comprehensive list of fugitive dust prevention and control measures that could be applied, refer to [Cheminfo Services Inc. Best Practices for the Reduction of Air Emissions from Construction and Demolition Activities](#) report prepared for Environment Canada. March 2005.

- The report should consider the potential impacts of increased noise levels during the operation of the completed project. The proponent should explore all potential measures to mitigate significant noise impacts during the assessment of alternatives.

Ecosystem Protection and Restoration

- Any impacts to ecosystem form and function must be avoided where possible. The report should describe any proposed mitigation measures and how project planning will protect and enhance the local ecosystem.
- Natural heritage and hydrologic features should be identified and described in detail to assess potential impacts and to develop appropriate mitigation measures. The following sensitive environmental features may be located within or adjacent to the study area:
 - Key Natural Heritage Features: Habitat of endangered species and threatened species, fish habitat, wetlands, areas of natural and scientific interest (ANSIs), significant valleylands, significant woodlands; significant wildlife habitat (including habitat of special concern species); sand barrens, savannahs, and tallgrass prairies; and alvars.
 - Key Hydrologic Features: Permanent streams, intermittent streams, inland lakes and their littoral zones, seepage areas and springs, and wetlands.
 - Other natural heritage features and areas such as: vegetation communities, rare species of flora or fauna, Environmentally Sensitive Areas, Environmentally Sensitive Policy Areas, federal and provincial parks and conservation reserves, Greenland systems etc.

We recommend consulting with the Ministry of Natural Resources and Forestry (MNRF), Fisheries and Oceans Canada (DFO) and your local conservation authority to determine if special measures or additional studies will be necessary to preserve and protect these sensitive features. In addition, for projects located in Central Region you may consider the provisions of the Rouge Park Management Plan if applicable.

Species at Risk

- The Ministry of the Environment, Conservation and Parks has now assumed responsibility of Ontario's Species at Risk program. Information, standards, guidelines, reference materials and technical resources to assist you are found at <https://www.ontario.ca/page/species-risk>.
- The Client's Guide to Preliminary Screening for Species at Risk (Draft May 2019) has been attached to the covering email for your reference and use. Please review this document for next steps.

- For any questions related to subsequent permit requirements, please contact SAROntario@ontario.ca.

Surface Water

- The report must include enough information to demonstrate that there will be no negative impacts on the natural features or ecological functions of any watercourses within the study area. Measures should be included in the planning and design process to ensure that any impacts to watercourses from construction or operational activities (e.g. spills, erosion, pollution) are mitigated as part of the proposed undertaking.
- Additional stormwater runoff from new pavement can impact receiving watercourses and flood conditions. Quality and quantity control measures to treat stormwater runoff should be considered for all new impervious areas and, where possible, existing surfaces. The ministry's [Stormwater Management Planning and Design Manual \(2003\)](#) should be referenced in the report and utilized when designing stormwater control methods. **A Stormwater Management Plan should be prepared as part of the Class EA process** that includes:
 - Strategies to address potential water quantity and erosion impacts related to stormwater draining into streams or other sensitive environmental features, and to ensure that adequate (enhanced) water quality is maintained
 - Watershed information, drainage conditions, and other relevant background information
 - Future drainage conditions, stormwater management options, information on erosion and sediment control during construction, and other details of the proposed works
 - Information on maintenance and monitoring commitments.
- Ontario Regulation 60/08 under the *Ontario Water Resources Act* (OWRA) applies to the Lake Simcoe Basin, which encompasses Lake Simcoe and the lands from which surface water drains into Lake Simcoe. If a proposed sewage treatment plant is listed in Table 1 of the regulation, the report should describe how the proposed project and its mitigation measures are consistent with the requirements of this regulation and the OWRA.
- Any potential approval requirements for surface water taking or discharge should be identified in the report. A Permit to Take Water (PTTW) under the OWRA will be required for any water takings that exceed 50,000 L/day, except for certain water taking activities that have been prescribed by the Water Taking EASR Regulation – *O. Reg. 63/16*. These prescribed water-taking activities require registration in the EASR instead of a PTTW. Please review the [Water Taking User Guide for EASR](#) for more information. Additionally, an

Environmental Compliance Approval under the OWRA is required for municipal stormwater management works.

Groundwater

- The status of, and potential impacts to any well water supplies should be addressed. If the project involves groundwater takings or changes to drainage patterns, the quantity and quality of groundwater may be affected due to drawdown effects or the redirection of existing contamination flows. In addition, project activities may infringe on existing wells such that they must be reconstructed or sealed and abandoned. Appropriate information to define existing groundwater conditions should be included in the report.
- If the potential construction or decommissioning of water wells is identified as an issue, the report should refer to Ontario Regulation 903, Wells, under the OWRA.
- Potential impacts to groundwater-dependent natural features should be addressed. Any changes to groundwater flow or quality from groundwater taking may interfere with the ecological processes of streams, wetlands or other surficial features. In addition, discharging contaminated or high volumes of groundwater to these features may have direct impacts on their function. Any potential effects should be identified, and appropriate mitigation measures should be recommended. The level of detail required will be dependent on the significance of the potential impacts.
- Any potential approval requirements for groundwater taking or discharge should be identified in the report. A Permit to Take Water (PTTW) under the OWRA will be required for any water takings that exceed 50,000 L/day, with the exception of certain water taking activities that have been prescribed by the Water Taking EASR Regulation – *O. Reg. 63/16*. These prescribed water-taking activities require registration in the EASR instead of a PTTW. Please review the [Water Taking User Guide for EASR](#) for more information.
- Consultation with the railroad authorities is necessary wherever there is a plan to use construction dewatering in the vicinity of railroad lines or where the zone of influence of the construction dewatering potentially intercepts railroad lines.

Excess Materials Management

- In December 2019, MECP released a new regulation under the Environmental Protection Act, titled “On-Site and Excess Soil Management” (O. Reg. 406/19) to support improved management of excess construction soil. This regulation is a key step to support proper management of excess soils, ensuring valuable resources don’t go to waste and to provide clear rules on managing and reusing excess soil. New risk-based standards referenced by

this regulation help to facilitate local beneficial reuse which in turn will reduce greenhouse gas emissions from soil transportation, while ensuring strong protection of human health and the environment. The new regulation is being phased in over time, with the first phase in effect on January 1, 2021. For more information, please visit <https://www.ontario.ca/page/handling-excess-soil>.

- The report should reference that activities involving the management of excess soil should be completed in accordance with O. Reg. 406/19 and the MECP's current guidance document titled "[Management of Excess Soil – A Guide for Best Management Practices](#)" (2014).
- All waste generated during construction must be disposed of in accordance with ministry requirements.

Contaminated Sites

- Any current or historical waste disposal sites should be identified in the report. The status of these sites should be determined to confirm whether approval pursuant to Section 46 of the EPA may be required for land uses on former disposal sites. We recommend referring to the [MECP's D-4 guideline](#) for land use considerations near landfills and dumps.
 - Resources available may include regional/local municipal official plans and data; provincial data on [large landfill sites](#) and [small landfill sites](#); Environmental Compliance Approval information for waste disposal sites on [Access Environment](#).
- Other known contaminated sites (local, provincial, federal) in the study area should also be identified in the report (Note – information on federal contaminated sites is found on the Government of Canada's [website](#)).
- The location of any underground storage tanks should be investigated in the report. Measures should be identified to ensure the integrity of these tanks and to ensure an appropriate response in the event of a spill. The ministry's Spills Action Centre must be contacted in such an event.
- Since the removal or movement of soils may be required, appropriate tests to determine contaminant levels from previous land uses or dumping should be undertaken. If the soils are contaminated, you must determine how and where they are to be disposed of, consistent with *Part XV.1 of the Environmental Protection Act* (EPA) and Ontario Regulation 153/04, Records of Site Condition, which details the new requirements related to site assessment and clean up. Please contact the appropriate MECP District Office for further consultation if contaminated sites are present.

Servicing, Utilities and Facilities

- The report should identify any above or underground utilities in the study area such as transmission lines, telephone/internet, oil/gas etc. The owners should be consulted to discuss impacts to this infrastructure, including potential spills.
- The report should identify any servicing infrastructure in the study area such as wastewater, water, stormwater that may potentially be impacted by the project.
- Any facility that releases emissions to the atmosphere, discharges contaminants to ground or surface water, provides potable water supplies, or stores, transports or disposes of waste must have an Environmental Compliance Approval (ECA) before it can operate lawfully. Please consult with MECP's Environmental Permissions Branch to determine whether a new or amended ECA will be required for any proposed infrastructure.
- We recommend referring to the ministry's [environmental land use planning guides](#) to ensure that any potential land use conflicts are considered when planning for any infrastructure or facilities related to wastewater, pipelines, landfills or industrial uses.

Mitigation and Monitoring

- Contractors must be made aware of all environmental considerations so that all environmental standards and commitments for both construction and operation are met. Mitigation measures should be clearly referenced in the report and regularly monitored during the construction stage of the project. In addition, we encourage proponents to conduct post-construction monitoring to ensure all mitigation measures have been effective and are functioning properly.
- Design and construction reports and plans should be based on a best management approach that centres on the prevention of impacts, protection of the existing environment, and opportunities for rehabilitation and enhancement of any impacted areas.
- The proponent's construction and post-construction monitoring plans must be documented in the report, as outlined in Section A.2.5 and A.4.1 of the MEA Class EA parent document.

Consultation

- The report must demonstrate how the consultation provisions of the Class EA have been fulfilled, including documentation of all stakeholder consultation efforts undertaken during the planning process. This includes a discussion in the report that identifies concerns that were raised and **describes how they have been addressed by the proponent** throughout

the planning process. The report should also include copies of comments submitted on the project by interested stakeholders, and the proponent's responses to these comments (as directed by the Class EA to include full documentation).

- Please include the full stakeholder distribution/consultation list in the documentation.

Class EA Process

- If this project is a Master Plan: there are several different approaches that can be used to conduct a Master Plan, examples of which are outlined in Appendix 4 of the Class EA. **The Master Plan should clearly indicate the selected approach for conducting the plan**, by identifying whether the levels of assessment, consultation and documentation are sufficient to fulfill the requirements for Schedule B or C projects. Please note that any Schedule B or C projects identified in the plan would be subject to Part II Order Requests under the Environmental Assessment Act, although the plan itself would not be. **Please include a description of the approach being undertaken (use Appendix 4 as a reference).**
- If this project is a Master Plan: Any identified projects should also include information on the MCEA schedule associated with the project.
- The report should provide clear and complete documentation of the planning process in order to allow for transparency in decision-making.
- The Class EA requires the consideration of the effects of each alternative on all aspects of the environment (including planning, natural, social, cultural, economic, technical). The report should include a level of detail (e.g. hydrogeological investigations, terrestrial and aquatic assessments, cultural heritage assessments) such that all potential impacts can be identified, and appropriate mitigation measures can be developed. Any supporting studies conducted during the Class EA process should be referenced and included as part of the report.
- Please include in the report a list of all subsequent permits or approvals that may be required for the implementation of the preferred alternative, including but not limited to, MECP's PTTW, EASR Registrations and ECAs, conservation authority permits, species at risk permits, MTO permits and approvals under the *Impact Assessment Act*, 2019.
- Ministry guidelines and other information related to the issues above are available at <http://www.ontario.ca/environment-and-energy/environment-and-energy>. We encourage you to review all the available guides and to reference any relevant information in the report.

Amendments to the EAA through the Covid-19 Economic Recovery Act, 2020

Once the EA Report is finalized, the proponent must issue a Notice of Completion providing a minimum 30-day period during which documentation may be reviewed and comment and input can be submitted to the proponent. The Notice of Completion must be sent to the appropriate MECP Regional Office email address.

The public can request a higher level of assessment on a project if they are concerned about potential adverse impacts to constitutionally protected Aboriginal and treaty rights. In addition, the Minister may issue an order on his or her own initiative within a specified time period. The Director (of the Environmental Assessment Branch) will issue a Notice of Proposed Order to the proponent if the Minister is considering an order for the project within 30 days after the conclusion of the comment period on the Notice of Completion. At this time, the Director may request additional information from the proponent. Once the requested information has been received, the Minister will have 30 days within which to make a decision or impose conditions on your project.

Therefore, the proponent cannot proceed with the project until at least 30 days after the end of the comment period provided for in the Notice of Completion. Further, the proponent may not proceed after this time if:

- a Section 16 Order request has been submitted to the ministry regarding potential adverse impacts to constitutionally protected Aboriginal and treaty rights, or
- the Director has issued a Notice of Proposed order regarding the project.

Please ensure that the Notice of Completion advises that outstanding concerns are to be directed to the proponent for a response, and that in the event there are outstanding concerns regarding potential adverse impacts to constitutionally protected Aboriginal and treaty rights, Section 16 Order requests on those matters should be addressed in writing to:

Minister of the Environment, Conservation and Parks
Ministry of the Environment, Conservation and Parks
777 Bay Street, 5th Floor
Toronto ON M7A 2J3
minister.mecp@ontario.ca

and

Director, Environmental Assessment Branch
Ministry of the Environment, Conservation and Parks
135 St. Clair Ave. W, 1st Floor
Toronto ON, M4V 1P5
EABDirector@ontario.ca

MUNICIPAL CLASS ENVIRONMENTAL ASSESSMENT

February 2024

ENTIRE MCEA DOCUMENT NOT PROVIDED IN APPENDIX FOR CLARITY

A PROPONENT'S INTRODUCTION TO THE DELEGATION OF PROCEDURAL ASPECTS OF CONSULTATION WITH ABORIGINAL COMMUNITIES

DEFINITIONS

The following definitions are specific to this document and may not apply in other contexts:

Aboriginal communities – the First Nation or Métis communities identified by the Crown for the purpose of consultation.

Consultation – the Crown's legal obligation to consult when the Crown has knowledge of an established or asserted Aboriginal or treaty right and contemplates conduct that might adversely impact that right. This is the type of consultation required pursuant to s. 35 of the *Constitution Act, 1982*. Note that this definition does not include consultation with Aboriginal communities for other reasons, such as regulatory requirements.

Crown – the Ontario Crown, acting through a particular ministry or ministries.

Procedural aspects of consultation – those portions of consultation related to the process of consultation, such as notifying an Aboriginal community about a project, providing information about the potential impacts of a project, responding to concerns raised by an Aboriginal community and proposing changes to the project to avoid negative impacts.

Proponent – the person or entity that wants to undertake a project and requires an Ontario Crown decision or approval for the project.

I. PURPOSE

The Crown has a legal duty to consult Aboriginal communities when it has knowledge of an existing or asserted Aboriginal or treaty right and contemplates conduct that may adversely impact that right. In outlining a framework for the duty to consult, the Supreme Court of Canada has stated that the Crown may delegate procedural aspects of consultation to third parties. This document provides general information about the Ontario Crown's approach to delegation of the procedural aspects of consultation to proponents.

This document is not intended to instruct a proponent about an individual project, and it does not constitute legal advice.

II. WHY IS IT NECESSARY TO CONSULT WITH ABORIGINAL COMMUNITIES?

The objective of the modern law of Aboriginal and treaty rights is the *reconciliation* of Aboriginal peoples and non-Aboriginal peoples and their respective rights, claims and interests. Consultation is an important component of the reconciliation process.

The Crown has a legal duty to consult Aboriginal communities when it has knowledge of an existing or asserted Aboriginal or treaty right and contemplates conduct that might adversely impact that right. For example, the Crown's duty to consult is triggered when it considers

issuing a permit, authorization or approval for a project which has the potential to adversely impact an Aboriginal right, such as the right to hunt, fish, or trap in a particular area.

The scope of consultation required in particular circumstances ranges across a spectrum depending on both the nature of the asserted or established right and the seriousness of the potential adverse impacts on that right.

Depending on the particular circumstances, the Crown may also need to take steps to accommodate the potentially impacted Aboriginal or treaty right. For example, the Crown may be required to avoid or minimize the potential adverse impacts of the project.

III. THE CROWN'S ROLE AND RESPONSIBILITIES IN THE DELEGATED CONSULTATION PROCESS

The Crown has the responsibility for ensuring that the duty to consult, and accommodate where appropriate, is met. However, the Crown may delegate the procedural aspects of consultation to a proponent.

There are different ways in which the Crown may delegate the procedural aspects of consultation to a proponent, including through a letter, a memorandum of understanding, legislation, regulation, policy and codes of practice.

If the Crown decides to delegate procedural aspects of consultation, the Crown will generally:

- Ensure that the delegation of procedural aspects of consultation and the responsibilities of the proponent are clearly communicated to the proponent;
- Identify which Aboriginal communities must be consulted;
- Provide contact information for the Aboriginal communities;
- Revise, as necessary, the list of Aboriginal communities to be consulted as new information becomes available and is assessed by the Crown;
- Assess the scope of consultation owed to the Aboriginal communities;
- Maintain appropriate oversight of the actions taken by the proponent in fulfilling the procedural aspects of consultation;
- Assess the adequacy of consultation that is undertaken and any accommodation that may be required;
- Provide a contact within any responsible ministry in case issues arise that require direction from the Crown; and
- Participate in the consultation process as necessary and as determined by the Crown.

IV. THE PROPONENT'S ROLE AND RESPONSIBILITIES IN THE DELEGATED CONSULTATION PROCESS

Where aspects of the consultation process have been delegated to a proponent, the Crown, in meeting its duty to consult, will rely on the proponent's consultation activities and documentation of those activities. The consultation process informs the Crown's decision of whether or not to approve a proposed project or activity.

A proponent's role and responsibilities will vary depending on a variety of factors including the extent of consultation required in the circumstance and the procedural aspects of consultation the Crown has delegated to it. Proponents are often in a better position than the Crown to discuss a project and its potential impacts with Aboriginal communities and to determine ways to avoid or minimize the adverse impacts of a project.

A proponent can raise issues or questions with the Crown at any time during the consultation process. If issues or concerns arise during the consultation that cannot be addressed by the proponent, the proponent should contact the Crown.

a) What might a proponent be required to do in carrying out the procedural aspects of consultation?

Where the Crown delegates procedural aspects of consultation, it is often the proponent's responsibility to provide notice of the proposed project to the identified Aboriginal communities. The notice should indicate that the Crown has delegated the procedural aspects of consultation to the proponent and should include the following information:

- a description of the proposed project or activity;
- mapping;
- proposed timelines;
- details regarding anticipated environmental and other impacts;
- details regarding opportunities to comment; and
- any changes to the proposed project that have been made for seasonal conditions or other factors, where relevant.

Proponents should provide enough information and time to allow Aboriginal communities to provide meaningful feedback regarding the potential impacts of the project. Depending on the nature of consultation required for a project, a proponent also may be required to:

- provide the Crown with copies of any consultation plans prepared and an opportunity to review and comment;
- ensure that any necessary follow-up discussions with Aboriginal communities take place in a timely manner, including to confirm receipt of information, share and update information and to address questions or concerns that may arise;

- as appropriate, discuss with Aboriginal communities potential mitigation measures and/or changes to the project in response to concerns raised by Aboriginal communities;
- use language that is accessible and not overly technical, and translate material into Aboriginal languages where requested or appropriate;
- bear the reasonable costs associated with the consultation process such as, but not limited to, meeting hall rental, meal costs, document translation(s), or to address technical & capacity issues;
- provide the Crown with all the details about potential impacts on established or asserted Aboriginal or treaty rights, how these concerns have been considered and addressed by the proponent and the Aboriginal communities and any steps taken to mitigate the potential impacts;
- provide the Crown with complete and accurate documentation from these meetings and communications; and
- notify the Crown immediately if an Aboriginal community not identified by the Crown approaches the proponent seeking consultation opportunities.

b) What documentation and reporting does the Crown need from the proponent?

Proponents should keep records of all communications with the Aboriginal communities involved in the consultation process and any information provided to these Aboriginal communities.

As the Crown is required to assess the adequacy of consultation, it needs documentation to satisfy itself that the proponent has fulfilled the procedural aspects of consultation delegated to it. The documentation required would typically include:

- the date of meetings, the agendas, any materials distributed, those in attendance and copies of any minutes prepared;
- the description of the proposed project that was shared at the meeting;
- any and all concerns or other feedback provided by the communities;
- any information that was shared by a community in relation to its asserted or established Aboriginal or treaty rights and any potential adverse impacts of the proposed activity, approval or disposition on such rights;
- any proposed project changes or mitigation measures that were discussed, and feedback from Aboriginal communities about the proposed changes and measures;
- any commitments made by the proponent in response to any concerns raised, and feedback from Aboriginal communities on those commitments;
- copies of correspondence to or from Aboriginal communities, and any materials distributed electronically or by mail;

- information regarding any financial assistance provided by the proponent to enable participation by Aboriginal communities in the consultation;
- periodic consultation progress reports or copies of meeting notes if requested by the Crown;
- a summary of how the delegated aspects of consultation were carried out and the results; and
- a summary of issues raised by the Aboriginal communities, how the issues were addressed and any outstanding issues.

In certain circumstances, the Crown may share and discuss the proponent's consultation record with an Aboriginal community to ensure that it is an accurate reflection of the consultation process.

c) Will the Crown require a proponent to provide information about its commercial arrangements with Aboriginal communities?

The Crown may require a proponent to share information about aspects of commercial arrangements between the proponent and Aboriginal communities where the arrangements:

- include elements that are directed at mitigating or otherwise addressing impacts of the project;
- include securing an Aboriginal community's support for the project; or
- may potentially affect the obligations of the Crown to the Aboriginal communities.

The proponent should make every reasonable effort to exempt the Crown from confidentiality provisions in commercial arrangements with Aboriginal communities to the extent necessary to allow this information to be shared with the Crown.

The Crown cannot guarantee that information shared with the Crown will remain confidential. Confidential commercial information should not be provided to the Crown as part of the consultation record if it is not relevant to the duty to consult or otherwise required to be submitted to the Crown as part of the regulatory process.

V. WHAT ARE THE ROLES AND RESPONSIBILITIES OF ABORIGINAL COMMUNITIES' IN THE CONSULTATION PROCESS?

Like the Crown, Aboriginal communities are expected to engage in consultation in good faith. This includes:

- responding to the consultation notice;
- engaging in the proposed consultation process;
- providing relevant documentation;

- clearly articulating the potential impacts of the proposed project on Aboriginal or treaty rights; and
- discussing ways to mitigate any adverse impacts.

Some Aboriginal communities have developed tools, such as consultation protocols, policies or processes that provide guidance on how they would prefer to be consulted. Although not legally binding, proponents are encouraged to respect these community processes where it is reasonable to do so. Please note that there is no obligation for a proponent to pay a fee to an Aboriginal community in order to enter into a consultation process.

To ensure that the Crown is aware of existing community consultation protocols, proponents should contact the relevant Crown ministry when presented with a consultation protocol by an Aboriginal community or anyone purporting to be a representative of an Aboriginal community.

VI. WHAT IF MORE THAN ONE PROVINCIAL CROWN MINISTRY IS INVOLVED IN APPROVING A PROPONENT'S PROJECT?

Depending on the project and the required permits or approvals, one or more ministries may delegate procedural aspects of the Crown's duty to consult to the proponent. The proponent may contact individual ministries for guidance related to the delegation of procedural aspects of consultation for ministry-specific permits/approvals required for the project in question. Proponents are encouraged to seek input from all involved Crown ministries sooner rather than later.

Client's Guide to Preliminary Screening for Species at Risk

***Ministry of the Environment, Conservation and Parks
Species at Risk Branch, Permissions and Compliance
DRAFT - May 2019***

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1.0 Purpose, Scope, Background and Context

1.1 Purpose of this Guide

This guide has been created to:

- help clients better understand their obligation to gather information and complete a preliminary screening for species at risk before contacting the ministry,
- outline guidance and advice clients can expect to receive from the ministry at the preliminary screening stage,
- help clients understand how they can gather information about species at risk by accessing publicly available information housed by the Government of Ontario, and
- provide a list of other potential sources of species at risk information that exist outside the Government of Ontario.

It remains the client's responsibility to:

- carry out a preliminary screening for their projects,
- obtain best available information from all applicable information sources,
- conduct any necessary field studies or inventories to identify and confirm the presence or absence of species at risk or their habitat,
- consider any potential impacts to species at risk that a proposed activity might cause, and
- comply with the *Endangered Species Act* (ESA).

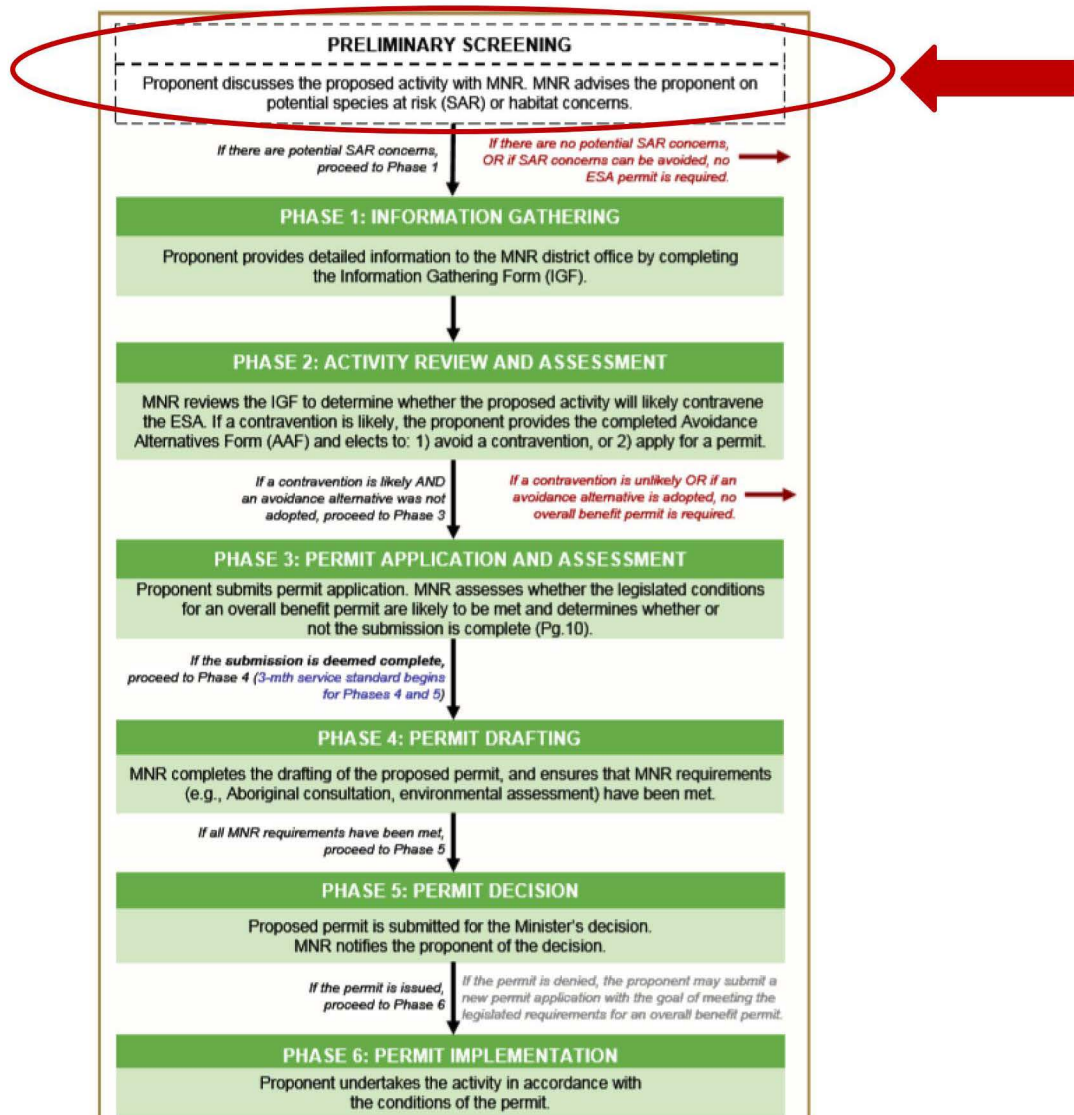
To provide the most efficient service, clients should initiate species at risk screenings and seek information from all applicable information sources identified in this guide, at a minimum, prior to contacting Government of Ontario ministry offices for further information or advice.

1.2 Scope

This guide is a resource for clients seeking to understand if their activity is likely to impact species at risk or if they are likely to trigger the need for an authorization under the ESA. It is not intended to circumvent any detailed site surveys that may be necessary to document species at risk or their habitat nor to circumvent the need to assess the impacts of a proposed activity on species at risk or their habitat. This guide is not an exhaustive list of available information sources for any given area as the availability of information on species at risk and their habitat varies across the province. This guide is intended to support projects and activities carried out on Crown and private land, by private landowners, businesses, other provincial ministries and agencies, or municipal government.

1.3 Background and Context

To receive advice on their proposed activity, clients must first determine whether any species at risk or their habitat exist or are likely to exist at or near their proposed activity, and whether their proposed activity is likely to contravene the ESA. Once this step is complete, clients may contact the ministry at SAROntario@ontario.ca to discuss the main purpose, general methods, timing and location of their proposed activity as well as information obtained about species at risk and their habitat at, or near, the site. At this stage, the ministry can provide advice and guidance to the client about potential species at risk or habitat concerns, measures that the client is considering to avoid adverse effects on species at risk or their habitat and whether additional field surveys are advisable. This is referred to as the “Preliminary Screening” stage. For more information on additional phases in the diagram below, please refer to the *Endangered Species Act Submission Standards for Activity Review and 17(2)(c) Overall Benefit Permits* policy available online at <https://www.ontario.ca/page/species-risk-overall-benefit-permits>



2.0 Roles and Responsibilities

To provide the most efficient service, clients should initiate species at risk screenings and seek information from all applicable information sources identified in this guide prior to contacting Government of Ontario ministry offices for further information or advice.

Step 1: Client seeks information regarding species at risk or their habitat that exist, or are likely to exist, at or near their proposed activity by referring to all applicable information sources identified in this guide.

Step 2: Client reviews and consider guidance on whether their proposed activity is likely to contravene the ESA (see section 3.4 of this guide for guidance on what to consider).

Step 3: Client gathers information identified in the checklist in section 4 of this guide.

Step 4: Client contacts the ministry at SAROntario@ontario.ca to discuss their preliminary screening. Ministry staff will ask the client questions about the main purpose, general methods, timing and location of their proposed activity as well as information obtained about species at risk and their habitat at, or near, the site. Ministry staff will also ask the client for their interpretation of the impacts of their activity on species at risk or their habitat as well as measures the client has considered to avoid any adverse impacts.

Step 5: Ministry staff will provide advice on next steps.

Option A: Ministry staff may advise the client they can proceed with their activity without an authorization under the ESA where the ministry is confident that:

- no protected species at risk or habitats are likely to be present at or near the proposed location of the activity; or
- protected species at risk or habitats are known to be present but the activity is not likely to contravene the ESA; or
- through the adoption of avoidance measures, the modified activity is not likely to contravene the ESA.

Option B: Ministry staff may advise the client to proceed to Phase 1 of the overall benefit permitting process (i.e. Information Gathering in the previous diagram), where:

- there is uncertainty as to whether any protected species at risk or habitats are present at or near the proposed location of the activity; or
- the potential impacts of the proposed activity are uncertain; or
- ministry staff anticipate the proposed activity is likely to contravene the ESA.

3.0 Information Sources

Land Information Ontario (LIO) and the Natural Heritage Information Centre (NHIC) maintain and provide information about species at risk, as well as related information about fisheries, wildlife, crown lands, protected lands and more. This information is made available to organizations, private individuals, consultants, and developers through online sources and is often considered under various pieces of legislation or as part of regulatory approvals and planning processes.

The information available from LIO or NHIC and the sources listed in this guide should not be considered as a substitute for site visits and appropriate field surveys. Generally, this information can be regarded as a starting point from which to conduct further field surveys, if needed. While this data represents best available current information, it is important to note that a lack of information for a site does not mean that species at risk or their habitat are not present. There are many areas where the Government of Ontario does not currently have information, especially in more remote parts of the province. The absence of species at risk location data at or near your site does not necessarily mean no species at risk are present at that location. On-site assessments can better verify site conditions, identify and confirm presence of species at risk and/or their habitats.

Information on the location (i.e. observations and occurrences) of species at risk is considered sensitive and therefore publicly available only on a 1km square grid as opposed to as a detailed point on a map. This generalized information can help you understand which species at risk are in the general vicinity of your proposed activity and can help inform field level studies you may want to undertake to confirm the presence, or absence of species at risk at or near your site.

Should you require specific and detailed information pertaining to species at risk observations and occurrences at or near your site on a finer geographic scale; you will be required to demonstrate your need to access this information, to complete data sensitivity training and to obtain a Sensitive Data Use License from the NHIC. Information on how to obtain a license can be found online at <https://www.ontario.ca/page/get-natural-heritage-information>.

Many organizations (e.g. other Ontario ministries, municipalities, conservation authorities) have ongoing licensing to access this data so be sure to check if your organization has this access and consult this data as part of your preliminary screening if your organization already has a license.

3.1 Make a Map: Natural Heritage Areas

The Make a Natural Heritage Area Map (available online at http://www.gisapplication.lrc.gov.on.ca/mamnh/Index.html?site=MNR_NHLUPS_NaturalHeritage&viewer=NaturalHeritage&locale=en-US) provides public access to natural heritage information, including species at risk, without the user needing to have Geographic Information System (GIS) capability. It allows users to view and identify generalized species at risk information, mark areas of interest, and create and print a custom map directly from the web application. The tool also shows topographic information such as roads, rivers, contours and municipal boundaries.

Users are advised that sensitive information has been removed from the natural areas dataset and the occurrences of species at risk has been generalized to a 1-kilometre grid to mitigate the risks to the species (e.g. illegal harvest, habitat disturbance, poaching).

The web-based mapping tool displays natural heritage data, including:

- Generalized Species at risk occurrence data (based on a 1-km square grid),
- Natural Heritage Information Centre data.

Data cannot be downloaded directly from this web map; however, information included in this application is available digitally through Land Information Ontario (LIO) at <https://www.ontario.ca/page/land-information-ontario>.

3.2 Land Information Ontario (LIO)

Most natural heritage data is publicly available. This data is managed in a large provincial corporate database called the LIO Warehouse and can be accessed online through the LIO Metadata Management Tool at <https://www.javacoeapp.lrc.gov.on.ca/geonetwork/srv/en/main.home>. This tool provides descriptive information about the characteristics, quality and context of the data. Publicly available geospatial data can be downloaded directly from this site.

While most data are publicly available, some data may be considered highly sensitive (i.e. nursery areas for fish, species at risk observations) and as such, access to some data maybe restricted.

3.3 Additional Species at Risk Information Sources

- The Breeding Bird Atlas can be accessed online at <http://www.birdsontario.org/atlas/index.jsp?lang=en>
- eBird can be accessed online at <https://ebird.org/home>
- iNaturalist can be accessed online at <https://www.inaturalist.org/>
- The Ontario Reptile and Amphibian Atlas can be accessed online at <https://ontarionature.org/programs/citizen-science/reptile-amphibian-atlas>
- Your local Conservation Authority. Information to help you find your local Conservation Authority can be accessed online at <https://conservationontario.ca/conservation-authorities/find-a-conservation-authority/>

Local naturalist groups or other similar community-based organizations

- Local Indigenous communities
- Local land trusts or other similar Environmental Non-Government Organizations
- Field level studies to identify if species at risk, or their habitat, are likely present or absent at or near the site.
- When an activity is proposed within one of the continuous caribou ranges, please be sure to consider the caribou Range Management Policy. This policy includes figures and maps of the continuous caribou range, can be found online at <https://www.ontario.ca/page/range-management-policy-support-woodland-caribou-conservation-and-recovery>

3.4 Information Sources to Support Impact Assessments

- Guidance to help you understand if your activity is likely to adversely impact species at risk or their habitat can be found online at <https://www.ontario.ca/page/policy-guidance-harm-and-harass-under-endangered-species-act> and <https://www.ontario.ca/page/categorizing-and-protecting-habitat-under-endangered-species-act>
- A list of species at risk in Ontario is available online at <https://www.ontario.ca/page/species-risk-ontario>. On this webpage, you can find out more about each species, including where it lives, what threatens it and any specific habitat protections that apply to it by clicking on the photo of the species.

4.0 Check-List

Please feel free to use the check list below to help you confirm you have explored all applicable information sources and to support your discussion with Ministry staff at the preliminary screening stage.

- ✓ Land Information Ontario (LIO)
- ✓ Natural Heritage Information Centre (NHIC)
- ✓ The Breeding Bird Atlas
- ✓ eBird
- ✓ iNaturalist
- ✓ Ontario Reptile and Amphibian Atlas
- ✓ List Conservation Authorities you contacted: _____

- ✓ List local naturalist groups you contacted: _____

- ✓ List local Indigenous communities you contacted: _____

- ✓ List any other local land trusts or Environmental Non-Government Organizations you contacted: _____

- ✓ List and field studies that were conducted to identify species at risk, or their habitat, likely to be present or absent at or near the site: _____

- ✓ List what you think the likely impacts of your activity are on species at risk and their habitat (e.g. damage or destruction of habitat, killing, harming or harassing species at risk): _____

From: Minkin, Dan (MCM) <Dan.Minkin@ontario.ca>
Sent: Monday, August 26, 2024 5:37 PM
To: Alonzo Moya, Carlos; Dania Chehab
Cc: Freya Wu; Carol Derrick
Subject: RE: R236814-20240724-Halton Hills #4 WWPS - Premier Gateway Employment Area Class EA Notice of Commencement [MCM File 0013436]
Attachments: 2024-08-26_PremierGateway_MCMcomments.pdf

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Good afternoon,
Please see attached.

Dan Minkin

Heritage Planner | Heritage Operations Branch
Ministry of Citizenship and Multiculturalism | Ontario Public Service
416-786-7553 | dan.minkin@ontario.ca



Taking pride in strengthening Ontario, its places and its people

From: Carol Derrick <cderrick@rvanderson.com>
Sent: Wednesday, July 24, 2024 10:18 AM
Cc: Freya Wu <mwu@rvanderson.com>
Subject: R236814-20240724-Halton Hills #4 WWPS - Premier Gateway Employment Area Class EA Notice of Commencement

CAUTION -- EXTERNAL E-MAIL - Do not click links or open attachments unless you recognize the sender.

Dear Sir/Madam:

Please see attached the Notice of Commencement for the Halton Hills #4 Wastewater Pumping Station – Premier Gateway Employment Area Municipal Class Environmental Assessment (Class EA).

The Regional Municipality of Halton is undertaking this Class EA to identify a preferred site for the Halton Hills #4 Wastewater Pumping Station based on current industry standards and due to growing development flows in the Premier Gateway Employment Area.

This notice is sent to your attention as it was deemed that you may be an interested stakeholder.

If you would like more information about the Halton Hills #4 Wastewater Pumping Station – Premier Gateway Employment Area or would like to be removed from the study's contact list, please contact the undersigned.

Yours very truly,

Dania Chehab

Consultant Project Manager
R.V. Anderson Associates Limited
2001 Sheppard Avenue East, Suite 300
Toronto, ON M2J 4Z8

Phone: 416-497-8600 ext. 1456
Email: dcchhab@rvanderson.com

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**Ministry of Citizenship
and Multiculturalism**

Heritage Planning Unit
Heritage Branch
Citizenship, Inclusion and
Heritage Division
5th Flr, 400 University Ave
Tel.: 416.786.7553

**Ministère des Affaires civiques
et du Multiculturalisme**

Unité de la planification relative au
patrimoine
Direction du patrimoine
Division des affaires civiques, de
l'inclusion et du patrimoine
Tél.: 416.786.7553



August 26, 2024

EMAIL ONLY

Carlos Alonzo Moya
Project Manager II, Halton Region
1151 Bronte Road
Oakville, ON L6M 3L1
carlos.alonzomoya@halton.ca

MCM File : **0013436**
Proponent : **Halton Region**
Subject : **Municipal Class EA – Schedule B – Notice of Commencement**
Project : **Halton Hills #4 Wastewater Pumping Station – Premier Gateway
Employment Area**
Location : **Town of Halton Hills, Ontario**

Dear Carlos Alonzo Moya:

Thank you for providing the Ministry of Citizenship and Multiculturalism (MCM) with the Notice of Commencement for the above-referenced project.

MCM's interest in this project relates to its mandate of conserving Ontario's cultural heritage, which includes:

- archaeological resources, including land and marine;
- built heritage resources, including bridges and monuments; and
- cultural heritage landscapes.

Under the EA process, the proponent is required to determine a project's potential impact on known (previously recognized) and potential cultural heritage resources.

Project Summary

Halton Region is initiating a Municipal Class Environmental Assessment study to identify the preferred site for the Halton Hills #4 Wastewater Pumping Station to support growth in the Premier Gateway Employment Area in the Town of Halton Hills. The new wastewater pumping station will service the area generally located north of Steeles Avenue between Ninth Line and Winston Churchill Boulevard. This project was identified in the 2008 South Halton Water and Wastewater Master Plan Update and confirmed in the 2011 Sustainable Halton Water and Wastewater Master Plan.

Identifying Cultural Heritage Resources

While some cultural heritage resources may have already been formally identified, others may be identified through screening and evaluation.

Archaeological Resources

Our records indicate that Stage 1 archaeological assessments (under Project Information Form numbers P051-0292-2023 and P051-0322-2024) have been initiated. We recommend that the licensed archaeologist submit the report to MCM prior to the issuance of the notice of completion.

Please note that archaeological concerns have not been fully addressed until reports have been entered into the Ontario Public Register of Archaeological Reports ('the Register') where those reports recommend that:

1. the archaeological assessment of the project area is complete and
2. all archaeological sites identified by the assessment are either of no further cultural heritage value or interest (as per Section 48(3) of the Ontario Heritage Act) or that mitigation of impacts has been accomplished through excavation or an avoidance and protection strategy.

Approval authorities and/or proponents should wait to receive the MCM's written confirmation that the archaeological assessment report(s) has been entered into the Register before issuing a decision or proceeding with any ground disturbing activities. The letter will also indicate either that there are no further concerns for impacts to archaeological resources or articulate next steps to mitigate those concerns.

Proponents must follow the recommendations of the archaeological assessment report(s). MCM recommends that further stages of archaeological assessment (if recommended) be undertaken as early as possible during detailed design and prior to any ground disturbing activities.

Built Heritage Resources and Cultural Heritage Landscapes

The Ministry's [Criteria for Evaluating Potential for Built Heritage Resources and Cultural Heritage Landscapes](#) should be completed to help determine whether this EA project may impact known or potential built heritage resources and/or cultural heritage landscapes.

If there is potential for built heritage resources and/or cultural heritage landscapes on the property or within the project area, a Cultural Heritage Evaluation Report (CHER) should be undertaken by a qualified person to determine the cultural heritage value or interest of the property (or project area). If the property (or project area) is determined to be of cultural heritage value or interest and alterations or development is proposed, MCM recommends that a Heritage Impact Assessment (HIA), prepared by a qualified consultant, be completed to assess potential project impacts. Please send the HIA to MCM (and the local municipality as appropriate) for review and comment and make it available to local organizations or individuals who have expressed interest in review.

Community input should be sought to identify locally recognized and potential cultural heritage resources. Sources include, but are not limited to, municipal heritage committees, historical societies and other local heritage organizations.

Cultural heritage resources are often of critical importance to Indigenous communities. Indigenous communities may have knowledge that can contribute to the identification of cultural heritage resources, and we suggest that any engagement with Indigenous communities includes a discussion about known or potential cultural heritage resources that are of value to them.

Environmental Assessment Reporting

All technical cultural heritage studies and their recommendations are to be addressed and incorporated into EA projects. Please advise MCM whether any technical cultural heritage studies will be completed for this EA project, and provide them to MCM before issuing a Notice of Completion or commencing any work on the site. If screening has identified no known or potential cultural heritage resources, or no impacts to these resources, please include the completed checklists and supporting documentation in the EA report or file.

Please note that the responsibility for administration of the *Ontario Heritage Act* and matters related to cultural heritage have been transferred from the Ministry of Tourism, Culture and Sport (MTCS) to the Ministry of Citizenship and Multiculturalism (MCM). Individual staff roles and contact information remain unchanged. Please continue to send any notices, report and/or documentation **via email only** to both Karla Barboza and myself.

- Karla Barboza, Team Lead - Heritage | Heritage Planning Unit (Citizenship and Multiculturalism) | 416-660-1027 | karla.barboza@ontario.ca
- Dan Minkin, Heritage Planner | Heritage Planning Unit (Citizenship and Multiculturalism) | 416.786.7553 | dan.minkin@ontario.ca

Thank you for consulting MCM on this project and please continue to do so throughout the EA process. If you have any questions or require clarification, please do not hesitate to contact me.

Sincerely,

Dan Minkin
Dan.Minkin@Ontario.ca

Copied to: Dania Chehab, R.V. Anderson

It is the sole responsibility of proponents to ensure that any information and documentation submitted as part of their EA report or file is accurate. The Ministry of Citizenship and Multiculturalism (MCM) makes no representation or warranty as to the completeness, accuracy or quality of the any checklists, reports or supporting documentation submitted as part of the EA process, and in no way shall MCM be liable for any harm, damages, costs, expenses, losses, claims or actions that may result if any checklists, reports or supporting documents are discovered to be inaccurate, incomplete, misleading or fraudulent.

Should previously undocumented archaeological resources be discovered, they may be a new archaeological site and therefore subject to Section 48(1) of the *Ontario Heritage Act*. The proponent or person discovering the archaeological resources must cease alteration of the site immediately and engage a licensed consultant archaeologist to carry out an archaeological assessment, in compliance with Section 48(1) of the *Ontario Heritage Act*.

The *Funeral, Burial and Cremation Services Act, 2002, S.O. 2002, c.33* requires that any person discovering human remains must cease all activities immediately and notify the police or coroner. If the coroner does not suspect foul play in the disposition of the remains, in accordance with *Ontario Regulation 30/11* the coroner shall notify the Registrar, Ontario Ministry of Public and Business Service Delivery, which administers provisions of that Act related to burial sites. In situations where human remains are associated with archaeological resources, the Ministry of Citizenship and Multiculturalism should also be notified (at archaeology@ontario.ca) to ensure that the archaeological site is not subject to unlicensed alterations which would be a contravention of the *Ontario Heritage Act*.

From: Slaght, Tyler <Tyler.Slaght@cvc.ca>
Sent: Tuesday, September 3, 2024 9:35 AM
To: Freya Wu
Cc: Jonathan Rudyk
Subject: RE: [External] R236814-20240724-Halton Hills #4 WWPS - Premier Gateway Employment Area Class EA Notice of Commencement - Follow up

[CAUTION EXTERNAL EMAIL] Make Sure that it is legitimate **before** Replying or Clicking on any links

Hi Freya,

The study area does not contain any features of interest to CVC. We request that appropriate SWM controls are implemented and that existing drainage patterns are maintained.

Regards,

Tyler Slaght | MCIP, RPP
Supervisor, Regulations and Development | Credit Valley Conservation
905-670-1615 ext. 4060 | M: 647-286-7427
tyler.slaght@cvc.ca | cvc.ca

Our working hours may be different. Please do not feel obligated to reply outside of your scheduled working hours. Let's work together to help foster healthy work-life boundaries.



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From: Slaght, Tyler
Sent: Thursday, August 22, 2024 12:41 PM
To: mwu@rvanderson.com
Cc: JRudyk@rvanderson.com
Subject: RE: [External] R236814-20240724-Halton Hills #4 WWPS - Premier Gateway Employment Area Class EA Notice of Commencement - Follow up

Hi Freya,

I wanted to let you know that I am the point of contact for this file. This is the first I am becoming aware of it. I will follow up as soon as possible.

Thanks,

Tyler Slaght | MCIP, RPP

Supervisor, Regulations and Development | Credit Valley Conservation

905-670-1615 ext. 4060 | M: 647-286-7427

tyler.slaght@cvc.ca | cvc.ca

Our working hours may be different. Please do not feel obligated to reply outside of your scheduled working hours. Let's work together to help foster healthy work-life boundaries.



**Credit Valley
Conservation**
inspired by nature

70
years



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From: Freya Wu <mwu@rvanderson.com>

Sent: Tuesday, August 20, 2024 11:33:24 AM

To: Campbell, Joshua <Joshua.Campbell@cvc.ca>

Cc: Jonathan Rudyk <JRudyk@rvanderson.com>

Subject: [External] R236814-20240724-Halton Hills #4 WWPS - Premier Gateway Employment Area Class EA Notice of Commencement - Follow up

You don't often get email from mwu@rvanderson.com. [Learn why this is important](#)

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Good morning Josh,

Hope this email finds you well.

My name is Freya and I am the project coordinator for the Premier Gateway Employment Area Class EA project.

We emailed you on July 24th, 2024 with a copy of the project Notice of Commencement (NOC). Just want to follow up and check if you have comments regarding the NOC or any questions regarding the project or specific review requirements.

Thank you,

Freya (Menghong) Wu, M.A.Sc. (she/her)

Process Designer



R.V. Anderson Associates Limited
2001 Sheppard Avenue East, Suite 300, Toronto ON M2J 4Z8
t 416 497 8600 ext. 1377

[LinkedIn](#) | [Facebook](#) | [Website](#)



From: Moore, Keith <Keith.Moore@haltonpolice.ca>
Sent: Tuesday, September 3, 2024 12:12 PM
To: Freya Wu
Cc: Bistas, Gus; Snow, Ryan
Subject: RE: Inquiry regarding receiving Class Environmental Assessment Notices

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Hi Freya:

You can forward them to my attention.

Thanks,

Keith

Keith Moore
Coordinator
Planning, Research and Emergency Management
Strategic Management Office
Halton Regional Police Service
905-825-4830

From: Bistas, Gus <Gus.Bistas@haltonpolice.ca>
Sent: September 3, 2024 12:09 PM
To: Moore, Keith <Keith.Moore@haltonpolice.ca>
Subject: FW: Inquiry regarding receiving Class Environmental Assessment Notices

Sir,

Just as an FYI,

From: Davies, Paul <Paul.Davies@haltonpolice.ca>
Sent: September 3, 2024 11:30 AM
To: Freya Wu <mwu@rvanderson.com>
Cc: Snow, Ryan <Ryan.Snow@haltonpolice.ca>; Bistas, Gus <Gus.Bistas@haltonpolice.ca>
Subject: RE: Inquiry regarding receiving Class Environmental Assessment Notices

Good Day,

I am no longer in my former roles in Traffic nor Strategic Management.

I have cc'd my successors here, and defer to them if indeed they would like notifications.

Thanks,

Paul

From: Freya Wu <mwu@rvanderson.com>
Sent: August 30, 2024 1:58 PM
To: Davies, Paul <Paul.Davies@haltonpolice.ca>
Cc: carlos.alonzomoya@halton.ca; Jonathan Rudyk <JRudyk@rvanderson.com>
Subject: Inquiry regarding receiving Class Environmental Assessment Notices

Good afternoon Paul,

Hope this email finds you well.

I am writing on behalf of R.V. Anderson Associates Limited, an engineering consulting company working with the Region of Halton on a wastewater pumping station project. The project study area is at Winston Churchill Boulevard and Steeles Avenue.

We are sending out Notice of Commencement for the Municipal Class Environmental Assessment (Class EA) Study to potential stakeholders and was wondering if Halton Regional Police would like to receive the Class EA Notices.

If Halton Regional Police would like to receive the Class EA Notices would this be the correct email to reach?

Thank you,

Freya (Menghong) Wu, M.A.Sc. (she/her)

Process Designer



R.V. Anderson Associates Limited
2001 Sheppard Avenue East, Suite 300, Toronto ON M2J 4Z8
t 416 497 8600 ext. 1377

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Jonathan Rudyk

From: [REDACTED]
Sent: January 31, 2025 8:42 PM
To: Farr, Andrew
Cc: Somerville, Clark; Carr, Gary; Gray, Bob; Jones, Lee Anne; jbrass@haltonhills.ca; jracinsky@haltonhills.ca; Bob Inglis; Lawlor, Ann; Jonathan Rudyk; Vivek.Nemani@halton.ca
Subject: Re: IMPORTANT: Halton Hills #4 Wastewater Pumping Station

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Thank you for your response regarding property access across our property. You can reach out to our Property Manager, [REDACTED] to coordinate the site visit. There are a number of rusty barrels still on our property - can these please be removed in the short term.

Continuing the discussion on our questions from last year, please be sure this is sent to the pertinent Regional staff.

We appreciate the re-sending of the response - it somehow did not make it to our inbox. In response to the Region's most recent provided information:

It's unclear if the Region's Transportation Development Review group direction is reflective of existing land-use and traffic regimes rather than approval as the Premier Gateway Node to Halton Hills at the intersection of two Major Regional Arterial Roads.

Also, we do want to ensure that the study addresses:

1. Proximity to the signalised intersection of two significant transportation corridors (Regional Road 8 & 19), considering projected traffic volumes and intersection improvements to address capacity issues including conflicts with transit layby typically located in proximity to the NEW entrance.
2. Compatibility with existing Major Retail land-use on (our) adjacent property and permissible development as hotel or free-standing office buildings up to ten storeys.
3. The physical or economic reason that "an existing entrance or access via new local road requirements are not being applied".
4. That sewage infrastructure projects the desired corporate image consistent with the "strong visual" identity requirements of this high-profile GATEWAY to Halton Hills, per applicable policies, plans and by-laws ("The highest quality of development will occur at key locations, particularly at the Winston Churchill Blvd. and Steeles Ave. intersection, in order to promote a distinct visual presence and sense of arrival".)
5. Current requirements approved by Town Council and how they are met are clearly addressed in the study update, for example:
 - Future access to the Regional Road Network will be restricted and/or controlled ...limited to the connections with the road network identified for the Secondary Plan area.

- The greatest (building) heights will be encouraged to locate adjacent to Steeles and Winston Churchill Blvd.

The above reflects the current applicable requirements where:

On October 10, 2023, Council for the Town of Halton Hills adopted by-law 2023-0089 and policies to facilitate the development of such lands for employment purposes.

Further, March 5, 2024, the Regional Municipality of Halton approved with modifications local Official Plan No. 50, regarding the Premier Gateway Phase 2B Secondary Plan.

The current appeal is being scoped and will not affect the policies pertinent to this issue at hand.

There does not seem to be any technical basis preventing the WWPS being located at an existing access/lot or from existing or planned local road consistent with local planning authority and Regional requirements.

As the Region is no longer the approval authority, we hope the Town will implement their authority and Council approved policy requirements that are not permissive of the proposed location, if the Region does not conform.

Sincerely,

From: Farr, Andrew <Andrew.Farr@halton.ca>

Sent: January 22, 2025 3:47 PM

To: [REDACTED]

Cc: Somerville, Clark <clarks@haltonhills.ca>; Carr, Gary <Gary.Carr@halton.ca>; Gray, Bob <Bob.Gray@halton.ca>; Jones, Lee Anne <LeeAnne.Jones@halton.ca>; jbrass@haltonhills.ca <jbrass@haltonhills.ca>; jracinsky@haltonhills.ca <jracinsky@haltonhills.ca>; Bob Inglis <bobinglis@haltonhills.ca>; Lawlor, Ann <Mayor@haltonhills.ca>; 'JRudyk@rvanderson.com' <JRudyk@rvanderson.com>

Subject: RE: IMPORTANT: Halton Hills #4 Wastewater Pumping Station

Hello [REDACTED]

I hope this message finds you well. Thank you for your patience as we reviewed your property access concerns related to the field work completed next to your property as part of the New Wastewater Pumping Station on Steeles Avenue, west of Winston Churchill Boulevard (project number PR-2869B).

With respect to accessing the site adjacent to your property, unfortunately the contractor undertaking the field work did not clearly communicate their access plan and requirements to the project team. I would like to extend my sincere apologies for any inconvenience you and your business have experienced as a result.

We have shared your concerns with all contractors involved and reinforced that they are not authorized to enter your property moving forward.

We can confirm that, at this time, we do not anticipate any additional field work that involves heavy equipment. Site visits might be required by team members to conduct survey work and to take

measurements. Access through your property or parking on your driveway will not be permitted during these visits without an acceptable access plan in place.

Members of the project team would like to meet with you to review the damage to landscaping as a result of the field work and discuss restoration plans, as well as develop an acceptable access plan for future site visits. Halton's Project Manager, Vivek Nemani, will contact you shortly to arrange this meeting. If you require additional information about this project, please feel free to contact him at Vivek.Nemani@halton.ca or 289-834-1043.

I trust this addresses your concerns regarding this recent project work and I can assure you that the project team will work closely with you as the project progresses.

Thank you,

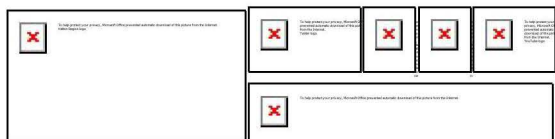
Andrew Farr, P.Eng.

Commissioner, Public Works

Public Works

Halton Region

905-825-6000, ext. 6040 | 1-866-442-5866



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From: Farr, Andrew <Andrew.Farr@halton.ca>

Sent: Thursday, January 16, 2025 10:37 AM

To: [REDACTED]

Cc: Somerville, Clark <clarks@haltonhills.ca>; Carr, Gary <Gary.Carr@halton.ca>; Gray, Bob <Bob.Gray@halton.ca>; Jones, Lee Anne <LeeAnne.Jones@halton.ca>; jbrass@haltonhills.ca; jracinsky@haltonhills.ca; Bob Inglis <bobinglis@haltonhills.ca>; Lawlor, Ann <Mayor@haltonhills.ca>; 'JRudyk@rvanderson.com' <JRudyk@rvanderson.com>

Subject: RE: IMPORTANT: Halton Hills #4 Wastewater Pumping Station

Hello [REDACTED]

Thank you for bringing these issues to our attention.

We are currently reviewing the matters that you raised concerning the Region's contractors with the parties involved and will advise shortly.

Please be assured that we take your concerns regarding access to the property very seriously and we have reminded our contractor that access to the site is not permitted until our review is complete.

With respect to access planning for the pumping station, a response to your comments was provided by email at the end of October and another copy has been attached for your review.

We will follow-up shortly regarding your concerns with the work currently underway.

Thank you,

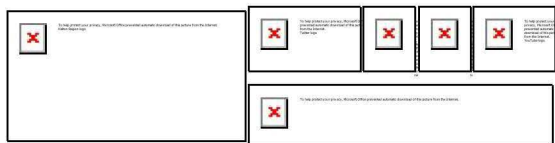
Andrew Farr, P.Eng.

Commissioner, Public Works

Public Works

Halton Region

905-825-6000, ext. 6040 | 1-866-442-5866



From: [REDACTED]

Sent: January 10, 2025 7:16 PM

To: Jonathan Rudyk <JRudyk@rvanderson.com>; Alonzo Moya, Carlos <carlos.alonzomoya@halton.ca>

Cc: jbrass@haltonhills.ca <jbrass@haltonhills.ca>; racinsky@haltonhills.ca <racinsky@haltonhills.ca>; bobinglis@haltonhills.ca <bobinglis@haltonhills.ca>; somerville01@bell.net <somerville01@bell.net>; mayor@haltonhills.ca <mayor@haltonhills.ca>

Subject: IMPORTANT: Halton Hills #4 Wastewater Pumping Station

Hello and Happy New Year.

We are writing to this group today on two matters.

The first is a follow up on our note below from last year that we did not receive a response to. I do hope we receive a response on this next week.

The second is a very concerning matter regarding the Region's contractors who are working on the Region's site studies, and the contractors' conduct.

With regard to the issues with the Region's contractors, and the site remediation we are now requesting, let me provide more detail.

The property under study does not have independent access. The Region's contractors had assumed that they had access from our property, without requesting authorization. While we attempted to be "good neighbours," their resulting actions really make us wish we had just called the police initially instead of holding off. There are three primary issues that we have been subjected to:

1. The entry onto our property and the blocking of the driveway of our business.
2. Destruction of the landscaping of our property, including movement of heavy machines and equipment, resulting in significant damage to the landscaping. At one point, a heavy vehicle was even parked on the front lawn over the septic tank. The contractors noted that there were 3 different companies on our property, and "nobody on site was the person responsible."
3. Ignoring of the lack of authorization to enter the property: by Christmas Eve we had to call the police to have all the parties vacate the premises – no customers could get into our store, and we couldn't even get the gate for our business closed because there were so many vehicles on the property lawns (Halton Police file number 24-

359089). Despite the police attending on Christmas Eve, again TODAY there were more vehicles parked on our lawn, and an assortment of contractors going across our property.

We had tried to address the issue on the 24th without police intervention, but 311 is not staffed significantly on the holidays, and neither a Region or Town staff member could identify who to call, and suggested we move forward with the police.

We have photos available that show the conduct of these contractors, and the damage done. However, it is also very clear to see when you attend the property.

Please get back to me with your plan for repairing the property, a confirmation that all of your contractors are aware that there is no permitted access from our property to the adjoining property, (and also a response to our outstanding note from last year, below.)

Thanks for your time.

Sincerely

From: [REDACTED]

Sent: October 2, 2024 8:12 PM

To: Jonathan Rudyk <JRudyk@rvanderson.com>; Alonzo Moya, Carlos <carlos.alonzomoya@halton.ca>; somerville01@bell.net <somerville01@bell.net>

Subject: Re: Halton Hills #4 Wastewater Pumping Station

Thank you for your response. We do not see where in the "Access Management Guideline"

1. A "Development" or "Regional Infrastructure" classification negates "spacing guideline of 250 to 400 metres between direct access points and potentially further in proximity to signalized intersections as supported by detailed studies".
2. "Right In Right Out" negates "spacing guideline of 250 to 400 metres between direct access points, and potentially further in proximity to signalized intersections as supported by detailed studies". For additional consideration, directly from the "Access Management Guideline" the EA would need to consider Right In and Right Out.

We are looking to have the Regional requirements most strenuously and technically reviewed within the EA process for this particular circumstance, which from the "Access Management Guideline" would be a Traffic Study that would support the assumptions being made by your firm. In the introduction of the "Access Management Guideline" it notes that this "Access Management Guideline" document provides the context for access approval for Halton Regional Roads. Its purpose is to provide direction to developers, planners, and consultants on Halton Region's best practices in access management.

We look forward to your prompt, technical response.

Sincerely

3.

From: Jonathan Rudyk <JRudyk@rvanderson.com>

Sent: September 27, 2024 9:17 AM

To: [REDACTED] Alonzo Moya, Carlos <Carlos.AlonzoMoya@halton.ca>

Subject: RE: Halton Hills #4 Wastewater Pumping Station

Hi [REDACTED]

My name is Jonathan Rudyk, I am taking over from Dania Chehab as the project manager from R.V. Anderson for this Class EA.

Thank you for your comments regarding site access from Steeles Avenue. The Region and RVA have and will continue to consider site access during the alternatives review, with site access being finalized as part of the detailed design of the project. The proposed wastewater pumping station is not considered development, but instead considered critical Regional infrastructure. The access into and out of the site will be Right in Right Out to avoid left hand turns into and out of the site in compliance with the Regional Municipality of Halton's Access Management Guideline. The proposed facility should not be a traffic-generating site, as the majority of its operation is managed remotely. At this time, it is anticipated that there may be occasional visits from Region Staff for maintenance purposes, and infrequent delivery of materials or equipment. All aspects of site access will be considered in greater detail through the detailed design process to provide safe travel for those using Steeles Avenue, Halton Region staff, and residents.

Please feel free to contact the project team if you have further questions.

Thanks,

Jonathan Rudyk, P. Eng (he/him)

Associate, Project Engineer, Water



R.V. Anderson Associates Limited
2001 Sheppard Avenue East, Suite 300, Toronto ON M2J 4Z8
t 416 497 8600 ext. 1497 | m 416 802 5048

[LinkedIn](#) | [Facebook](#) | [Website](#)



From: [REDACTED]

Sent: September 19, 2024 7:27 PM

To: Alonzo Moya, Carlos <Carlos.AlonzoMoya@halton.ca>

Subject: Fw: Halton Hills #4 Wastewater Pumping Station

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We received notice that the Project Manager Dania Chehab is no longer with R.V. Anderson

From: [REDACTED]
Sent: September 19, 2024 7:20 PM
To: dchehab@rvanderson.com carlos.alonzomoya@halton.ca <somerville01@bell.net>
Subject: Halton Hills #4 Wastewater Pumping Station

As owners of property adjacent to the "study area" for the above pumping station following are our concerns;

Halton has responsibility to manage the provision of access to the public road system for new development, to provide safe access consistent with the function and operation of the public road, impacts to needs of the adjacent land uses, and without exacerbating the existing safety problems.

New direct access in close proximity to the existing interchange at Steeles Ave. and Winston Churchill Blvd. does not represent careful control of the location, type and design of driveways and intersections.

Any new development within proximity to this major interchange must be supported by access via a local road, as described in the applicable design references:

"The Traffic Engineering Handbook, 6th Edition" indicates that "direct access to/from arterials should be discouraged or denied except where no alternative access exists".

Regional Municipality of Halton "Access Management Guideline" dated January 2015, indicates "spacing guideline of 250 to 400 metres between direct access points, and potentially further in proximity to signalized intersections as supported by detailed studies".

Sincerely,

[REDACTED]

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Jonathan Rudyk

From: Alonzo Moya, Carlos <Carlos.AlonzoMoya@halton.ca>
Sent: October 28, 2024 3:41 PM
To: [REDACTED] Jonathan Rudyk
Cc: Longworth, Erin; Jones, Lee Anne; Rees, Alexandria; somerville01@bell.net
Subject: RE: Halton Hills #4 Wastewater Pumping Station

Categories: Filed by Newforma

[CAUTION EXTERNAL EMAIL] Make Sure that it is legitimate **before** Replying or Clicking on any links

Good afternoon, [REDACTED]

Thank you for contacting the project team about your concerns regarding access to the Halton Hills #4 Wastewater Pumping Station.

Through the Schedule B Municipal Class Environmental Assessment study (MCEA study) for the wastewater pumping station, traffic analysis is being completed as required by the Region's Access Management Guidelines (the Guidelines). The Guidelines provide a framework for evaluating that new or modified property accesses maintain a high level of service for through-traffic, while providing reasonable access to abutting properties.

The assessment can take various forms depending on the nature of development, and in some cases, as noted in the Guidelines, a Traffic Impact Study may be required. In consultation with the Region's Transportation Development Review group, which is responsible for implementing the Guidelines, it was determined that, given the site specifics and that the proposed facility is remotely managed, it is not expected to be a significant traffic-generating site and that a full Traffic Impact Study was not required to identify the traffic impact.

As a note, it is proposed that access to the wastewater pumping station will be restricted to right-in/right-out movements and is permitted through the Access Management Guidelines (2015) as noted below:

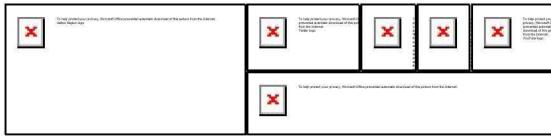
The general spacing guidelines for right in/out access is 115 metres to 140 metres between accesses. Where spacing is not conducive to the land use, physical or economical reasons, a spacing of 30 metres to 60 metres may be considered.

As part of the Schedule B MCEA study process and in coordination with the internal review by Halton's Transportation Development Review group, a transportation letter that documents anticipated trip generation and distribution from the site and resulting alignment with the Guidelines will be submitted. The analysis will be documented in the Project File for the MCEA study, and the requirements of the access will be finalized through the project's detailed design to support safe access for all users.

Please do not hesitate to contact me if you have any further questions.

Best regards,
Carlos Alonzo Moya

Carlos Alonzo Moya (he/him/his), M.A.Sc., P.Eng.



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From: [REDACTED]
Sent: Wednesday, October 2, 2024 8:13 PM
To: Jonathan Rudyk <JRudyk@rvanderson.com>; Alonzo Moya, Carlos <Carlos.AlonzoMoya@halton.ca>; somerville01@bell.net
Subject: Re: Halton Hills #4 Wastewater Pumping Station

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Thank you for your response. We do not see where in the "Access Management Guideline"

1. A "Development" or "Regional Infrastructure" classification negates "spacing guideline of 250 to 400 metres between direct access points and potentially further in proximity to signalized intersections as supported by detailed studies".
2. "Right In Right Out" negates "spacing guideline of 250 to 400 metres between direct access points, and potentially further in proximity to signalized intersections as supported by detailed studies". For additional consideration, directly from the "Access Management Guideline" the EA would need to consider Right In and Right Out.

We are looking to have the Regional requirements most strenuously and technically reviewed within the EA process for this particular circumstance, which from the "Access Management Guideline" would be a Traffic Study that would support the assumptions being made by your firm. In the introduction of the "Access Management Guideline" it notes that this "Access Management Guideline" document provides the context for access approval for Halton Regional Roads. Its purpose is to provide direction to developers, planners, and consultants on Halton Region's best practices in access management.

We look forward to your prompt, technical response.

Sincerely

[REDACTED]

3.

From: Jonathan Rudyk <JRudyk@rvanderson.com>
Sent: September 27, 2024 9:17 AM

To: [REDACTED] Alonzo Moya, Carlos <Carlos.AlonzoMoya@halton.ca>
Subject: RE: Halton Hills #4 Wastewater Pumping Station

Hi [REDACTED]

My name is Jonathan Rudyk, I am taking over from Dania Chehab as the project manager from R.V. Anderson for this Class EA.

Thank you for your comments regarding site access from Steeles Avenue. The Region and RVA have and will continue to consider site access during the alternatives review, with site access being finalized as part of the detailed design of the project. The proposed wastewater pumping station is not considered development, but instead considered critical Regional infrastructure. The access into and out of the site will be Right in Right Out to avoid left hand turns into and out of the site in compliance with the Regional Municipality of Halton's Access Management Guideline. The proposed facility should not be a traffic-generating site, as the majority of its operation is managed remotely. At this time, it is anticipated that there may be occasional visits from Region Staff for maintenance purposes, and infrequent delivery of materials or equipment. All aspects of site access will be considered in greater detail through the detailed design process to provide safe travel for those using Steeles Avenue, Halton Region staff, and residents.

Please feel free to contact the project team if you have further questions.

Thanks,

Jonathan Rudyk, P. Eng (he/him)

Associate, Project Engineer, Water



R.V. Anderson Associates Limited
2001 Sheppard Avenue East, Suite 300, Toronto ON M2J 4Z8
t 416 497 8600 ext. 1497 | m 416 802 5048

[LinkedIn](#) | [Facebook](#) | [Website](#)



From: [REDACTED]
Sent: September 19, 2024 7:27 PM
To: Alonzo Moya, Carlos <Carlos.AlonzoMoya@halton.ca>
Subject: Fw: Halton Hills #4 Wastewater Pumping Station

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We received notice that the Project Manager Dania Chehab is no longer with R.V. Anderson

Sent: September 19, 2024 7:20 PM
To: dchehab@rvanderson.com carlos.alonzomoya@halton.ca <somerville01@bell.net>
Subject: Halton Hills #4 Wastewater Pumping Station

As owners of property [REDACTED] to the "study area" for the above pumping station following are our concerns;

Halton has responsibility to manage the provision of access to the public road system for new development, to provide safe access consistent with the function and operation of the public road, impacts to needs of the adjacent land uses, and without exacerbating the existing safety problems.

New direct access in close proximity to the existing interchange at Steeles Ave. and Winston Churchill Blvd. does not represent careful control of the location, type and design of driveways and intersections.

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"The Traffic Engineering Handbook, 6th Edition" indicates that "direct access to/from arterials should be discouraged or denied except where no alternative access exists".

Regional Municipality of Halton "Access Management Guideline" dated January 2015, indicates "spacing guideline of 250 to 400 metres between direct access points, and potentially further in proximity to signalized intersections as supported by detailed studies".

Sincerely,

[REDACTED]

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Jonathan Rudyk

From: Pasquale, Christopher <Christopher.Pasquale@halton.ca>
Sent: July 24, 2025 1:51 PM
To: Jonathan Rudyk
Cc: Freya Wu
Subject: FW: Halton Hills #4 Wastewater Pumping Station
Attachments: L - Halton Hills No 4 Wastewater Pumping Station Municipal Class Environmental Assessment Study.pdf

Categories: Filed by Newforma

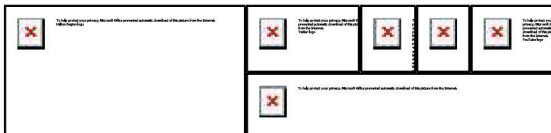
[CAUTION EXTERNAL EMAIL] Make Sure that it is legitimate **before** Replying or Clicking on any links

Hi Jonathan,

FYI, see below email and attached response letter that went to Lisa Teggart. Please add to the consultation appendix and proceed with the AODA process.

Thanks,
Chris

Christopher Pasquale, P.Eng., PMP
Project Manager II
Infrastructure and Environmental Services
Public Works
Halton Region
289-834-4731 x7134 | 1-866-442-5866



From: Di Tomasso, Dan <Dan.DiTomasso@halton.ca>
Sent: Thursday, July 24, 2025 12:55 PM
To: Mills, Chris <Chris.Mills@halton.ca>; Pasquale, Christopher <Christopher.Pasquale@halton.ca>; Giles, Graham <Graham.Giles@halton.ca>
Subject: Fw: Halton Hills #4 Wastewater Pumping Station

Hi Guys

FYI response letter went back to the resident beside HH#4, see attached for your info/review/action/records.

Thanks

Dan

Dan Di Tomasso, P.Eng

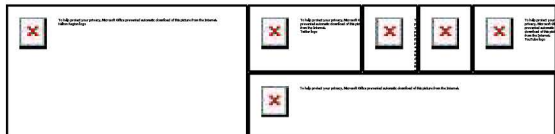
Director - Infrastructure & Environmental Services

Infrastructure and Environmental Services

Public Works

Halton Region

905-825-6000, ext. 7983 | 1-866-442-5866



From: Jones, Lee Anne <LeeAnne.Jones@halton.ca>
Sent: 24 July 2025 12:52
To: [REDACTED]
Cc: Di Tomasso, Dan <Dan.DiTomasso@halton.ca>
Subject: RE: Halton Hills #4 Wastewater Pumping Station

Hi [REDACTED]

Thank you for your patience as we completed our review. Attached please find letter in response to your concerns.

Please feel free to reach out to me if you have any additional questions or concerns.

Thank you,

Lee Anne Jones, P. Eng.

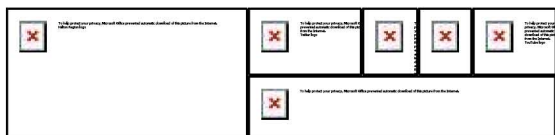
Commissioner

Office of the Commissioner

Public Works

Halton Region

905-825-6000, ext. 7547 | 1-866-442-5866



From: Di Tomasso, Dan <Dan.DiTomasso@halton.ca>
Sent: Wednesday, July 16, 2025 9:22 AM
To: [REDACTED]
Cc: Connell, Mark <Mark.Connell@halton.ca>; Jones, Lee Anne <LeeAnne.Jones@halton.ca>
Subject: Re: Halton Hills #4 Wastewater Pumping Station

Good Morning Ms. Teggart,

Thanks for following up and apologies for the delay. I don't believe we require anything further at this time, we are still working on the response and should have something back to you shortly.

Best Regards,
Dan

From: [REDACTED]
Sent: Tuesday, July 15, 2025 18:31
To: Di Tomasso, Dan <Dan.DiTomasso@halton.ca>
Cc: Connell, Mark <Mark.Connell@halton.ca>; Jones, Lee Anne <LeeAnne.Jones@halton.ca>
Subject: Re: Halton Hills #4 Wastewater Pumping Station

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Hello,

I wanted to check in on the status of your review, and ask if any further assistance from our team could be helpful.

Thanks, and have a great evening.

Regards,

[REDACTED]

[REDACTED]

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From: Di Tomasso, Dan <Dan.DiTomasso@halton.ca>
Date: Friday, June 20, 2025 at 10:44 AM
To: [REDACTED]
Cc: Connell, Mark <Mark.Connell@halton.ca>, Jones, Lee Anne <LeeAnne.Jones@halton.ca>
Subject: Re: Halton Hills #4 Wastewater Pumping Station

Good Morning [REDACTED]

I hope this email finds you well. I wanted to follow up and let you know that the Region is still in the process of reviewing the information you provided following your meeting with Mark Connell on May 15, 2025. The Region is currently reviewing the materials, and we're working on a thorough response. We expect to have an update for you within the next few weeks.

If you have any questions in the meantime, please don't hesitate to reach out.

Kind Regards,
Dan Di Tomasso

From: [REDACTED]
Sent: Monday, April 28, 2025 10:58 AM
To: Jones, Lee Anne <LeeAnne.Jones@halton.ca>
Subject: Re: Halton Hills #4 Wastewater Pumping Station

Hello Lee Anne,

Thanks for the note. No questions from our side. I am hoping that a direct conversation with Andrew will move us forward, and I would prefer there not be a crowd.

Have a great day.

Regards,

[REDACTED]

[REDACTED]

From: Jones, Lee Anne <LeeAnne.Jones@halton.ca>
Date: Monday, April 28, 2025 at 9:00 AM
To: [REDACTED]
Subject: RE: Halton Hills #4 Wastewater Pumping Station

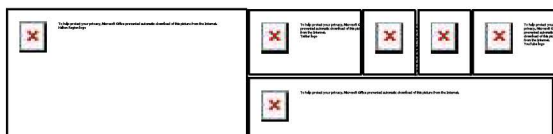
Good morning [REDACTED]

We are working on scheduling the meeting and wanted to follow-up on the questions that you'd like discussed. If you can forward those at your earliest convenience, we can ensure we have the right people at the meeting.

Thank you

Lee Anne Jones

Lee Anne Jones, P. Eng.
Director, Infrastructure & Environmental Services
Infrastructure and Environmental Services
Public Works
Halton Region
905-825-6000, ext. 7547 | 1-866-442-5866



From: [REDACTED]
Sent: Wednesday, April 23, 2025 11:26 AM
To: Farr, Andrew <Andrew.Farr@halton.ca>
Cc: Halton Homestead <haltonhomestead@hotmail.com>; Carr, Gary <Gary.Carr@halton.ca>; Somerville, Clark <clarks@haltonhills.ca>; Gray, Bob <Bob.Gray@halton.ca>; Jones, Lee Anne <LeeAnne.Jones@halton.ca>; Lawlor, Ann <AnnL@haltonhills.ca>; 'jbrass@haltonhills.ca' <jbrass@haltonhills.ca>; Matt Kindbom <mkindbom@haltonhills.ca>
Subject: Re: Halton Hills #4 Wastewater Pumping Station

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Hello Andrew,

I appreciate the offer and would like to get this meeting in the calendar as soon as possible. I am supplying my booking link to that end. If no times can be coordinated, please let me know what works best on your end I will look to coordinate my schedule if possible.

[REDACTED]

In advance of the session, I will send a summary of our concerns to date, so that we can focus the discussion on a workable solution agreeable by both parties. I have my “Property Manager” hat on for this meeting, and I will be presenting on behalf of the property owners.

Thanks again, and have a great day.

Regards,

[REDACTED]

[REDACTED]

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From: Farr, Andrew <Andrew.Farr@halton.ca>

Sent: March 6, 2025 4:10 PM

To: [REDACTED]

Cc: Carr, Gary <Gary.Carr@halton.ca>; Somerville, Clark <clarks@haltonhills.ca>; Gray, Bob <Bob.Gray@halton.ca>; Jones, Lee Anne <LeeAnne.Jones@halton.ca>; Lawlor, Ann <Annl@haltonhills.ca>; 'jbrass@haltonhills.ca' <jbrass@haltonhills.ca>

Subject: FW: Halton Hills #4 Wastewater Pumping Station

Hello [REDACTED]

Your correspondence to Councillor Somerville has been forwarded to our attention.

We appreciate that infrastructure construction adjacent to existing properties can have a temporary impact on existing uses and are committed to working with all landowners in the Study Area to deliver the required water and wastewater services in a responsible and timely fashion.

We would be happy to sit down with you and the Town of Halton Hills to discuss means that the project can be delivered without impacting on the current and future uses that you have identified for your property. Please provide us with your availability to meet and we will make the arrangements.

Thank you,

Andrew Farr, P.Eng.

Commissioner, Public Works

Public Works

Halton Region

905-825-6000, ext. 6040 | 1-866-442-5866

From: [REDACTED]
Sent: Sunday, March 2, 2025 5:27:20 PM
To: Clark Somerville <clarks@haltonhills.ca>
Cc: Mayor Lawlor <annl@haltonhills.ca>; Jason Brass <jbrass@haltonhills.ca>
Subject: Halton Hills #4 Wastewater Pumping Station

[EXTERNAL EMAIL]

Clark,

I hate to bother you on this, but we are quite frustrated.

This sewage station has always been presented on the South side of Steeles (South Halton Master Plan - Figure 14.3 Preferred Wastewater Servicing Strategy, Sustainable Halton Master Plan - Executive Summary ATTACHMENT 2 PREFERRED WASTEWATER SERVICING STRATEGY MAP, and during our attendance at many previous Public Information events).

And while the stated purpose of the current study is ***to identify the preferred site for the Halton Hills #4 Wastewater Pumping Station*** we understand that the Region preselected the location, and is just going through the motions in place of meaningful consultation and assessment.

There is no provable necessity to focus on creation of a new lot adjacent to our property when they could easily set their view on an existing parcel on the South side of Steeles or along Tenth Line with existing access as planned since 2002. We have not been provided any technical or financial justification that would support this change.

Our family business has supported the community for more than 40 years and served as a landmark at the Premier Gateway to Halton Hills of Whaley's Corner.

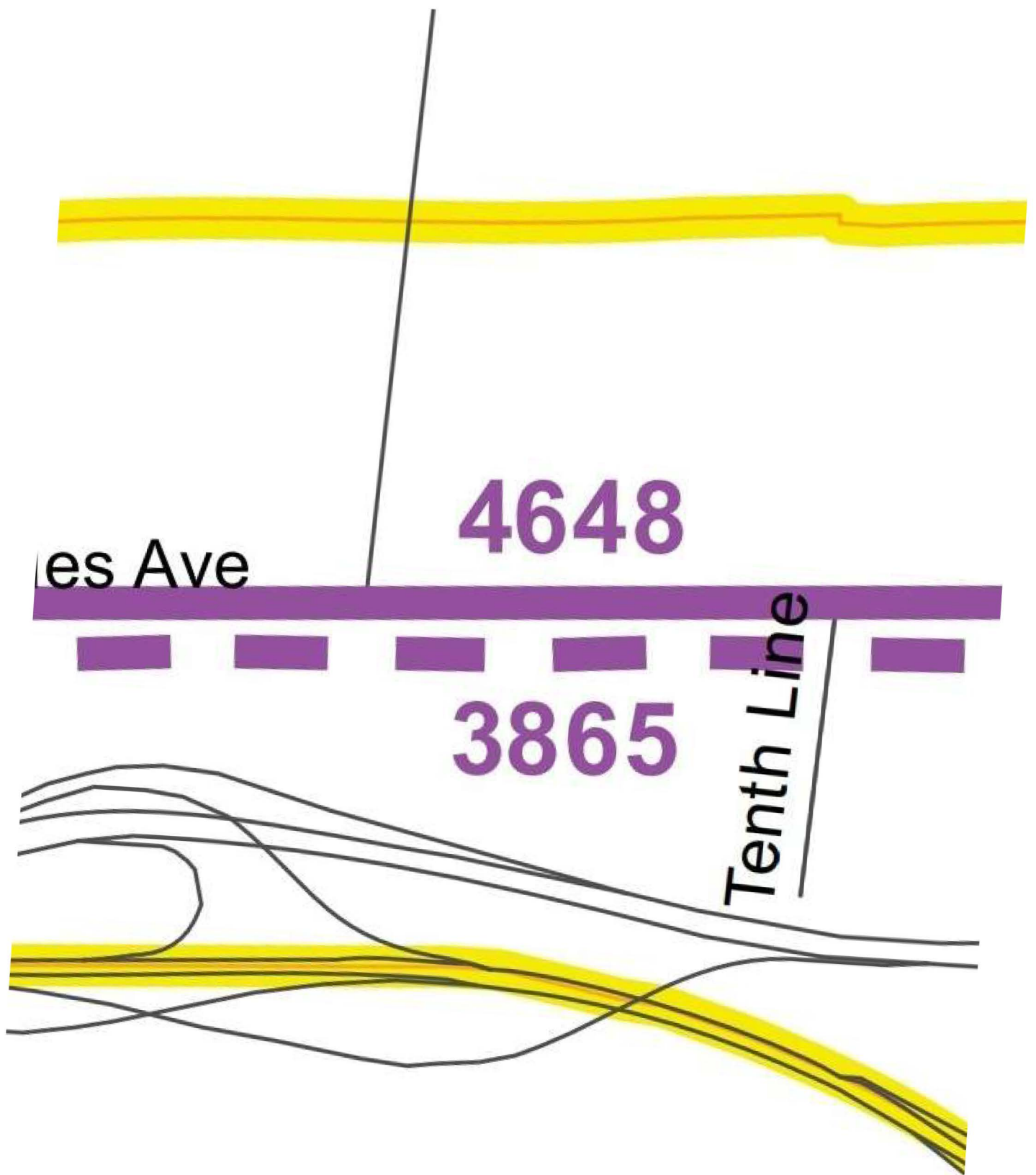
There is no reason for the Region to encumber our business and property through creation of a new parcel for sewage works at odds with standard engineering criteria, Region and Town plans and guidelines, and long standing Master Plans and feel we will be forced register a detailed objection to Class EA.

On Monday, we will advise the Region that we are no longer going to support whatever this charade is about, and there will be no access granted via our property.

I would also like to engage with Town staff on the statement that an exception was made for the Region- as there is no technical or financial justification pressing need for an exemption, I strongly suspect that protocols need to be put in place for merit based evaluations go forward.

Regards,

A solid black rectangular box used to redact the signature of the sender.



ECCHINI LINE

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16863 Steeles Avenue



MAY 23, 2025

PREPARED FOR JOHN & MARG TEGGART

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01 SUMMARY

This document collates the details from historical planning documents and consultation along with information discussed at our meeting May 15th, 2025.

The existing site features Halton Homestead, located in historic Whaley's Corners a crossroads community with a tradition as destination commercial use for hundreds of years, property is municipally known as 16863 Steeles Avenue in the Town of Halton Hills, Ontario

16863 Steeles Avenue operating as Halton Homestead Limited, has been in business at this location for more than 40 years. A major retail use marketing Canadian made, heirloom quality, luxury, handmade, solid wood, custom design and bespoke furniture and millwork solutions to the public. Our brand is built on providing excellence in customer experience and satisfaction, maintaining a Five (5) Star Google review as an example.

This existing Major Retail land use site is perfectly situated to benefit from the millions of luxury retail customers drawn to the area annually by the Toronto Premium Outlet mall, We have repeatedly engaged in Public Information Center, and in writing to both the Town and Region, regarding the transportation and development planning along Steeles Avenue, with concern for maintaining the viability of our business and personal interests, ensuring traffic safety, and that future development of our property is not encumbered.

There is the potential for diminished good-will for our business interests. Ontario's Expropriations Act, R.S.O. 1990, c. E.26 recognizes compensation for landowners for personal and business damages even if the municipality does not acquire part of the land of a subject property.

02 SCOPING OF ISSUES

In recognition of our concerns being escalated, the unresolved issues have been scoped as follows:

- A. Location and resultant land use compatibility
- B. Compliance with Town and Region plans, policies, and guidelines and good engineering practice; and
- C. Mitigating risk to private drinking water system.

SOLUTION FOCUSED

We are supportive of the overall development plan, and as such our focus is on proposed settlements:

Settlement 1

→ **Develop local road (right-of-way) between existing major retail land use and proposed lot creation to support a major sewage infrastructure use.**

- Access via a new local road addresses safety, separation, and other technical issues of establishing a new entrance from Steeles Avenue and provide safe access for both;
- Consistent with local planning authority plans and policies, Regional Road access management guidelines, and good engineering practice given proximity to intersection of major arterial roads; and
- Right-of-way will establish a permanent set-back, approximately 20 meters, buffering non-compatible land uses.
- Subject to appropriate landscape and architectural treatments, and design that prevent adverse effects from sewage odour, including interference with the normal conduct of business, proposed settlement would

Settlement 2

→ Provide municipal water service, disrupting contamination pathway between major sewage works and existing dug wells, resolving potential encumbrance, inclusive of private side cost for design, connection, and to decommission wells.

- Halton is obligated to provide compensation where land is injuriously affected by the construction, maintenance or operation of sewage works infrastructure [R.S.O. 1990, c. O.40, s. 58]
- Setback between sewage works, wet well, gravity sewers, and pressure mains, should exceed 60m, however bacterial contamination can move easily through soil and detailed studies may be required [Regulation 903 Water Supply Wells: Requirements and Best Practices, Table 4-1: Minimum Horizontal Separation Distances]
- Given legally existing commercial and residential land use, situated along existing municipal water system infrastructure, eligible for servicing via a 'connection request' and not subject to process for 'extension request' or 'development application' under the Halton's Official Plan Guidelines.
- Provision of water servicing for existing uses on the North side of Steeles Avenue under mandatory supply responsibility was adjudicated in an OMB decision (re: Snow v. Halton) benefitting our property.
- Existing watermain fronting property recently put into service under the Halton-Peel Cross-Boundary Water Servicing Agreement, which is permissive of additional service connections and subject Mandatory supply responsibility [Municipal Act, 2001 (section 86 (1))]
- Subject property is eligible for municipal water service under mandatory supply responsibility adjudicated through OMB, and Halton's current guidelines. Any cost burden on public and private side should be Halton's as the proponent for the major sewage infrastructure, this proposed settlement would resolve scoped issue C.

RESOLUTION

In aggregate, the two (2) proposed settlements would resolve scoped issues A, B and C. A formal agreement can preclude objections during, Notice of Completion, Zoning, Site Plan Approval, ECA Application, etc, and avoid litigation to addresses the injurious affection caused, and for the business losses, disturbance, nuisance and other damages probable.

02 NECESSITY

The Premier Gateway Employment Area from approximately 1km east of Ninth Line North would be serviced by proposed HH#4 Wastewater Pumping Station, the scheme includes:

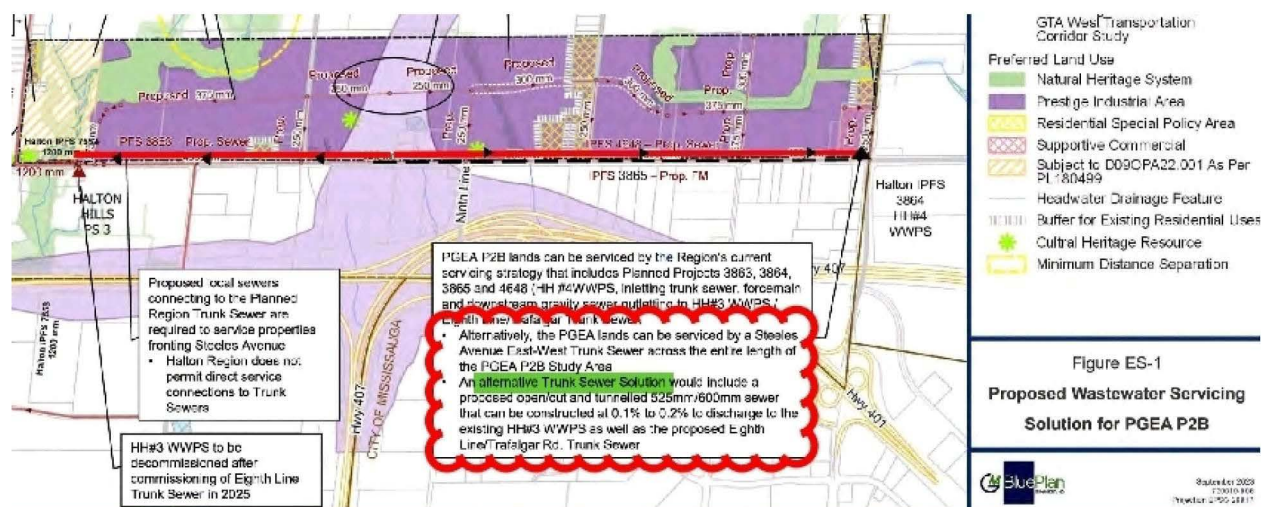
- New lot creation/property acquisition;
- New access to Steeles Avenue (RR 8);
- Site servicing, including power, gas, and communications;
- Sewage wet well, equipment gallery, mechanical, electrical, piping, instrumentation, superstructure;
- Standby power, driveway and parking, and landscaping;
- 2.5 km trunk sewer 450mm;
- 5 km forcemains (dual); and
- 1.5 km downstream trunk sewer (600mm) outletting to the 1,200mm Eighth Line/Trafalgar Trunk Sewer in proximity to the former Halton Hills #3 WWPS site.

This type of facility-based infrastructure, in addition to significant capital costs, will require significant energy inputs, and resultant GHGeq emissions). Additional ongoing costs will be incurred to monitor and maintain the complex mechanical and electrical components, and upkeep of the site and facilities.

Conversely, a gravity sewer generally has lower capital, labor, maintenance and repair costs. The same section of the Premier Gateway Employment Area can be serviced by a single gravity sewer.

- 4 km Open cut and tunneled 525mm/600mm trunk sewer outletting to the 1,200mm Eighth Line/Trafalgar Trunk Sewer in proximity to the former Halton Hills #3 WWPS site.
- ✓ 44% of the required overall sewer length
- ✓ Near 0 operational GHGeq emissions
- ✓ Near 0 operational energy inputs (Hydro, gas, and diesel) and resultant costs

Gravity sewers are the most reliable method of transferring sewage with lower lifecycle costs than pumping stations. [Niagara Region PW 29-2022 - Appendix 2]



Water and Wastewater Area Servicing Plan for the Premier Gateway Phase 2B Employment Area. GM BluePlan: September 2023.

Halton set a corporate target of *net-zero greenhouse gas emissions* and actions to *maintain tax rate increases at or below the rate of inflation*.

Implementing the **Alternative Trunk Sewer Solution** for servicing the eastern PGEA lands

- ✓ **reduces overall infrastructure;**
- ✓ **mitigates upward GHG and cost pressure; and**
- ✓ **can be expedited to support economic development.**

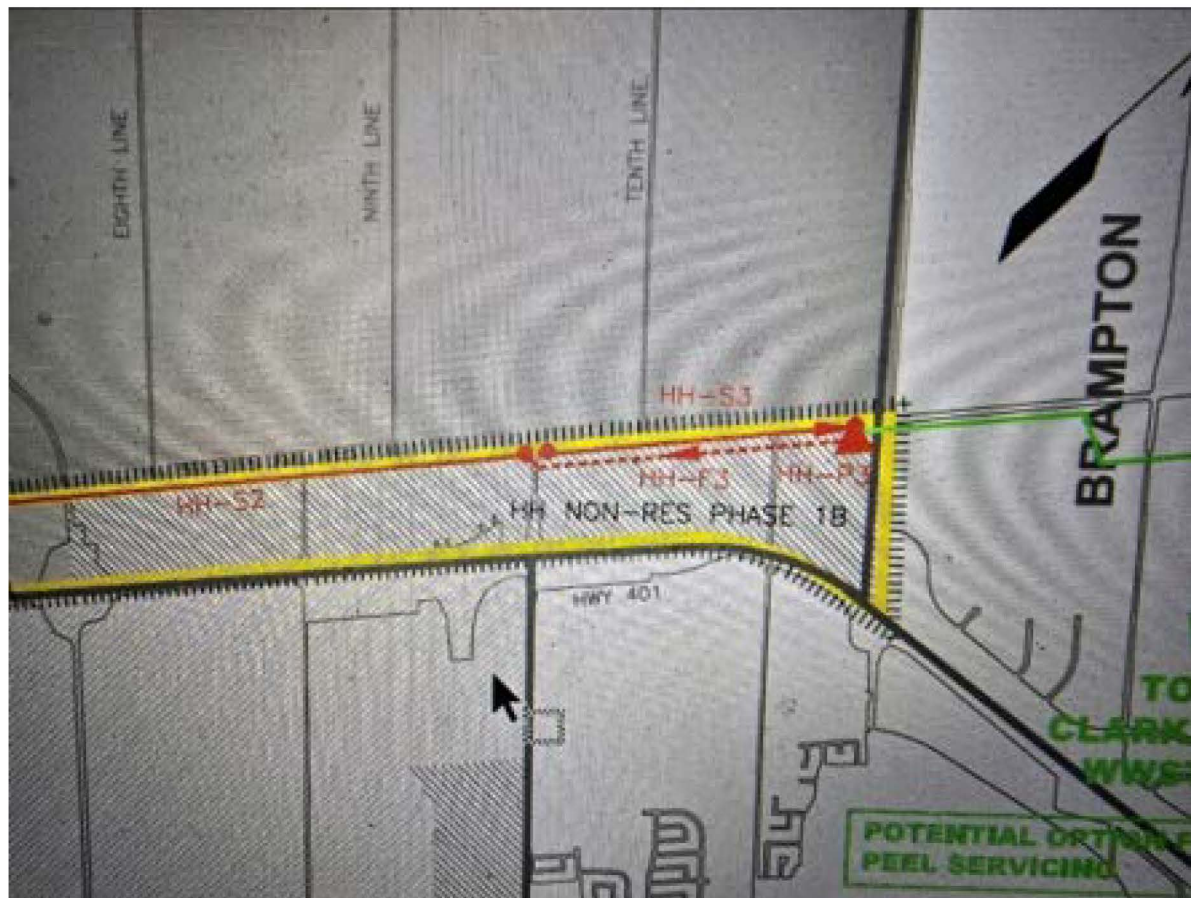
With the gravity sewer wholly located within an existing Regional Road, an opportunity is presented to catch up with the development which is already well underway in the

03 LOCATION

The sewage station was historically presented on the South side of Steeles, including the forcemains (consistent with proposed Urban Area at the time).

- South Halton Master Plan – Figure 14.3 Preferred Wastewater Servicing Strategy
- Sustainable Halton Master Plan – Executive Summary Attachment 2 Preferred Wastewater Servicing Strategy Map
- and during our attendance at many previous Public Information events, as noted March 2013 email:

> Date: Wed, 13 Mar 2013 21:53:42 -0400
 > From: email@haltonhomestead.com
 > To: somerville01@bell.net
 > Subject: Pump Stn 4 Steeles & Winston
 >
 > Hi Clark - Attached is our objection to Pump Station #4, which is HUGE,
 > and in our opinion does not conform to Town Official Plan. The Master
 > Plan shows it on the South side, what process was followed to change it?
 > What happened to planning for development of "Gateway". Why is this
 > being proposed for the North side of Steeles, to service development on
 > the South side of Steeles, when there are so many other more appropriate
 > locations for it, where it wouldn't have as great an impact. The
 > Engineer from Stantec and the Project Manager from the Region say it is
 > because of the flow of the pipes,
 >
 > So close to the intersection of Steeles and Winston which will greatly
 > impact Halton Hills future development opportunities.
 >
 > Thankn you for your attention
 >
 > John & Margaret Teggart
 > 905 858 9317



New Wastewater Pumping Station on Steeles Avenue, west of Winston Churchill Boulevard (project number PR-2869B).



Additional Tracking Items:

- 3.5. And while the stated purpose of the current study is ***to identify the preferred site for the Halton Hills #4 Wastewater Pumping Station*** we understand that the Region preselected the location and is just going through the motions in place of meaningful consultation and assessment.
- 3.6. There is no provable necessity to focus on creation of a new lot adjacent to our property when they could easily set their view on an existing parcel on the South side of Steeles or along Tenth Line with existing access as planned since 2002. We have not been provided any technical or financial justification that would support this change.
- 3.7. No soil sampling is being conducted for alternative locations confirming the preselection of the preferred location.
- 3.8. The site plan proposed by the Region (Dwg. C900, January 2013) did not conform to the Town's policy (401 Corridor Integrated Planning Project: Design Guidelines. Town of Halton Hills, 2000) and does not conform to Premier Gateway Employment Area Phase 2B Secondary Plan. OPA 50.

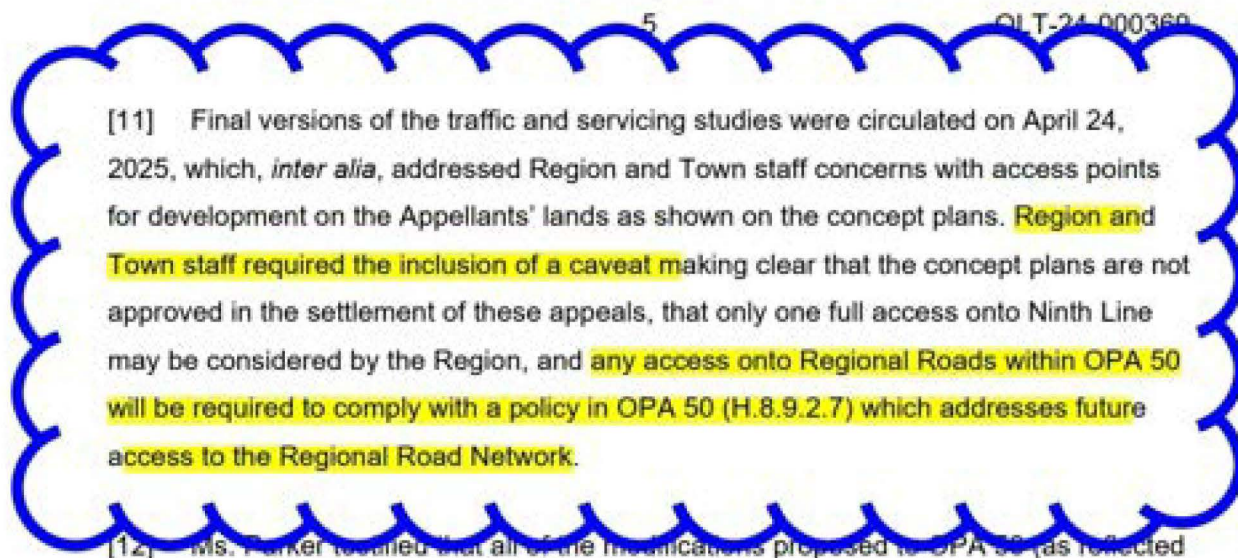
04 NEW REGIONAL ROAD ACCESS

New direct access in close proximity to the existing interchange at Steeles Ave. and Winston Churchill Blvd. does not represent careful control of the location, type and design of driveways and intersections.

- Proposed to be the fifth access point within the first 200m of the eastern gateway, a signalized intersection of two Regional transportation corridors (RR 8 - Steeles Avenue and RR 19 – Winston Churchill Blvd).
- No technical or financial necessity has been demonstrated to create new access consistent with governing municipal requirements.
- Official Plan Amendment 50, as modified and approved by Ontario Land Tribunal decision Order dated May 08, 2025 in case OLT 24-000369 brings the Secondary plan in to force.

<https://www.omb.gov.on.ca/e-decisions/OLT-24-000369-MAY-08-2025.PDF>

- As part of the evidence presented at the hearing, Halton and the Town reinforced that **any access onto Regional Roads within OPA 50 will be required to comply** with the policy and guidelines for access management.



Halton did not undertake the types of traffic study required by the highlighted policy.

Halton's [Access Management Guideline](#)

- The [Traffic Engineering Handbook](#), 6th Edition indicates that direct access to/from arterials should be discouraged or denied except where no alternative access exists. In addition, access to a property should be obtained from the local road system where possible. Where access must be provided from an arterial road, it should be proven to be safe without affecting the capacity of the roadway and balance the needs of all road users using criteria established within this Guideline
- Right in/out accesses should be considered where necessary and only when the minimum spacing requirements can be achieved. [2.1 RIGHT IN/OUT ACCESS]
- The general spacing guidelines for right in/out access is 115 metres to 140 metres between accesses. Where spacing is not conducive to the land use, physical or economical reasons, a spacing of 30 metres to 60 metres may be considered. [3.2 ACCESS SPACING]

Additional Tracking Items

- 3.9. It's unclear if the Region's Transportation Development Review group direction is reflective of existing land-use and traffic regimes rather than approval as the Premier Gateway Node to Halton Hills at the intersection of two (2) Major Regional Arterial Road which demand a higher degree of access control.
- 3.10. Secondary Plan – Future access to Regional Road network will be restricted and/or controlled, limited to the connections with the new local road network identified for the Secondary Plan area
- 3.11. Proximity to the signalised intersection of two significant transportation corridors (Regional Road 8 & 19), considering projected traffic volumes and intersection improvements to address capacity issues including conflicts with transit layby typically located in proximity to the proposed NEW entrance.
- 3.12. The physical or economical reason that an existing entrance or access via new local road requirements are not being applied.
- 3.13. Spacing guideline of 250 to 400 metres between direct access points and potentially further in proximity **to signalized intersections as supported by detailed studies**
- 3.14. Access to development lots shall be from local streets only. (S.1.2 Site Access)
- 3.15. Future access to Steeles Avenue:
 - will not cause traffic hazards or an unacceptable level of congestion (D3.5.3.3.a)
 - will not limit the ability for logical development of adjacent parcels (D3.5.3.3.b).

- 3.16. Good Engineering Practice: The Traffic Engineering Handbook, 6th Edition indicates that “direct access to/from arterials should be discouraged or denied except where no alternative access exists”.
- 3.17. Regional Municipality of Halton “Access Management Guideline” dated January 2015, indicates “spacing guideline of 250 to 400 metres between direct access points, and potentially further in proximity to signalized intersections as supported by detailed studies.”
- 3.18. Halton has responsibility to manage the provision of access to the public road system for new development, to provide safe access consistent with the function and operation of the public road, impacts to needs of the adjacent land uses, and without exacerbating the existing safety problems.
- 3.19. The submitted site plan over-estimates separation between the existing access and the future access and has not been supported by the required transportation studies to demonstrate a traffic hazard is not being created.

05 COMPATIBLE LAND USES

Our existing and permitted Major Retail use (H8.6.1.4 (a)) and the adjacent corner lot Business Commercial Area designation make these among the only lots in Phase 2 (A & B) permissive of uses other than Prestige Industrial Area and thereby least compatible for collocating an odorous infrastructure site.

- Property identified as Prestige Industrial Area [Schedule H8 – Land Use]
- Policy of the Plan to prohibit major retail uses, except to recognize existing uses [H8.6.1.4 (a)]
- Abutting Business Commercial Area permissive of hotels and free-standing office buildings up to ten storeys in height [H8.7.2.4]

Additional Tracking Items:

- 3.20. The site plans promotion of berms to the south and east of the development site, would significantly impair the continued commercial viability of our property, interfering with visibility and signage.
 - 3.20.1. There shall be a minimum landscape buffer of 20 m provided where industrial uses abut residential uses. (S.3.4 Required Landscape Buffers)
 - 3.20.2. The highest quality of development will occur at key locations, particularly at the Winston Churchill Blvd. and Steeles Ave. intersection, in order to promote a distinct visual presence and sense of arrival.
 - 3.20.3. The greatest (building) heights will be encouraged to locate adjacent to Steeles and Winston Churchill Blvd.
 - 3.20.4. Commercial uses shall be concentrated at identified gateway development node. (S.4.3 Gateways)
- 3.21. The Official Plan recognizes that residential and institutional uses are sensitive to noise, vibration, odour, dust or other emissions, particulates or contaminants from industrial, transportation and utility sources.
- 3.22. Where new facilities are proposed in proximity to existing sensitive uses, a proponent will be required to undertake the necessary impact analysis and implement, as a condition of approval, the appropriate abatement measures, in accordance with Ministry of Environment, Conservation and Parks Guidelines and the Halton Region Land Use Compatibility Guidelines. [H8.13]
- 3.23. Odour may be caused by discharges from a variety of sources ... As a result, odour assessment, and odour management plans and control measures that are specific to the facility that is being proposed should be required at the Site Plan application stage to avoid odour release and off-site effects. [H8.13.2.1]

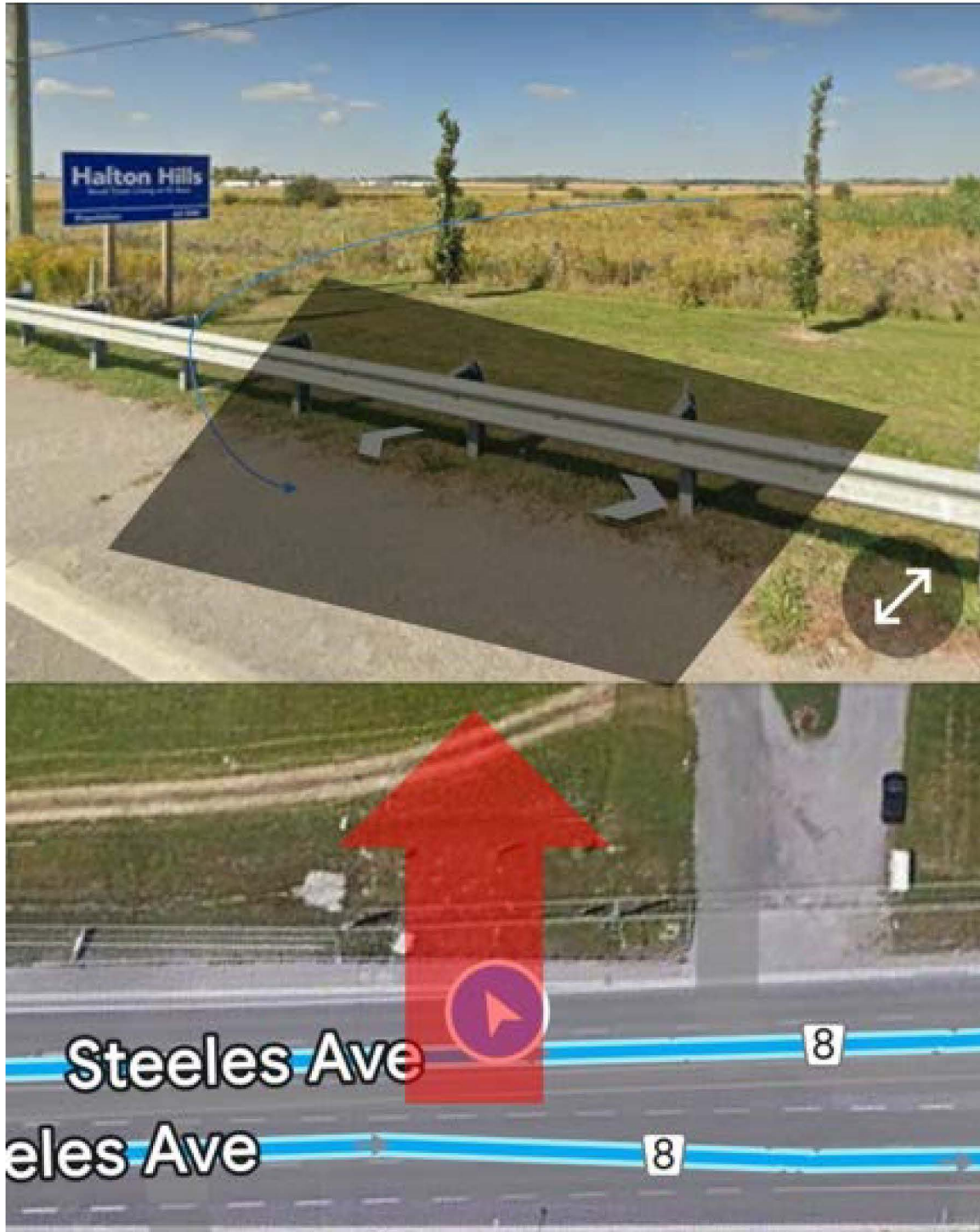
- 3.24. H8.13.2.2 Effective fugitive dust management will be required in
- 3.25. Suitable buffer between the existing land-use and the proposed obnoxious land use is of primary concern.
- 3.26. Setback between sewage works, wet well, gravity sewers, and pressure mains, should exceed 60m, however bacterial contamination can move easily through soil and detailed studies may be required [Regulation 903 Water Supply Wells: Requirements and Best Practices, Table 4-1: Minimum Horizontal Separation Distances]

06 NEW LOCAL ROAD ACCESS

SOLUTIONS FOCUS

A New Local Right of Way between the properties serves to address impact of a new access to Steeles Avenue and establishes a buffer that mitigates visibility impacts of berms and landscape buffers and is consistent with plans, policies, guidelines and good engineering practice.

- Access via a new local road addresses safety, separation, and other technical issues of establishing a new entrance from Steeles Avenue and provide safe access for both;
- Consistent with local planning authority plans and policies, Regional Road access management guidelines, and good engineering practice given proximity to intersection of major arterial roads; and
- Right-of-way will establishes a permanent set-back, approximately 20 meters, buffering non-compatible land uses. Subject to appropriate landscape and architectural treatments, and design that prevent adverse effects from sewage odour, including interference with the normal conduct of business,
- Town of Halton Hills also has a location and space to create high quality gateway signage appropriate to the eastern gateway node to Region of Halton and the Town of Halton Hills Premier Gateway Employment Area.
- While the existing demarcation signs may meet achieve minimum technical requirements but they are not appropriate to for defining this strategically important entrance.



Examples:

- Brampton's Gateway Beautification Program
- City of Markham's Gateway Master Plan
- The Blue Mountains Gateway & Wayfinding Project

PW-25-23WATER SUPPLY AGREEMENT WITH THE REGIONAL M_250524_160214[30]

Report To:	Regional Chair and Members of Regional Council
From:	Andrew Farr, Commissioner, Public Works
Date:	June 14, 2023
Report No:	PW-25-23
Re:	Water Supply Agreement with The Regional Municipality of Peel for Temporary Servicing of Properties in the Premier Gateway Employment Area

RECOMMENDATION

1. THAT Report No. PW-25-23 re: "Water Supply Agreement with The Regional Municipality of Peel for Temporary Servicing of Properties in the Premier Gateway Employment Area" be endorsed by Regional Council.
2. THAT the Commissioner of Public Works be authorized to negotiate, enter into and execute, a new agreement and any ancillary documents with The Regional Municipality of Peel including any future amendments, renewals, extensions or termination thereof, on legal terms satisfactory to the Director of Legal Services based upon the principles as set out in Report No. PW-25-23, to address the cross-boundary municipal water servicing arrangements between The Regional Municipality of Halton and The Regional Municipality of Peel regarding the Halton Hills Premier Gateway Employment Area.
3. THAT the Regional Clerk forward a copy of Report No. PW-25-23 to the Town of Halton Hills and The Regional Municipality of Peel for their information.

REPORT

Executive Summary

- This Report seeks Regional Council approval to enter into a temporary municipal water service connection from The Regional Municipality of Peel ("Peel Region") to service the Halton Hills Premier Gateway Employment Area on the basis that the connection can meet the conditions within a Water Supply Agreement with Peel Region.

- Conestoga Cold Storage Limited contacted Halton Region and Peel Region to request a municipal water connection, on an interim basis, through an existing emergency connection on Steeles Avenue (Regional Road 8) to service 16316 Steeles Avenue, 16408 Steeles Avenue, PIN 250260130 (LT) and 7876 Tenth Line South in the Town of Halton Hills.
- In discussions with Peel Region, the framework of a water supply agreement was developed where the existing emergency infrastructure would be commissioned and temporarily placed into service for connection to Halton Hills Premier Gateway Employment Area until Halton Region's permanent infrastructure is constructed.
- Halton Region's Urban Services Guidelines permit connections to other municipalities' infrastructure without an amendment to the Regional Official Plan, subject to criteria. Given the location of the subject lands and in recognition of the overall goals and objectives of the Regional Official Plan, consideration has been given to such a connection on a temporary basis.

Background

On March 6, 2023, staff received a letter from Conestoga Cold Storage Limited regarding the development of their lands located at 16316 Steeles Avenue, 16408 Steeles Avenue, PIN 250260130 (LT) and 7876 Tenth Line South in the Town of Halton Hills Premier Gateway Employment Area as shown in yellow in Attachment #1. The subject lands are located southwest of Steeles Avenue and Winston Churchill Boulevard (Regional Road 19) and close to the border with Peel Region in the City of Brampton. The letter requested that Halton Region provide water servicing to these lands.

The Conestoga Cold Storage Limited development represents an investment of approximately \$250 million and comprises a state-of-the-art warehouse facility spanning 290,000 square feet, along with ancillary office space. The proposed vertical cold storage warehouse is anticipated to create up to 250 jobs in Halton Region, including high-tech support positions. This facility is strategically planned to enhance Conestoga Cold Storage Limited's distribution network and complement its existing major cold storage facility located at Meadowpine Boulevard in the City of Mississauga.

Water and wastewater servicing needs for the Premier Gateway Area was identified through the 2008 Master Plan and included a new 600 mm watermain (ID 3844 and ID 5948) and new 525 mm and 375 mm wastewater mains (ID 3863 and ID 4648) to run along Steeles Avenue from Eighth Line to Winston Churchill Boulevard. Due to the topography, lands east of Ninth Line would drain to a new pumping station (ID 3864) at Winston Churchill Boulevard and be pumped through twin 250 mm forcemains (ID 3865) back to connect to the wastewater mains west of Ninth Line. The design of the required works was initiated in 2011. In 2013, Corridor Protection Zones were implemented through the announcement of planning for Highway 413, and the infrastructure projects were paused.

Implementation of municipal water and wastewater services to the Premier Gateway Area has now been re-initiated in coordination with the Town of Halton Hills' completion of the secondary planning process. A feasibility study has been initiated to confirm the wastewater servicing strategy, in light of the on-going construction of servicing to Georgetown and this study will be completed by the fall of 2023, followed by any identified Municipal Class Environmental Assessment requirements. It is then anticipated that design of the water and wastewater infrastructure projects will be finalized in mid-to-late 2025 allowing for construction to be completed late in 2027.

Halton Region's Official Plan policies has terms that would permit Conestoga Cold Storage Limited to proceed with private services in advance of municipal services being available however there are significant challenges with the servicing of these lands with groundwater. As a result, Conestoga Cold Storage Limited approached Peel Region regarding the ability to connect to an emergency connection that was constructed (but not placed in service) in 2011, as part of Peel Region's widening of Winston Churchill Boulevard project. The infrastructure consists of a meter chamber and approximately 270 m of 400 mm watermain on Steeles Avenue as noted in Attachment #1.

If approved, Conestoga Cold Storage Limited would undertake the construction of a connection to Regional infrastructure, including a segment of the new 600 mm watermain on Steeles Avenue. When the remainder of the 600 mm watermain on Steeles Avenue is constructed by Halton Region as part of Premier Gateway servicing, servicing from Peel Region would be terminated and Conestoga Cold Storage Limited would be required to connect to Halton Region's infrastructure, bearing all costs of connection and disconnection.

Discussion

Overview and Assessment of Criteria for Connection to Other Municipalities' Systems

The Regional Official Plan contains policies related to the provision of urban services in Halton Region. With respect to connections other municipal infrastructure systems, Section 89(17) of the Regional Official Plan states:

- Consider and permit, based on individual merit and in accordance with goals and objectives of this Plan, new urban service system interconnections between the Region of Halton and abutting municipalities.

In 2020, Halton Region's Urban Services Guidelines (<https://www.halton.ca/Repository/Urban-Services-Guidelines>) (the "Guidelines") were updated to provide more specific direction on how Halton Region would consider and permit connections to other municipalities' infrastructure. The updated Guidelines include a new process for advancing such connections without a Regional Official Plan Amendment.

Discussions with Peel Region

In discussions with Peel Region, the framework of a water supply agreement was developed where the existing emergency infrastructure would be commissioned and placed into service for temporary connection to the Premier Gateway Employment Area as shown in Attachment #2. Halton Region owns and maintains the infrastructure within Halton's boundaries and the connection between Peel Region and Halton Region at Steeles Avenue would be commissioned to supply Halton Region's existing infrastructure. In general, the agreement includes the following concepts:

- Peel Region controls, operates and maintains services and infrastructure ("Peel Works") within Peel's boundary;
- Halton Region would own and be responsible for oversight, capital, operations and maintenance of the connected infrastructure within Halton ("Halton Works");
- Additional connections may be considered with Peel Region's consent;
- There will be a separate agreement between benefiting property owners within the Premier Gateway Employment Area and Halton Region through Halton's development application process;
- Halton Region would enforce its Backflow Prevention requirements;
- Halton Region would be responsible for billing meter and measurements;
- Benefiting property owner would pay Halton Region directly and Halton Region to pay Peel Region;
- There will be a fixed duration to this agreement;
- There will be a limit on peak flow established/allocated and capacity is not unlimited;
- The existing Intermunicipal Adverse Water Quality Notification Protocol will be followed; and
- Peel Region would be made whole by Halton Region if there is an issue collecting the revenue for billed volume.

Recommended Option

As part of the Premier Gateway Phase 2B Infrastructure Implementation Plan, Halton Region will be constructing permanent water and wastewater infrastructure to service the subject lands.

An interim solution is needed to service Conestoga Cold Storage Limited's commercial development. Since Peel Region has constructed infrastructure in the area of the subject lands, entering into a water servicing agreement with Peel Region is a reasonable solution.

This recommendation is consistent with Section 89(17) of the Regional Official Plan which states that it is the policy of Halton Region to "consider and permit, based on individual merit and in accordance with goals and objectives of this Plan, new urban service system interconnections between the Region of Halton and abutting municipalities". Thus, Regional Council approval of cross border servicing is required.

Conclusion and Next Steps

Conestoga Cold Storage Limited, regarding the development of their lands located at 16316 Steeles Avenue, 16408 Steeles Avenue, PIN 250260130 (LT) and 7876 Tenth Line South, has requested water servicing from Halton Region. As Halton Region's infrastructure has not yet been constructed and there are significant challenges with the servicing of these lands with groundwater, an interim solution is required.

Peel Region has constructed an emergency water connection on Steeles Avenue to this area that is currently not used and as such, a framework to create a "Water Supply Agreement with the Region of Peel Servicing of Properties in the Premier Gateway Employment Area" was developed.

Upon Regional Council's approval of Report No. PW-25-23, staff will continue discussions with Peel Region to identify and secure the requisite permissions and agreements to connect municipal water services from Peel Region to the properties in the Premier Gateway Employment Area for Regional Council's approval. Staff will also continue to work with Conestoga Cold Storage Limited through the development application process to assist with satisfying requirements to proceed with the municipal water connection.

FINANCIAL/PROGRAM IMPLICATIONS

There are no financial implications at this time.

Respectfully submitted,



Lee Anne Jones
Director, Infrastructure Planning and Policy



Andrew Farr
Commissioner, Public Works

Approved by



Jane MacCaskill
Chief Administrative Officer

If you have any questions on the content of this report,
please contact:

Lee Anne Jones

Tel. # 7547

Attachments: Attachment #1 – Copy of Conestoga Cold Storage Limited's Letter to Halton Region
Attachment #2 – Locational Map of the Premier Gateway Employment Area

ATTACHMENT #1 TO REPORT NO. PW-25-23



March 6, 2023

Submitted Digitally

Ms. Lee Anne Jones
Director Infrastructure Planning & Policy
Halton Region
1151 Bronte Road
Oakville, ON
L6M 3L1

Dear Ms. Jones:

**Re: Peel Region Water Connection
Conestoga Cold Storage Limited
Town of Halton Hills**

As you are aware, Conestoga Cold Storage Limited ("CCS") is in the process of developing lands located at 16408 and 16316 Steeles Avenue and 7876 Tenth Line South in the Halton Hills Premier Gateway Employment Area for a cold storage warehouse facility and related office. CCS has put together a team of consultants to assist us and we are at a pivotal point where we must finalize crucial design aspects of the project. Specifically, a municipal water connection from Peel is critical for fire protection purposes.

It is our understanding that staff from Halton have had numerous discussions with our consulting team and that there have also been very fruitful discussions with Peel staff about the possibility of obtaining a municipal water connection on an interim basis through an existing connection on Steeles Avenue to service our site. We are appreciative of these efforts and would like to take these discussions to the next level.

Our consulting team is in the process of finalizing a design for infrastructure along Steeles Avenue to the CCS property and will be submitting the required Services Permit application shortly for connection to the Halton system. Our ask is one of timing and process. We would like to know the next steps and whether authorization from Regional Council is necessary to facilitate the Peel connection and if so, could you share with us the timing.

Any information you can provide us would be appreciated and again we are extremely grateful and appreciative of Halton's support and guidance to date. Please let us know if there is any information you need from CCS or our consulting team to bring this matter to a successful conclusion.

Sincerely,

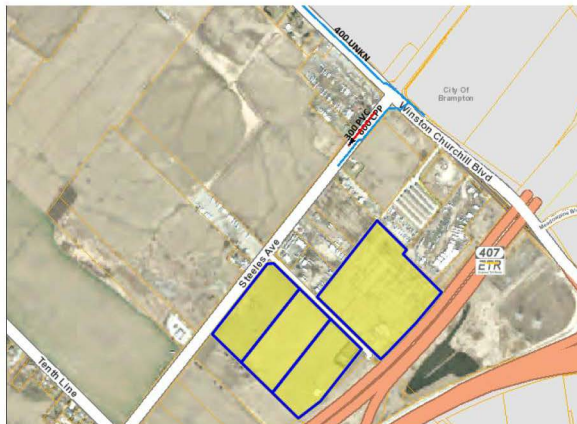
Gavin Sargeant, B.ASc.
Vice President, Conestoga Cold Storage Limited.

c. Bob Gray, Commissioner, Legislative & Planning Services and Corporate Counsel
Andrew Farr, Commissioner of Public Works

ATTACHMENT #2 TO REPORT NO. PW-25-23

Conestoga Cold Storage Owned Lands, Halton Hills

Attachment #2
to Report No. PW-25-23



1: 9,659

Information Valid as of: 6/8/2023

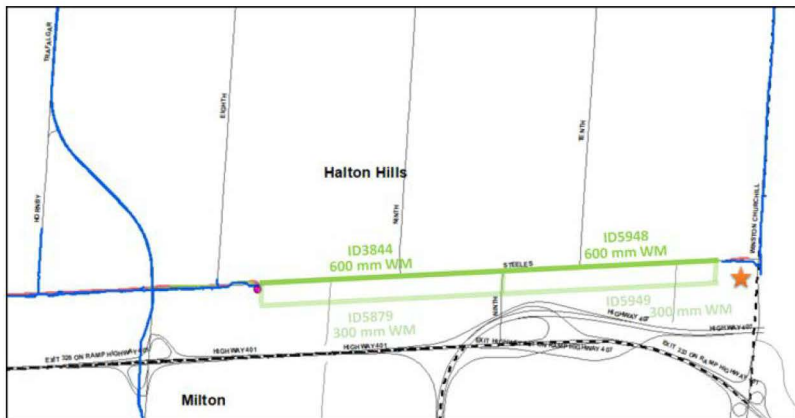
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Halton.ca 311



HALTON-PEEL CROSS-BOUNDARY WATER SERVICING AGREEMENT REPORT

REPORT TITLE: Halton-Peel Cross-Boundary Water Servicing Agreement

FROM: Kealy Dedman, Commissioner of Public Works

RECOMMENDATION

- 1. That the Commissioner of Public Works be authorized to negotiate, enter into and execute, a new agreement with The Regional Municipality of Halton (Halton Region), including any future amendments, renewals, extensions or termination thereof, on legal terms satisfactory to the Regional Solicitor and on business terms satisfactory to the Commissioner of Public Works, from time to time, based upon the principles set out in this report, to address the cross-boundary water servicing arrangements between Halton Region and The Regional Municipality of Peel (Peel Region); and**
- 2. That Halton Region be responsible for all connection charges and legal disbursements, including the applicable fees as set out in the Region's User Fees By-law 43-2002, as amended; and**
- 3. That Halton Region be added to Peel Region's water billing system; and**
- 4. That the required documents be executed by Peel Region's authorized signing officers.**

REPORT HIGHLIGHTS

- The Regional Municipality of Halton (Halton Region) Council, on June 14, 2023, endorsed Report Number PW-25-23 "Water Supply Agreement with the Regional Municipality of Peel for Temporary Servicing of Properties in the Premier Gateway Employment Area", agreeing to a temporary water connection to the Region's drinking water system to supply Conestoga Cold Storage, a large business with other locations in the Region as well as in the Town of Halton Hills.
 - Peel Region has sufficient in-term water system capacity for this temporary cross-boundary water servicing proposal; it conforms with land use policies in both municipalities and poses no risk to the Region's utility ratepayers.
 - Halton Region will be responsible for all cross-boundary water servicing requirements, including engineering costs, connection charges, construction costs, inspection costs, legal disbursements and applicable fees as set out in the Region's User Fees By-law 43-2002, as amended by By-law 5-2023.
-

Halton-Peel Cross-Boundary Water Servicing Agreement

DISCUSSION

1. Background

a) Water Servicing Request

Conestoga Cold Storage (CCS) is a large business located in Peel Region. CCS owns a property near Winston Churchill and Steeles Avenue in the Town of Halton Hills that CCS wishes to develop and build a second facility. Halton Region is planning to upgrade infrastructure services to the area by 2027/2028. Therefore, CCS contacted Peel Region and Halton Region in March 2023 to determine the feasibility of a temporary cross-boundary water servicing agreement utilizing existing water infrastructure at Steeles Avenue West and Winston Churchill Boulevard.

CCS is developing lands at 16408 Steeles Avenue, in the Town of Halton Hills, and proposes to fund the design and construction of a watermain from their property to an existing Peel Region-owned 400mm diameter watermain at the Halton-Peel municipal boundary. The new watermain may provide water service to additional industrial users, subject to development application approval from Halton Region and available water infrastructure capacity. Peel Region and Halton Region staff would collaborate on the design and construction to ensure the works meet required standards. Infrastructure within Halton Region will be owned and maintained by the Region of Halton at no cost to Peel Region.

2. Findings

a) Existing Water Infrastructure

Peel Region has an existing section of 400mm diameter watermain that crosses Winston Churchill Boulevard and terminates at a chamber at the municipal boundary (Appendix I). The chamber contains valves and a water meter. These works were constructed in 2011 as part of a Peel Region Transportation capital project (Project 04-4075) and funded by Halton Region to enable the supply of drinking water in the event of an emergency. To date, the works have not been used to transfer water from Peel Region to Halton Region and the section of main remains out of service at this time.

b) Capacity Assessment

Peel Region analyzed the water servicing requirements provided by Halton Region and the hydraulic modelling confirmed sufficient in-term capacity exists in Peel Region's water system to supply the needs of the subject lands without adversely affecting Peel Region's water system.

c) Future Halton Region Servicing

Halton Region's servicing is anticipated to be available in 2027-2028 when Halton Region completes the construction of a 600mm diameter watermain along Steeles Avenue, from Eighth Line to Winston Churchill Boulevard. The proposed cross-boundary water servicing agreement would expire once Halton Region's servicing becomes available. Halton Region has committed to take all reasonable efforts to expedite water servicing to the industrial area as soon as possible. Halton Region will decommission the cross-boundary water

Halton-Peel Cross-Boundary Water Servicing Agreement

connection, at no cost to Peel Region, in accordance with Peel Region's standards when the water supply from Peel Region is no longer necessary.

d) Intermunicipal Water and Wastewater Servicing Agreements

Peel Region has successfully executed and managed long-term intermunicipal water and wastewater servicing agreements with the Region of York and the City of Toronto. These agreements follow Regional Council Resolution 96-918 and Corporate Policy W10-07 Provision of Water and Wastewater Services to Adjacent Municipalities (Appendix II). The proposed cross-boundary water servicing agreement between Halton Region and Peel Region will adhere to this policy and include provisions that protect Peel Region's infrastructure, system capacity, drinking water quality, and utility rate payers.

Peel Region also currently has 14 active water accounts in the billing system for services to private users in adjacent municipalities (Halton and York Region), out of which eight are from Halton (two Institutional, Commercial and Industrial sector and six residential). These accounts are charged at the same rate as Peel users.

Halton Region Council, on June 14, 2023, endorsed Report Number PW-25-23 "Water Supply Agreement with the Regional Municipality of Peel for Temporary Servicing of Properties in the Premier Gateway Employment Area" (Appendix III), agreeing to a temporary cross-boundary water connection to Peel Region's drinking water system.

3. Proposed Direction

Staff recommend that Regional Council authorize the Commissioner of Public Works to negotiate, enter into, and execute an agreement, including any future amendments, renewals, extensions, or termination thereof, on legal terms satisfactory to the Regional Solicitor and on business terms satisfactory to the Commissioner of Public Works, based upon the principles set out in this report to address the cross-boundary water service arrangements between Halton Region and Peel Region.

Staff will collaborate and negotiate with Halton Region to develop and execute a temporary cross-boundary water servicing agreement.

RISK CONSIDERATIONS

On May 18, 2023, the Province introduced Bill 112, the *Hazel McCallion Act (Peel Dissolution)*, 2023 that, if passed, will dissolve the Region of Peel and make the Cities of Brampton and Mississauga and the Town of Caledon single-tier municipalities, effective January 1, 2025. The proposed legislation provides for the establishment of a Transition Board to make recommendations on implementing the restructuring. Details of the transition including matters as they relate to Regional roles and responsibilities are not known at this time and are to be addressed in future reporting to Regional Council. Additional assessment to support the transition under Bill 112 will be provided as further details become known.

The proposed cross-boundary water servicing agreement consumes some in-term available capacity in Peel Region's water system. However, hydraulic modelling analysis confirms there is no in-term adverse impact to Peel Region's water system. This is a short-term agreement and Peel Region will ultimately recover this capacity once Halton Region servicing becomes available in less than five years.

Halton-Peel Cross-Boundary Water Servicing Agreement

The temporary water servicing enables Conestoga Cold Storage to operate earlier and thus, provide employment opportunities.

FINANCIAL IMPLICATIONS

Halton Region will be responsible for all cross-boundary water servicing requirements, including engineering costs, connection charges, construction costs, inspection costs, legal disbursements and applicable fees as set out in Schedule A of the User Fees By-law 43-2002, as amended by By-law 5-2023, including the Cross Boundary Servicing Application Fee and engineering and inspection fees.

Halton Region will be charged the Peel Region 2023 water rate (\$1.8672 per cubic meter), as adjusted and effective annually, until the cross-boundary agreement expires. The annual consumption is anticipated to provide Peel Region with approximately \$1 million or more in additional annual revenue.

CONCLUSION

This proposed temporary cross-boundary water servicing agreement between Peel Region and Halton Region supports a large Peel business owner at no risk to Peel's water quality, Peel Region utility rate payers or current and future water system capacity for development.

APPENDICES

- Appendix I – Proposed Peel-Halton Water Servicing Infrastructure Map
- Appendix II – Corporate Policy W10-07 Provision of Water and Wastewater Services to Adjacent Municipalities
- Appendix III – Halton Region Council Endorsement of Cross-Boundary Agreement



Kealy Dedman, Commissioner of Public Works

Authored By: Stefan Ulman, Project Manager, Contract Administration and Oversight

TOWN OF HALTON HILLS: WATER AND WASTEWATER AREA SERVICING PLAN FOR THE PREMIER GATEWAY PHASE 2B EMPLOYMENT AREA- FINAL REPORT



Water and Wastewater Area Servicing Plan For the Premier Gateway Phase 2B Employment Area Final Report

Prepared by

GM BluePlan for:

Town of Halton Hills

Project No. 717029

September 2023



Guelph | Owen Sound | Listowel | Kitchener | London | Hamilton | GTA

Royal Centre, 3300 Highway No. 7, Suite 402, Vaughan, ON L4K 4M3 P: 416-703-0667
www.GMBluePlan.ca



PEOPLE | ENGINEERING | ENVIRONMENTS

September 11, 2023
Our File: 720010

Macaulay Shiomi Howson Ltd
600 Annette Street
Toronto, Ontario M6S 2C4

Attention: Lorelei Jones, MCIP, RPP
Principal

Re: Condition Assessment and Capital Plan Recommendations Final Report

Dear Ms. Jones,

GM BluePlan Engineering Limited is pleased to submit this Revised Water and Wastewater Area Servicing Plan for the Town of Halton Hills Premier Gateway Phase 2B Employment Area. This final report addresses Town of Halton Hills and Halton Region comments on the November 2021 version and September 2022 version of the draft report (including comments received at the February 7, 2023 meeting with the Town and Halton Region to review alignment of the Area Servicing Plan with the Region's planned work within the area) and Region comments on the March 2, 2023 Final Draft for Review Report.

If you have any questions, or require any additional information, please contact the undersigned.

Yours truly,

GM BLUEPLAN ENGINEERING LIMITED

A handwritten signature in black ink, appearing to read 'Matthew Fisher', is written over a light blue horizontal line.

Per: Matthew Fisher, P.Eng.
Infrastructure Planning

GUELPH | OWEN SOUND | LISTOWEL | KITCHENER | LONDON | HAMILTON | GTA

3300 Highway 7, Suite 402, Vaughan, ON L4K 4M3

VERSION LOG

Version	Date	Author(s)	Description
1	November 24, 2021	Matthew Fisher	Draft for Review
2	September 15, 2022	Matthew Fisher	Revised Draft for Review
3	March 2, 2023	Matthew Fisher	Final Draft for Review
4	September 11, 2023	Matthew Fisher	Final

Executive Summary

GM BluePlan have been retained to complete the water and wastewater Area Servicing Plan (ASP) for the Premier Gateway Phase 2B Employment Area (PGEA P2B) Secondary Plan. The PGEA is an important designated employment area in the Town of Halton Hills located in the western GTA/Highway 401 corridor. The ASP study area is comprised of the Phase 2B area of the PGEA and includes the lands within the Urban Area located north of Steeles Avenue between Eighth Line and Winston Churchill Boulevard.

The Water and Wastewater ASP for the PGEA P2B has developed a comprehensive servicing strategy that builds on previous studies, can be cost-efficiently constructed, effectively phased to support the growth needs of the Town of Halton Hills and provides flexibility to be incorporated into Halton Region's approved and updated servicing plans for the area. The Water and Wastewater ASP for the PGEA P2B includes consideration for the Region's ongoing work to refine the servicing strategy to service the PGEA P2B.

Proposed water servicing is summarized in Figure ES-1 and proposed wastewater servicing is summarized in Figure ES-2.

The PGEA P2B ASP confirms that the planned Employment Area can ultimately be serviced by the Region's proposed upgrades to water and wastewater linear infrastructure along Steeles Avenue, Ninth Line and Tenth Line and crossing the GTA West Corridor.

The Region's planned water and wastewater projects for the area are being considered further as part of studies supporting the Regional Municipal Comprehensive Review, including an ongoing Premier Gateway Phase 2B Wastewater Servicing Strategy Feasibility Study and Provisional Municipal Class Environmental Assessment (EA) that is scheduled to be completed ahead of completion of the Region's ongoing Water, Wastewater and Transportation Master Plan Update project. It is anticipated that in order for the Town to supply area employment lands to meet their 2031 planning targets, adoption of the Secondary Plan ahead of completion of the Region's Master Plan Update will be required, and the Area Servicing Plan has been completed to meet the development timing requirements of the Town and provide for flexibility for recommended water and wastewater servicing to be effectively incorporated into the Region's ongoing studies.



Water & Wastewater Area Servicing Plan for the Premier Gateway Phase 2B

Water Infrastructure

- ▲ Pumping Station (PS)
- Storage
- Treatment
- Feeder Main (Diameter ≥ 400mm)
- Local Watermain (Diameter < 400mm)

General Features

- ▭ Premier Gateway Phase 2B
- Highway
- Major Road
- Local Road
- Property Parcel
- ▭ Municipal Boundary
- ▭ Preferred Route and Interchange Locations for GTA West Transportation Corridor Study

Preferred Land Use

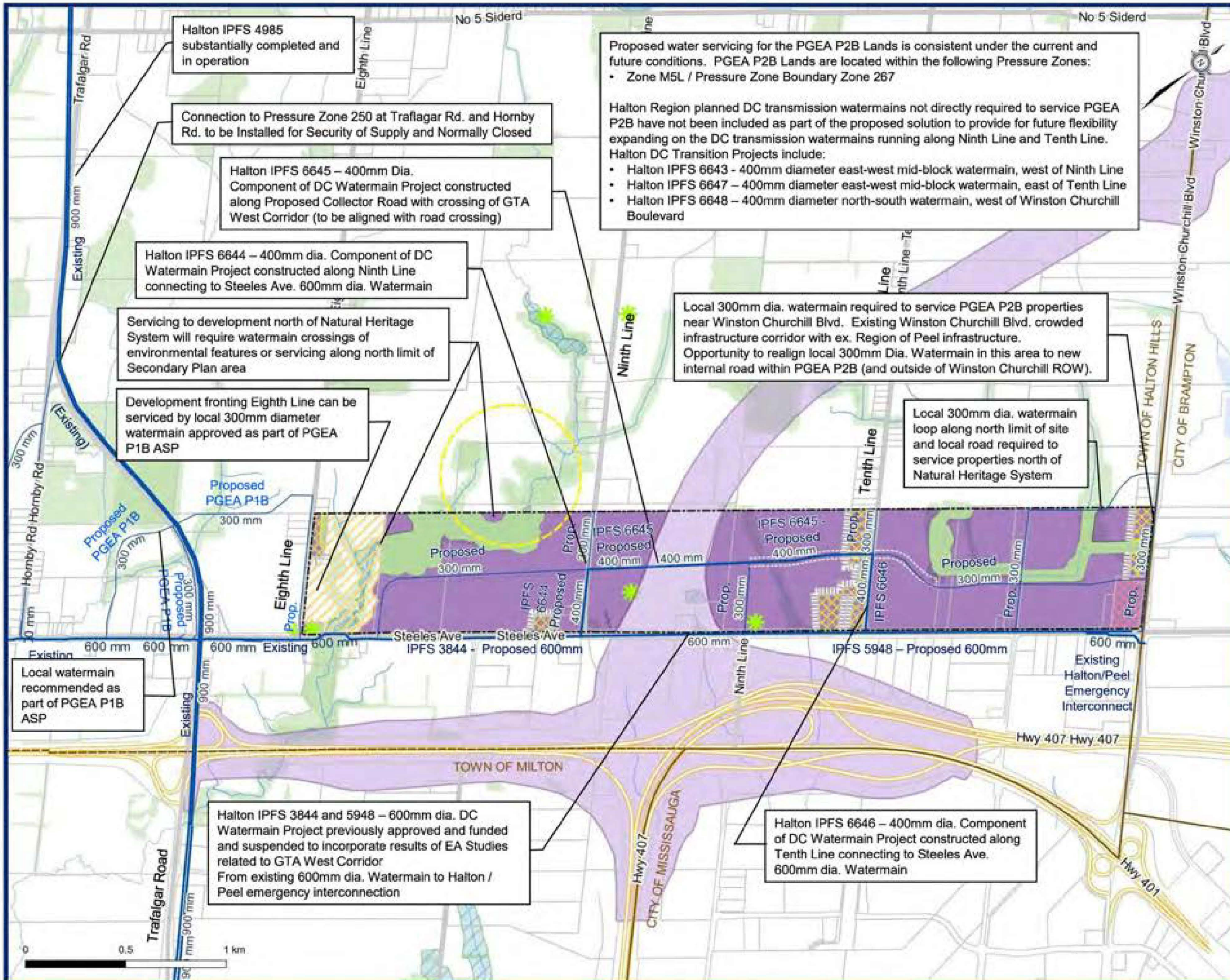
- Natural Heritage System
- Prestige Industrial Area
- Residential Special Policy Area
- Supportive Commercial
- Subject to D09OPA22.001 As Per PL180499
- Headwater Drainage Feature
- Buffer for Existing Residential Uses
- ★ Cultural Heritage Resource
- ▭ Minimum Distance Separation

Figure ES-1

Proposed Water Servicing Solution for PGEA P2B



September 2023
720010-004
Project: EPSO 35917



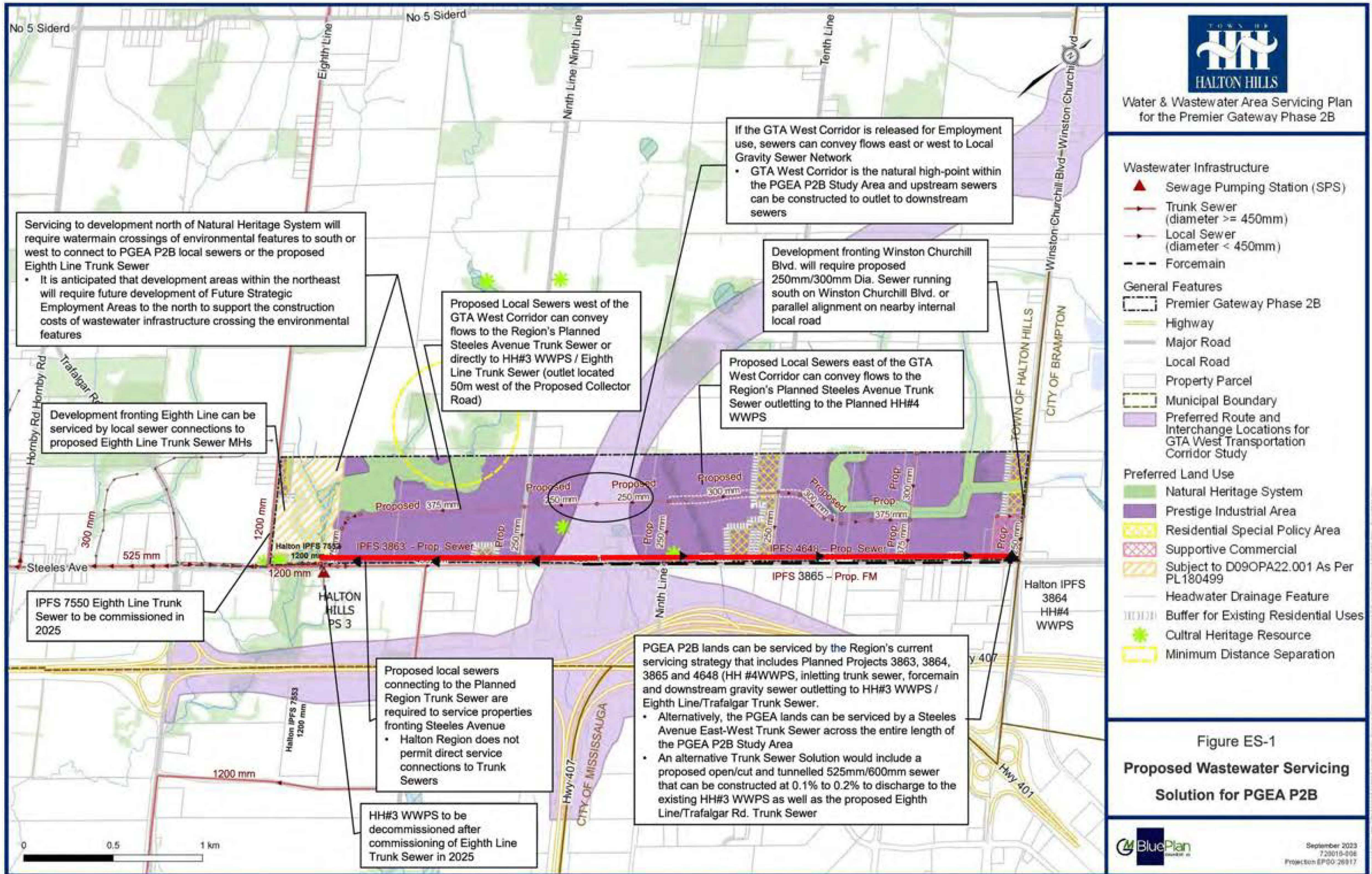


Figure ES-1
Proposed Wastewater Servicing
Solution for PGEA P2B

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Appendices

Appendix A	Halton Region Future Water Pressure Zones, Wastewater Drainage Areas and Water and Wastewater Capital Implementation Plans (2017-2031)
Appendix B	Proposed Wastewater Servicing Plan and Profile Drawings
Appendix C	Drainage Plans and Sanitary Design Sheets
Appendix D	Cost Estimates

1 Introduction

GM BluePlan have been retained to complete the water and wastewater Area Servicing Plan (ASP) for the Premier Gateway Phase 2B Employment Area (PGEA P2B) Secondary Plan. GMBP are part of the project team lead by Macaulay Shiomi Howson Ltd (MSH) to develop the Secondary Plan for the Town of Halton Hills.

The Water and Wastewater ASP for the PGEA P2B will identify and evaluate water and wastewater servicing alternatives and recommend a servicing solution. The Water and Wastewater ASP will support the Premier Gateway Employment Area (PGEA), which is designated as an urban area, a natural heritage system as well as an Employment Area in the Town of Halton Hills and Halton Region Official Plans.

The PGEA will serve as a key employment growth area including industrial, office, commercial and institutional services. The completion of this Water and Wastewater ASP for the PGEA P2B is a critical step in the development of a key employment area by Halton Region and the Town of Halton Hills.

The key objectives of this Water and Wastewater ASP are to:

- Develop a comprehensive servicing strategy to meet the requirements of PGEA P2B that can be cost-effectively constructed.
- Provide a defensible framework and implementation plan for servicing of the PGEA P2B.
- Provide justification and recommendations for timing and phasing of new Regional and local infrastructure.
- Build on previous studies and create a forward-looking document to support the Town of Halton Hills that aligns with infrastructure planning across Halton Region.

1.1 Proposed Development

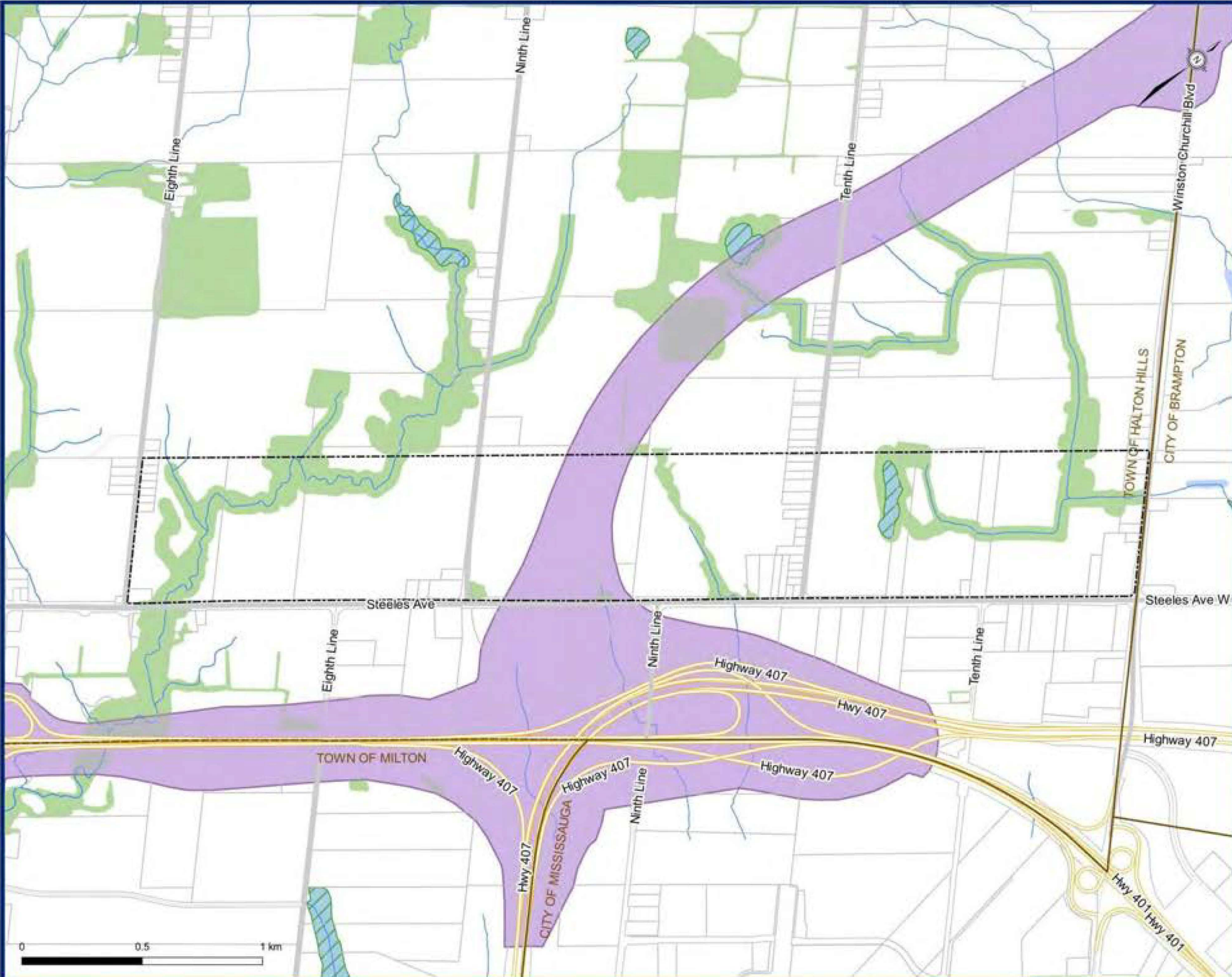
The PGEA is an important designated employment area in the Town of Halton Hills located in the western GTA/Highway 401 corridor. The PGEA is located along Steeles Avenue north of Highway 401, west of Winston Churchill Blvd, and east of Esquesing Line. The PGEA consists of four distinct Phases: 1A, 1B, 2A and 2B. The Area Servicing Plan will focus on Phase 2B of the PGEA.

The ASP study area is comprised of the Phase 2B area of the PGEA and includes the lands within the Urban Area located north of Steeles Avenue between Eighth Line and Winston Churchill Boulevard.

The study area is shown in Figure 1-1.



Water & Wastewater Area Servicing Plan
for the Premier Gateway Phase 2B



Premier Gateway Phase 2B

- General Features
- Highway
 - Major Road
 - Local Road
 - Property Parcel
 - Municipal Boundary
 - Preferred Route and Interchange Locations for GTA West Transportation Corridor Study

- Environmental Features
- Water Course
 - Water Body
 - Natural Heritage System
 - Wetland

Figure 1-1
Study Area



1.2 Timing and Phasing

The PGEA 2B area is the second phase of employment lands to the 2031 planning horizon. It is anticipated that development of areas will occur concurrently with the required planning processes (zoning by-law amendments with supporting functional servicing plans, etc.). The Secondary Plan project aims to supply employment lands to accommodate employment growth within the area to 2031. Proposed water and wastewater infrastructure will be coordinated together with stormwater infrastructure and road improvements recommended as part of the Town of Halton Hills' PGEA P2B Secondary Plan.

Halton Region's Region-wide Integrated Water, Wastewater and Transportation Master Plan Update commenced in March 2022. At the time of submission of this revision of the Area Servicing Plan, the ongoing Master Plan has been progressed to the background and existing conditions review. Planning numbers have not yet been made available for the Master Plan work.

The Area Servicing Plan has been completed to meet the development timing requirements of the Town (in order to meet the Town's 2031 growth targets) and provide for flexibility for recommended water and wastewater servicing to be effectively incorporated into the Region's Master Plan Update. It is understood that the Town will require the adoption of the Secondary Plan ahead of completion of the Region's Master Plan Update in order to supply employment lands to accommodate growth within the area to 2031.

1.3 Interim Servicing

This Report has been prepared to provide the Town of Halton Hills and Halton Region with a proposed plan for the water and wastewater servicing of the PGEA P2B area. The primary objective of the analysis is the provision of servicing across the entire secondary plan area that aligns with the respective Secondary Plan's planning horizon.

The analysis has considered existing Halton Region water and wastewater infrastructure, previously planned (and funded) Halton Region infrastructure, ongoing studies and review of Region trunk infrastructure to service the area (as well as anticipated timing for Halton Region's planned area infrastructure, including the Ultimate Water Pressure Zone Boundary Realignment, construction and commissioning of the Eighth Line Trunk Sewer and Region's PGEA P2B Wastewater Servicing Strategy Feasibility Study and Provisional Municipal Class EA).

This Report references the approved Region Master Plan projects with consideration for interim servicing to allow for planned development to proceed ahead of commissioning of planned Region infrastructure for the area. Interim servicing recommendations have been included to support detailed servicing designs to be undertaken as part of future Zoning By-law Amendments and Draft Plans of Subdivision.

1.4 Organization of Report

The ASP Report documents the comprehensive process undertaken to develop and recommend a proposed water and wastewater servicing strategy for the PGEA P2B Study Area. The Report is organized as follows:

- **Section 1 – Introduction**

An introduction to the study, description of study area, study purpose and objectives, and the report outline.

- **Section 2 – Background Study Context**

Provides the background plans, related studies, legislative and policy planning context, water and wastewater servicing principles and policies relevant to the PGEA P2B Water and Wastewater ASP.

- **Section 3 – Land Use and Best Planning Estimates**

Outlines the existing land use and environmental conditions, future planned land use, and population and employment growth forecasts for the PGEA P2B area.

- **Section 4 – Water**

Baseline description of the existing water system, estimated water demands, assessment of existing infrastructure capacity and development of servicing strategies.

- **Section 5 – Wastewater**

Baseline description of the existing wastewater system, estimated wastewater flows, assessment of existing infrastructure capacity and development of servicing strategies.

- **Section 6 – Phasing, Timing and Cost Estimate**

Identifies the phasing / timing and cost estimate of capital projects to service the PGEA P2B area, taking into consideration the system-wide needs.

- **Section 7 – Conclusion**

Summarizes the servicing solution for the study area and lists the capital upgrades and improvements recommended.

2 Relevant Documents and Studies

2.1 Town of Halton Hills

2.1.1 Town of Halton Hills Official Plan

The Town of Halton Hills Official Plan (OP) provides policies related to the Town of Halton Hills' growth and development through to the year 2031. The OP relates to all lands within the Town of Halton Hills.

According to the OP, the PGEA is divided into six land designations: prestige industrial area, gateway area, green lands, major parks and open space area, private open space area, and Employment.

All development shall proceed based on full municipal services. Halton Region is responsible for the extension of municipal water and wastewater services.

2.2 Halton Region

2.2.1 Halton Region Official Plan (2016)

The Halton Region Official Plan (OP) provides policies for Halton Region and all its municipalities including the Town of Halton Hills. The OP also includes strategies and objectives related to Regional growth and development through to the year 2031.

The Phase 2B study area is designated as an *Employment Area* within the Urban Area. The OP defines an employment area as:

"...areas designated for clusters of business and economic activities including, but not limited to, manufacturing, warehousing, offices and associated retails and ancillary facilities".

The OP also identifies the area north of the PGEA lands for Future Strategic Employment Area. The Future Strategic Employment Area is not a land use designation but represent lands that are strategically located near major transportation facilities and existing Employment Areas and are best suited for employment beyond the planning horizon of the current OP. Sizing of the proposed water and wastewater infrastructure in the PGEA P2B ASP does not include capacity allocation for the future strategic employment area.

Regional Official Plan Amendment (ROPA) 43 – HPBATS/GTA West Corridor Protection identified a corridor protection area to be protected for the *Halton Peel Boundary Area Transportation Study / Greater Toronto Area West Corridor Study Area* through the Town of Halton Hills and Town of Milton until the completion of the GTA West Corridor Environmental Assessment study. The area protected is generally bounded by Winston Churchill Boulevard to the east, No. 10 Side Road to the north, Eight Line to the west and Steeles Avenue to the south.

In 2017, the previous Provincial government announced the suspension of the GTA West Study and the re-evaluation of the project to consider additional transportation options for the corridor such as utilities, transit or other transportation alternatives, and released a refined corridor which partially affected the Premier Gateway Phase 2B Lands.

In June 2019, the current Provincial government announced that it would resume the GTA West Environmental Assessment. In September 2019, the draft Technically Preferred Route (TPR) was presented and the MTO stated that they have reduced interest in areas outside the draft Focused Analysis Area (FAA). The FAA continues to be refined through the Class EA Process. MTO issued an updated 2020 FAA based on review and feedback following the presentation of the

2019 FAA at PIC #2. Much of the east and west portions of the Study Area now fall within the Reduced Interest Area; with the FAA through the Study Area refined to an area between Ninth and Tenth Line.

For properties within the MTO's Reduced Interest Area, applications can proceed through municipal development processes. MTO will continue to review all development applications in the GTA West Study Area (which includes the entire Phase 2B lands). It is anticipated that applications in the MTO's Reduced Interest Area will not be impacted by the GTA West Transportation Corridor.

The GTA West Project Team aims to further reduce the FAA when the preliminary design of the Preferred Route is presented at PIC #3.

2.2.2 Halton Regional Council Motions Related to the GTA West Corridor

On November 20, 2019, Regional Council endorsed a motion on the GTA West Environmental Assessment Study:

"THAT the Region of Halton Council opposes further investment by the Province in the GTA West Transportation Corridor".

Further, on March 24, 2021, Halton Region Council passed a Motion regarding the Designation Request for the Proposed GTA West Project Under the Impact Assessment Act:

"THAT further to the letter, The Regional Municipality of Halton reaffirms its opposition to the GTA West project and its commitment to protecting and preserving the natural environment and its work to mitigate the impacts of climate change,

AND FURTHER THAT The Regional Municipality of Halton hereby reiterates its request as set out in the March 3, 2021 letter and forwards a copy of that letter and this resolution to the Minister of Environment and Climate Change Canada Wilkinson urging designation of the GTA West project for an Impact Assessment under the Impact Assessment Act.

AND FURTHER THAT this resolution be circulated to the Prime Minister of Canada, Halton's MPs, Premier of Ontario, Ontario Minister of Transportation, Ontario Minister of Environment, Conservation and Parks, Halton's MPPs, City of Burlington, Towns of Halton Hills, Milton and Oakville."

Further, on May 3, 2021, The Minister of Environment and Climate Change determined that the GTA West Project proposed by the Ontario Ministry of Transportation warrants designation under the Impact Assessment Act (Federal assessment).

2.2.3 Sustainable Halton Water and Wastewater Master Plan (2011)

In 2011, Halton Region completed the Sustainable Halton Water and Wastewater Master Plan (SHWWMP) to support Regional implementation of the Official Plan Amendment (ROPA 38/39) based on Halton Region's Bests Planning Estimates (June 2011). The Master Plan provided a Region-wide water and wastewater servicing strategy to accommodate growth from 2011 to 2031.

Halton Region, with support from local municipalities, updated their planning data to 2031 as part of the Master Planning process.

The key water servicing components for the Milton/Halton Hills 401 Employment Corridor are:

- Serviced by Zone M5L located along Steeles Avenue

- Water supply is lake based. Pumping stations pump the water north to Milton/Halton Hills 401 Corridor

Servicing of the PGEA is reliant on the following water and wastewater capital projects identified in Halton Region's Development Capital Plan (as outlined in the 2017 Development Charges (DC) Update Technical Report (detailed below)).

Key water servicing components for the broader Milton/Halton Hills 401 Employment Corridor include:

- Infrastructure upgrades maximizing use of existing capacity;
- New Zone 4/5 boundary;
- Second spine up Trafalgar Road alignment and third spine along Neyagawa Boulevard;
- Burloak WPP and Oakville Water Purification Plant (WPP) water supply capacity expansion; and,
- Integration of Zone 5 infrastructure providing Milton supply security.

The following Sustainable Halton servicing components have been removed from the Region's capital program:

- Addition of Zone 5 Pumping Station (at Zone 4 Reservoir) and transmission for additional feed to 401 Corridor.

Key wastewater servicing components for the broader Milton/Halton Hills 401 Employment Corridor include:

- Additional capacity at Mid-Halton Wastewater Treatment Plant (WWTP);
- Utilization of the two (2) existing wastewater pumping stations located along Steeles Avenue to minimize sewer depth and transfer flows along Steeles Avenue to the existing Milton gravity system to the south.
- Eastern area will continue to pump wastewater flows to existing infrastructure to the west;
- Diversion of flows to the future Eighth Line/Trafalgar Trunk Sewer.

The Master Plan was generally an Approach 2 style Master Plan – with the Master Plan document being completed at the conclusion of Phase 1 and Phase 2 of the Municipal Class Environmental Assessment (EA) process. The Approach 2 Master Plan provided for all recommended Schedule A, A+ and B projects to move forward to implementation. (The final public notice for the Master Plan can serve as the Notice of Completion for the Schedule B projects within it).

2.2.4 2017 Water & Wastewater Development Charges Update

The 2017 DC Update Water and Wastewater Technical Report was completed in September 2016 to update the 2012 DCs and includes a number of technical updates to the SHWWMP and its associated Capital Implementation Plan. The report provides the basis for developing costs and capital implementation timing of water and wastewater projects required to service population and employment growth across Halton Region from 2017 to 2031 using 2011 Best Planning Estimates (BPEs).

The following summarizes the water and wastewater servicing recommendations made under the 2017 DC Update that are relevant to the PGEA P2B study area:

Water Servicing Recommendations

- Realignment of water pressure zone boundaries in the Town of Milton and the Town of Oakville (Zones 3, 4, and 5) to optimize customer water pressure in these areas.

Previously Approved and Funded Projects, Constructed and Commissioned

- 600 mm diameter Zone M5L watermain on Steeles Avenue from Trafalgar Road to East of Ninth Line (Region IPFS ID 3844)
- Zone 4 (Future Zone 250) Twin 900mm diameter trunk watermain along Trafalgar Road from Britannia Road to new Zone 4 (Existing Zone M4L / Future Zone 250) Reservoir (SH Halton Region IPFS ID 4985)

Previously Approved and Funded Projects, Not Yet Constructed

- 600mm diameter Zone M5L watermain on Steeles Avenue from East of Ninth Line to Peel Interregional Connection at Winston Churchill Boulevard (Region IPFS ID 5948)

Significant Water Projects (2017-2031):

- Oakville/Milton Water Pressure Zone Realignment (Zones 3, 4, 5) and alterations to Neyagawa, Fourth Line and Eighth Line Pumping Stations (Region IPFS IDs 7509, 7513, 7514)
- 400mm diameter watermain along Hornby Road (Zone M5L / Future Zone 250) (Region IPFS ID 6641)
- 400mm diameter watermain from Hornby Road to Trafalgar Road (Zone M5L / Future Zone 250) (Region IPFS ID 6642)
- 400mm diameter watermain from Trafalgar Road to approximately 400m east of Eight Line (Zone M5L / Future Zone 250) (Region IPFS ID 6643)
- 400mm diameter watermain from Steeles Avenue to approximately 300m north (Zone M5L / Future Zone 250) (Region IPFS ID 6644)
- Future Zone 250 400mm diameter watermain in the 401 growth corridor north of Steeles Avenue from 1,000 metres west of Ninth Line to 900 metres east of Ninth Line (Region IPFS ID 6645)
- Future Zone 250 400mm diameter watermain in the 401 growth corridor from Steeles Avenue to approximately 330 metres north of Steeles Avenue (Region IPFS ID 6646)
- Future Zone 250 400mm watermain in the 401 growth corridor north of Steeles Avenue from 600 metres west of Tenth Line to 1,000 metres east of Tenth Line (Region IPFS ID 6647)
- Future Zone 250 400mm diameter watermain in the 401 growth corridor from Steeles Avenue to 340 metres north of Steeles Avenue (Region IPFS ID 6648)

Wastewater Servicing Recommendations

Previously Approved and Funded Projects, Currently Under Construction

- Georgetown Eighth Line/Trafalgar Trunk Sewer (Region IPFS ID 6569/7550, 6572/7552, 6573/7553, 6574/7554, 6575/7555, 6576/7529, 6577/7530)

Previously Approved and Funded Projects, Not Yet Constructed

- Wastewater main on Steeles Avenue from West of Ninth Line to Halton Hills (HH) #3 Wastewater Pumping Station (WWPS) – In the future, HH#3 WWPS will be decommissioned and the WWM will be transferred to Region IPFS ID 7553 – Eighth Line Trunk Sewer) (Region IPFS ID 3863)
- Halton Hills (HH) #4 WWPS at intersection of Steeles Avenue and Winston Churchill Boulevard (Region IPFS ID 3864)
- Wastewater forcemain on Steeles Avenue from HH #4 WWPS to wastewater main on Steeles Avenue, east of Ninth Line (Region IPFS ID 3865)
- Wastewater main on Steeles Avenue from East of Ninth Line to Winston Churchill Boulevard (outletting to HH#4 WWPS) (Region IPFS ID 4648)

Significant Wastewater Projects (2017-2031):

- Decommissioning of Halton Hills #3 WWPS and connection to new Eighth Line trunk sewer and conversion of site to septage receiving facility (Region IPFS ID 6508)

2.2.5 Ongoing Halton Region Servicing Studies

Halton Region is currently completing projects that will support the future update of the Region-wide water and wastewater servicing strategies (including the Premier Gateway Employment Area Phase 2B Wastewater Servicing Strategy Feasibility Study and Provisional Municipal Class EA. The work is ongoing and final recommendations will consider the update of the water and wastewater servicing strategies for the Town's PGEAs that will better align with the anticipated development timing and Town growth needs/targets for the area. The Region's feasibility study is anticipated to be completed in Summer 2023 – ahead of the Region-wide Master Plan Update.

Until the studies are issued as final, servicing of the Secondary Plan area will consider the approved servicing strategy and projects included in the Sustainable Halton Water & Wastewater Master Plan and 2017 Water & Wastewater Development Charges Update.

Servicing requirements outlined in this Area Servicing Plan will be considered as part of the Region's updated servicing strategy for the area.

2.2.5.1 Wastewater Pumping Station Servicing Strategy Update

Halton Region is currently undertaking a Wastewater Pumping Station Servicing Strategy Update. The outcomes of this project will ultimately be incorporated in Halton Region's future infrastructure planning studies such as the ongoing Water and Wastewater Master Plan Update. The study primarily considers opportunities for WWPS related capital projects that will result in lower life-cycle costs and reduced energy consumption.

The study is considering the planned Halton Hills #4 WWPS and finalized recommendations from the Wastewater Pumping Station Servicing Strategy Update will be incorporated into the Region's future Master Plan Update project.

2.2.5.2 Halton Region Integrated Growth Management Strategy

Halton Region is currently completing an Integrated Growth Management Strategy (IGMS) Study as part of the Regional Municipal Comprehensive Review (including Halton's Official Plan review). The Study is currently considering alternative planning scenarios with focus on growth in alternative areas across Halton Region.

Growth management within the Future Strategic Employment Areas north of HH PGEA P2B lands and generally surrounding the GTA West Transportation Corridor Preferred Route is included as part of the Study. Various densification and greenfield expansion scenarios are being considered as part of the Study's four growth concepts. Servicing potential is a primary consideration for the Growth Concepts Employment Areas.

GMBP is supporting Halton Region's IGMS study, undertaking the review of water and wastewater servicing requirements. GMBP have completed a review of the water and wastewater servicing opportunities and constraints for alternative Growth Concepts, including review of impacts of projected growth to 2041 and 2051 on existing and approved future water and wastewater infrastructure; and high-level servicing needs to meet 2041 and 2051 growth.

At this stage of the Study, a preferred growth concept has been developed. GM BluePlan has identified servicing needs based on the preferred growth concept to meet 2041 and 2051 growth and completed a cost analysis of potential water and wastewater capital improvements. Servicing requirements to support the preferred growth concept will be updated as the Strategy is finalized, and the final recommendations will support Halton Region's Official Plan review and the supporting servicing studies will support the Region's future Master Plan update project.

2.2.5.3 2022 Water and Wastewater Development Charges Update

Halton Region is currently completing the background studies in support of the 2022 Development Charges By-Law. The Water and Wastewater Background Study in support of the 2022 DC Update is being completed by GM BluePlan, based on the Sustainable Halton Water and Wastewater Master Plan and has updated cost estimates and capital implementation timing for water and wastewater projects to service Halton Region growth from 2023 to 2031.

As part of the 2022 DC Update work, the 2012 and 2017 Water and Wastewater DC Background Studies were reviewed with focus on the following key elements:

- Re-assessing existing and future water and wastewater system capacities;
- Comparing actual growth uptake with planned theoretical growth projections;
- Identifying opportunities to further optimize water and wastewater system infrastructure; and,
- Validating the long-range Water and Wastewater Development Capital Implementation Plan to 2031 (i.e. project scope, timing, and cost) as identified in the 2011 Master Plan and refined through the 2017 DC Update Technical Report.

The Final 2022 Development Charges Update Water/Wastewater Technical Report was submitted to Halton Region in September 2021.

3 Land Use and Planning Projections

3.1 Land Use

3.1.1 Existing

The existing land use within the PGEA P2B study area currently consists of largely vacant lands, agricultural lands and a few areas of commercial and residential uses. Most commercial properties are located along Steeles Avenue, while residential rural areas are located along Steeles Avenue, Eighth Line, Ninth Line, Tenth Line and Winston Churchill.

3.1.2 Future

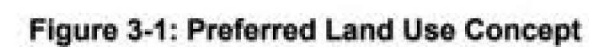
The objective of the PGEA is to ensure the availability of land to accommodate projected employment growth and support the Town of Halton Hills' and Halton Region's economy. The PGEA contains areas that are designated as Prestige Industrial Area with the intention to form an economically competitive and attractive employment area. PGEA P2B has been identified as a Provincially Significant Employment Zone.

The preferred land use concept is shown in Figure 3-1.

The permitted uses within this area will be limited to mainly employment such as industrial uses, business and professional offices, and some other facilities that do not cause or are not likely to cause air pollution, offensive odours, ground or water pollution, or noise in excess of current regulations.

The Supportive Commercial Area located northwest of the intersection of Steeles Avenue West and Winston Churchill Boulevard is anticipated to include facilities such as restaurants, gyms, etc.

The Residential Special Policy Areas represent the existing concentration of rural residential developments, which are unlikely to redevelop in the short term for employment uses.



3.2 Planning Estimates and Growth Assumptions

3.2.1 Best Planning Estimates (BPEs)

Halton Region Best Planning Estimates (BPEs) Data from June 2011 are generally used to determine the current and future water and wastewater servicing needs in Halton Region. This data is geographically distributed by Traffic Survey Zone (TSZ) and Small Geographic Units (SGUs) and contains approved population and employment projections for Halton Region up to the year 2031 consistent with Halton Region's Official Plan.

Figure 3-2 shows the SGUs associated with the PGEA P2B Water and Wastewater Area Servicing Plan.

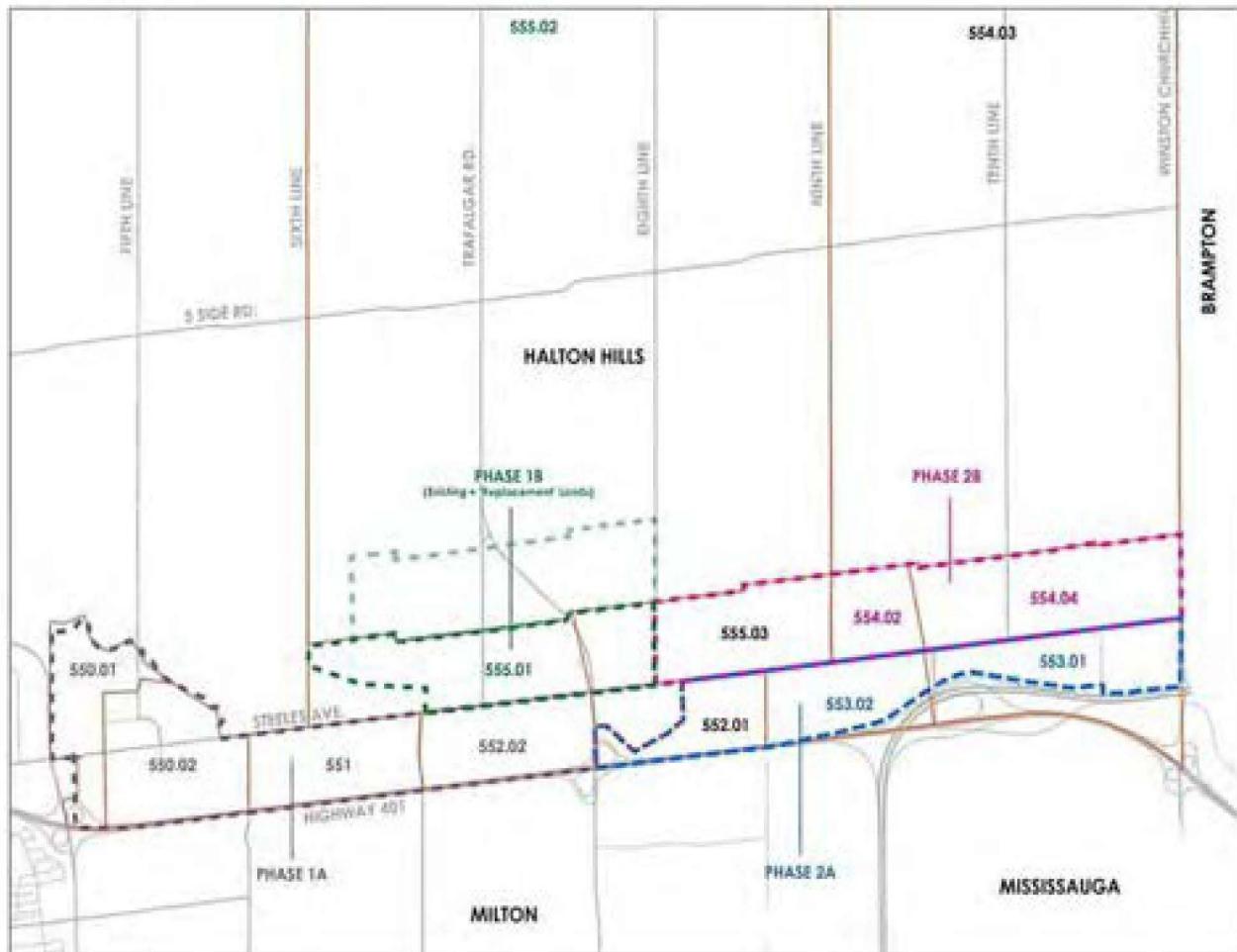


Figure 3-2: Premier Gateway Employment Area Phases & SGU Boundaries

3.2.2 Premier Gateway Employment Area Phase 2B Secondary Plan Area - Growth Assumptions

For the P2B lands located within the Urban Area, the planning forecasts provided by Halton Region were based on the 2011 Best Planning Estimates (BPEs) consistent with the SHWWMP. The PGEA P2B SGU growth projections are summarized in Table 3-1.

Table 3-1: PGEA P2B Secondary Plan Employment Growth Projections (Halton Region)

PGEA P2B SGU	2031 Projections				
	Res	Com	Ind	Ins	Total Employment
554.02	0	189	857	29	1,075
554.04	99	480	2,056	338	2,874
555.03	41	332	1,289	45	1,665
Total	140	1,001	4,202	412	5,614

Growth projections for the area were updated in support of the PGEA P1B ASP completed by Halton Region. The PGEA P1B ASP growth projections considered the impact of the potential GTA West Highway Corridor and the reallocation of growth from SGU 554.02 (where the potential GTA West Corridor will intersect) to replacement lands within SGU 555.01 located within the P1B lands.

3.2.3 Town of Halton Hills Projections

Employment targets for the P2B lands continue to be developed as part of the Secondary Plan. The P2B lands were previously frozen for development as the Greater Toronto Area (GTA) West Class Environmental Assessment (EA) process was progressed. The Class EA has progressed to a point where a Secondary Plan can be commenced for the area of the P2B lands will not be required for the transportation corridor and will be released for development.

Employment forecasting completed as part of the economic study in support of the Secondary Plan (completed by Watson Associates Ltd.) anticipated an employment density of 25 jobs per net hectare. The calculated net area discounted the environmental features (and the potential for restricted development within the GTA West Corridor Focused Analysis Area). The economic study assumed that 80% of the gross developable area will be developable.

The economic study employment forecasting growth projections are shown in Table 3-2.

Table 3-2: PGEA P2B Secondary Plan Employment Growth Projections (Secondary Plan Economic Study)

PGEA P2B SGU	2031 Projections		
	Gross Developable Area (Excluding Environmental Features and GTA West Corridor)	Net Developable Area (80% of Gross Developable Area)	Total Employment (@ 25 jobs per net hectare)
SGU 554.02			
Excluding GTA West Corridor Lands	56.4 Ha	45.1 Ha	1,128
GTA West Corridor Lands within 554.02 (If released for Development)	21.9 Ha	17.5 Ha	438
Total SGU 554.02 (Including GTA West Corridor)	78.3 Ha	62.7 Ha	1,566
SGU 554.04	58.6 Ha	45.2 Ha	1,132
SGU 555.03	43.6 Ha	34.9 Ha	872
SGUs Total	178.5 Ha	142.8 Ha	3,570

The Region's total growth projections for the PGEA 2B area are more than 50% greater than the growth projections based on Watson's economic study (5,614 compared to 3,570).

Consideration for growth projections for the area will be focused on the impacts related to P2B lands and phasing of development to meet the Town of Halton Hills planned and anticipated timelines. Long-term servicing for the larger area is being addressed as part of the Regional Municipal Comprehensive Review and the subsequent Halton Region Water and Wastewater Master Plan.

The ASP has considered servicing based on the more conservative (higher total growth projections) established by the Region. The Region's BPEs have been adopted as part of previously approved planning and servicing studies and remain the approved planning projections for the area.

Total employment projections will be included as part of the final Secondary Plan.

4 Water

4.1 Existing Water System

Three (3) water treatment plants provide potable water for Halton Region's lake-based service areas: Burlington WTP, Oakville WTP and Burloak WTP. Halton Region's water transmission and distribution network is interconnected throughout Burlington and Oakville; however, the Oakville WTP and Burloak WTPs are the main supply sources to the Milton/Halton Hills lake-based area. PGEA P2B lies predominantly within the existing Milton Zone 5 (M5L) pressure zone whose boundaries have recently been reviewed.

Reference maps from the Sustainable Halton Water and Wastewater Master Plan Update, including Halton Region's existing water infrastructure (at the time of issue of the Sustainable Halton Report) and existing pressure zones are included in Appendix A.

4.1.1 PGEA Area

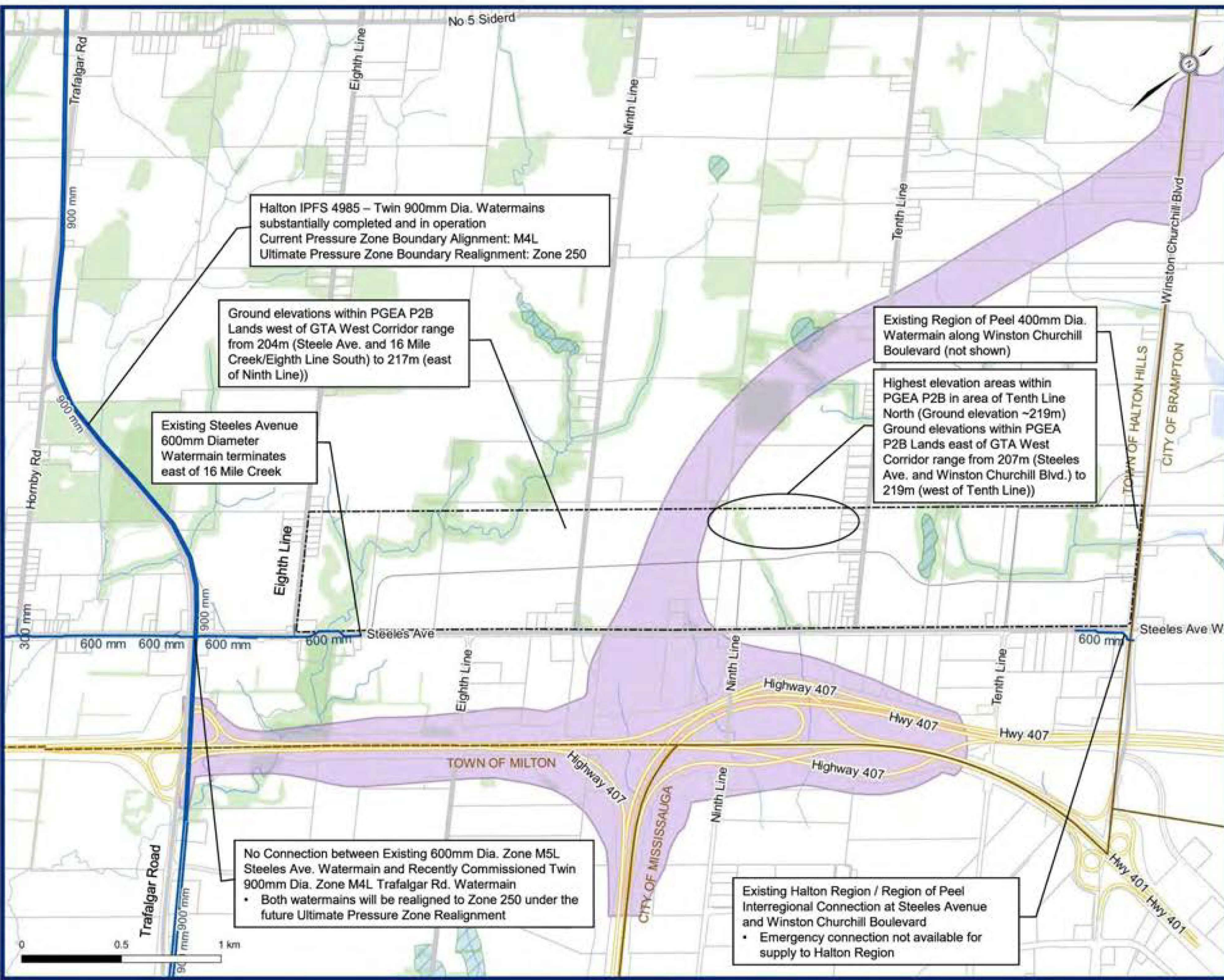
Currently the area is serviced from the west via an existing 600mm diameter trunk watermain running along Steeles Avenue (from James Snow Parkway). Additionally, there is an emergency regional interconnect with Peel at Steeles Avenue and Winston Churchill Boulevard. Halton Region has confirmed that the emergency Regional interconnect is not available for supply to Halton Region. The Region of Peel has an existing 400mm diameter watermain running north along Winston Churchill Boulevard. The current supply to the PGEA P2B area is effectively the 5 km long single dead-end feed running along Steeles Avenue from James Snow Parkway.

Within the area of the PGEA, Zone M4L 900mm diameter watermain along Trafalgar Road to the new Zone 4 Reservoir have recently been commissioned. Existing water infrastructure in the area of the PGEA P2B lands is shown in Figure 4-1.



Water & Wastewater Area Servicing Plan for the Premier Gateway Phase 2B

- Water Infrastructure**
- ▲ Pumping Station (PS)
 - Storage
 - Treatment
 - Feeder Main (Diameter >= 400mm)
 - Local Watermain (Diameter < 400mm)
- General Features**
- ▭ Premier Gateway Phase 2B
 - Highway
 - Major Road
 - Local Road
 - ▭ Property Parcel
 - ▭ Municipal Boundary
 - ▭ Preferred Route and Interchange Locations for GTA West Transportation Corridor Study



Halton IPFS 4985 – Twin 900mm Dia. Watermains substantially completed and in operation
Current Pressure Zone Boundary Alignment: M4L
Ultimate Pressure Zone Boundary Realignment: Zone 250

Ground elevations within PGEA P2B Lands west of GTA West Corridor range from 204m (Steeles Ave. and 16 Mile Creek/Eighth Line South) to 217m (east of Ninth Line))

Existing Steeles Avenue 600mm Diameter Watermain terminates east of 16 Mile Creek

Existing Region of Peel 400mm Dia. Watermain along Winston Churchill Boulevard (not shown)

Highest elevation areas within PGEA P2B in area of Tenth Line North (Ground elevation ~219m)
Ground elevations within PGEA P2B Lands east of GTA West Corridor range from 207m (Steeles Ave. and Winston Churchill Blvd.) to 219m (west of Tenth Line))

No Connection between Existing 600mm Dia. Zone M5L Steeles Ave. Watermain and Recently Commissioned Twin 900mm Dia. Zone M4L Trafalgar Rd. Watermain
• Both watermains will be realigned to Zone 250 under the future Ultimate Pressure Zone Realignment

Existing Halton Region / Region of Peel Interregional Connection at Steeles Avenue and Winston Churchill Boulevard
• Emergency connection not available for supply to Halton Region

Figure 4-1
Existing Water System



4.2 Planned Water System

4.2.1 Ultimate Pressure Zone Boundary Realignment

Due to existing and potential future level of service challenges, Pressure Zones 3, 4 & 5 boundaries have recently undergone extensive review. This review and analysis have resulted in the recommendation to realign the pressure zones boundaries within the existing Oakville and Milton Zones 3, 4 & 5. New pressure zones will be created and will be referred to based on their proposed top water level (TWL). These zones are 211 m, 223.5 m and 250 m. The boundaries for Milton Zone M5L (TWL 267 m) have also been modified. The PGEA P2B study area generally lies at the lower elevations within the existing M5L pressure zone where high pressures can occur during certain conditions. The Region continues to review the Ultimate Pressure Zone Boundary Alignment as part of their ongoing studies. As of March 2023, the Study Area is planned to remain within the Milton Zone M5L (TWL 267 m) pressure zone boundary upon commissioning of the Ultimate Pressure Zone Boundaries Realignment.

The existing and future pressure zone of the HH PGEA P2B lands is summarized in Table 4-1.

Table 4-1: HH PGEA P2B Lands Existing and Future Water Pressure Zone

Existing Pressure Zone	Future Pressure Zone (After Commissioning of Halton Region Ultimate Pressure Zone Boundary Realignment)
Zone M5L	Zone M5L (267 m)

Region staff have noted that under the Ultimate Pressure Zone strategy for this area, the Highway 401 corridor will be supplied from Milton Zone 5 / Zone 267 via a pressure reducing valve (PRV) at James Snow Parkway and Steeles Avenue (it is anticipated that the interconnect to Zone 250 at Trafalgar Road and Hornby Road will be normally closed). Outside of the scope of this assignment, the Region is considering optimizing the PRV pressure setting to improve pressures along the 401 corridor.

The reference map from the Sustainable Halton Water and Wastewater Master Plan Update, showing Halton Region's Ultimate Pressure Zone Boundary Realignment is included in Appendix A.

4.2.2 Pumping and Storage

The proposed Zone 250 400mm diameter watermain running along Hornby Road between Trafalgar Road and Steeles Avenue (in Halton Region's current capital program (Region IPFS 6641)) will provide the area with additional security of supply. The proposed Hornby Road watermain will connect the existing Steeles Avenue watermain with the Zone 250 900mm diameter trunk watermain running along Trafalgar Road. This will provide the area with additional supply from Neyagawa BPS and the new Trafalgar Road Zone 4 / Zone 250 Reservoir.

Halton Region has identified a potential water storage deficiency within the future Zone 250 that will service PGEA P2B lands. Halton Region continues to monitor the demand projections for the pressure zone. The potential deficiency will be addressed through the on-going Regional Municipal Comprehensive Review and the next Water and Wastewater Master Plan. Future water storage requirements estimated as part of work completed to support Halton Region's 2017 Development Charges Background Study are summarized in Table 4-2.

Table 4-2: Future Water Storage Requirements

Pressure Zone Service Area	Total Storage Requirement (ML)			Planned Available Storage (ML)	Related Infrastructure
	2021	2026	2031	2031	
250, 223.5, 211	30.8	39.0	42.4	45.0	Zone 4 Reservoir
267	9.9	11.0	12.0	12.0	

Total Storage Requirement taken from Table 12 – Water Storage Requirements, 2022 Development Charges Update Water/Wastewater Technical Report, Revision 5 (Final), prepared for Halton Region by GM BluePlan Engineering Limited, September 24, 2021

Planned Available Storage taken from Table 10 – Future Water Storage Requirements, Technical Memorandum #2 – Baseline and Future Capacity, Opportunities and Constraints, prepared for Halton Region by GM BluePlan Engineering Limited, April 2017.

It is not expected that the potential Zone 250 storage deficiency will impact the servicing timing for the P2B lands. Required storage for development phasing of the P2B lands will be reviewed compared to available storage capacity, and the P2B lands phasing plan will outline the servicing of planned development within the anticipated timeframe and within the context of Halton Region's proposed water servicing to the area.

4.2.3 Region's Timing and Development Charges Projects

Table 4-3 summarizes Halton Region's planned water infrastructure projects for the area with timing.

Table 4-3: Halton Region Area Water Projects

Region Project ID	Project Description	Project Municipal Class EA Schedule	Timing	Timing Reference	Pressure Zone	
					Current Pressure Zone Boundary Alignment	Ultimate Pressure Zone Boundary Realignment
3844	600 mm Zone M5L WM on Steeles Ave. from Trafalgar Rd. to East of Ninth Line	A+	Funded, but not constructed	Funded Project – Construction delayed (Region considering GTA West Corridor requirements)	M5L	250
5948	600mm Zone M5L WM on Steeles Ave. from East of Ninth Line to Peel Interregional Connection at Winston Churchill Boulevard	A+	Funded, but not constructed		M5L	250

Region Project ID	Project Description	Project Municipal Class EA Schedule	Timing	Timing Reference	Pressure Zone	
					Current Pressure Zone Boundary Alignment	Ultimate Pressure Zone Boundary Realignment
4985	900 mm WMs on Trafalgar Rd from Britannia Rd to new Zone 4 Reservoir (Zone M4L / Zone 250)	A+	Substantially completed and in service		M4L	250
6641	400 mm WM on Hornby Rd. from Steeles Ave to Trafalgar Rd.	A+	2025	2019 Budget and Business Plan (Development Capital Plan)	-	250
6642	400 mm WM in the 401 growth corridor north of Steeles from Hornby Rd. to Trafalgar Rd.	A+	2025		-	250
6643	400 mm WM in the 401 growth corridor north of Steeles from Trafalgar Rd to approximately 400m east of 8th Line	A+	2025	2017/2022 Development Charges Water/Wastewater Technical Report	-	250
6644	400mm WM in the 401 growth corridor from Steeles Ave to approximately 300 m north	A+	2025		-	250
6645	400mm WM in the 401 growth corridor north of Steeles Ave. from 1,000 m west of 9th Line to 900 m east of 9th Line	A+	2029		-	250
6646	400mm WM in the 401 growth corridor from Steeles Ave to approximately 330 m north	A+	2029		-	250
6647	400mm WM in the 401 growth corridor north of Steeles Ave. from 600 m west of 10 th Line to 1,000 m east of 10th Line	A+	2029		-	250
6648	400mm WM in the 401 growth corridor from Steeles Ave to 340 m north	A+	2029		-	250

Halton Region Project 3844 and 5948 – 600mm diameter transmission watermain running along Steeles Avenue was funded and carried forward to detailed design in 2012. Design of the project was suspended after implementation of the corridor protection area and policies to protect for the

Halton Peel Boundary Area Transportation Study (now the GTA West Corridor Study) and adoption of ROPA 43 (discussed further in Section 2.2.1).

September 2012 Design Drawings for Halton Region Project 3844/5948 show the proposed 600mm diameter watermain connecting to the existing 600mm diameter Emergency Regional Interconnect at the east limit (near Winston Churchill Boulevard).

The existing system and planned water projects for the area are shown in Figure 4-2.

The reference map from Halton Region's 2017 Development Charges Water and Wastewater Technical Report, showing Halton Region's Water Development Capital Implementation Plan (2017-2031) is included in Appendix A.



Water & Wastewater Area Servicing Plan for the Premier Gateway Phase 2B

- Water Infrastructure**
- ▲ Pumping Station (PS)
 - Storage
 - ⬢ Treatment
 - Feeder Main (Diameter >= 400mm)
 - Local Watermain (Diameter < 400mm)
- General Features**
- ▭ Premier Gateway Phase 2B
 - Highway
 - Major Road
 - Local Road
 - ▭ Property Parcel
 - ▭ Municipal Boundary
 - ▭ Preferred Route and Interchange Locations for GTA West Transportation Corridor Study

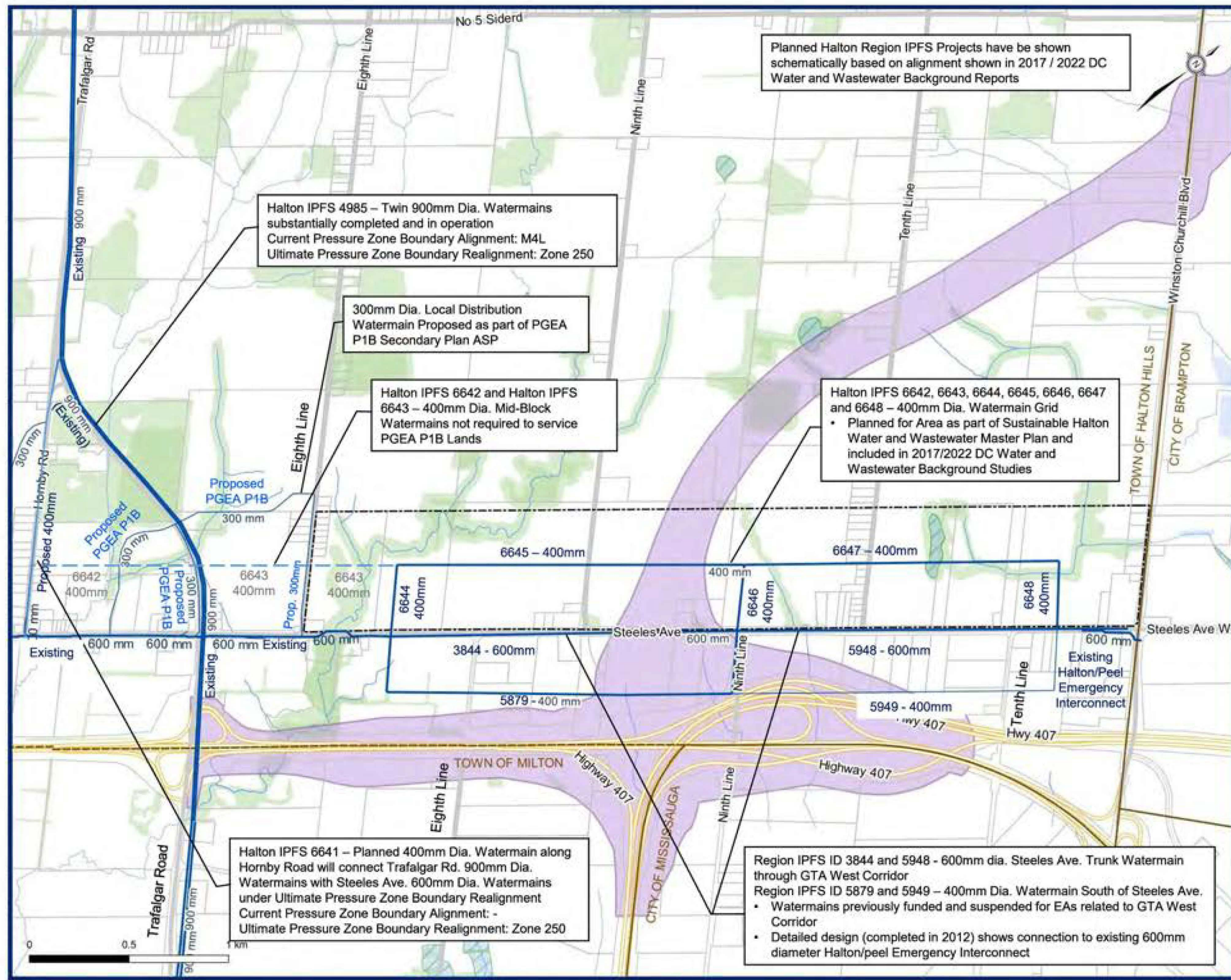


Figure 4-2
Planned Region Water Projects

4.3 Estimated Water Demands

4.3.1 Design Criteria

For the PGEA P2B ASP the recommendation is to use the design criteria developed for the Region's 2017 DC Update (also used for the Region's 2022 DC Update). The 2017/2022 DC Update Design Criteria is the best information available, developed based on a comprehensive review of the water and wastewater design criteria using 2011-2015 demand and flow data and updated estimates of actual population and employee numbers based on the 2011 census. At the time of the DC Update, Halton Region expressed that the revised criteria were representative of existing and ongoing system measures to reduce lost water and I/I (which will offset the need to upsize trunk infrastructure).

The recommended design criteria for the PGEA P2B proposed water demands is summarized in Table 4-4.

Table 4-4: Water Design Criteria

Design Criteria	Design Criteria	Design Criteria Reference
Residential	265 lpcd ¹	Based on Design Criteria from the 2017 DC Update
Industrial	295 lped ²	Based on Design Criteria from the 2017 DC Update
Commercial	175 lped	Based on Design Criteria from the 2017 DC Update
Institutional	220 lped	Based on Design Criteria from the 2017 DC Update
Max Day (lake based) PF	1.9	Based on Design Criteria from the 2017 DC Update
Peak Hour PF	3	Based on Design Criteria from the 2017 DC Update

¹lpcd refers to litres per capita per day.

²lped refers to litres per employee per day (for non-residential uses).

Similar to recommendations for the PGEA P1B ASP completed for Halton Region, it is recommended that industrial design criteria be applied for the projection of employment water demands (as well as wastewater flows) throughout the study area. This a conservative and reasonable approach that provides flexibility with regards to the future employment development in the study area. This also provides for a consistent design criteria approach applied to all PGEAs simplifying future comparison and allocation considerations.

Design criteria for water system components is summarized in Table 4-5.

Table 4-5: Water Design Criteria for Water System Components

Component	Design Criteria	
Feeder mains	Flow capacity	Convey maximum day demand while achieving water velocity requirements
Local Water mains	Flow capacity	Convey the greater of: • Maximum day demand plus fire flow demand, or • Peak hour demand

Component		Design Criteria
Pumping Stations	With adequate zone storage available	Supply maximum day demand to zone and all subsequent zones
	Without adequate storage available	Supply peak hour demand to zone and maximum day demand to all subsequent zones
Storage Facilities	Equalization (A)	25% of maximum day demand
	Fire (B)	Largest expected fire in zone (based on land use)
	Emergency (C)	25% of (A + B)
	Total Volume	= A + B + C
Fire Flow	Residential Flow	5,500 L/min for 2 hours @ minimum 140 kPa (20 psi)
	Minimum Employment Flow (Industrial / Commercial / Institutional)	15,000 L/min for 3 hours @ minimum 140 kPa (20 psi)
System Pressures	Minimum and maximum operating conditions	280 kPa (40 psi) to 700 kPa (100 psi)

For pressure zones with sufficient storage volume, water supply requirements are based on the maximum day demands (MDD). For pressure zones without floating storage, water supply requirements are based on peak hour demands. Transmission mains are required to convey the total pumping capacity of the receiving pumping station and the upper zone reservoir.

4.3.2 Water Demands

Consistent practice in the SHWWMP and 2017 DC Update is to develop water demands using existing conditions + growth demands. Existing conditions plus growth demands have been developed for the PGEA P2B lands based on the Region's updated planning projections.

As noted in Section 3.2.3, the employment targets for the P2B lands, developed as part of the Secondary Plan's supporting economic study, anticipate growth less than the Region's BPEs. The ASP has considered servicing based on the more conservative (higher total growth projections) established by the Region. The Region's BPEs have been adopted as part of previously approved planning and servicing studies and remain the approved planning projections for the area. Total employment projections will be included as part of the final Secondary Plan.

Long-term servicing for the area for projected growth from 2041 to 2051 is being addressed as part of the Regional Municipal Comprehensive Review, including development of Halton Region's Integrated Growth Management Strategy and the subsequent Halton Region Water and Wastewater Master Plan. The findings of the PGEA P2B ASP will support Halton Region's Municipal Comprehensive Review and MP Update projects.

4.4 Water Servicing Review and Needs Assessment

Assessment of the existing water system included review of existing GIS asset data, current Halton Region water model and most recent available design and construction drawings.

Hydraulic modelling was undertaken to assess preliminary water infrastructure demand and capacity.

4.5 Development of the Proposed Water Servicing Strategy

The water servicing strategy in support of the Secondary Plan was developed based on supplying water to the PGEA P2B area under the current zone alignment (study area located within Zone M5L) as well as through the connection to the new Zone 250, in case this connection is used to secure water supply to the PGEA P2B area.

Consideration of full development of the PGEA P2B lands under both zone boundary configurations ensures that development can proceed independent of the pressure zone supplying the area.

The proposed water servicing strategy was developed to meet the operational and fire flow requirements within the PGEA P2B lands and provide flexibility to:

- Be effectively incorporated into a future Region Master Plan water servicing update for the area; and,
- Meet the short-term needs of anticipated development within the area.

Supply and transmission to the area will be updated as part of the Region's Water and Wastewater Master Plan Update. Water servicing of the area to the 2051 horizon will need to further consider inclusion of Future Strategic Employment Area lands north of the PGEA, and the potential GTA West Corridor.

Future Strategic Employment Area lands located north of the PGEA do not have land use designations and inclusion of lands within the Future Strategic Employment Areas into the Urban Area is to be completed through a municipal comprehensive review.

Servicing to the small development areas located in the northwest portion of PGEA P2B (north of the designated Natural Heritage System between Eighth Line and Ninth Line) will require watermain crossings of environmental features or servicing along north limit of Secondary Plan area. It is anticipated that 3.5 Ha of development area within the northwest will require development of Future Strategic Employment Areas to the north to support the construction costs of water (and wastewater) infrastructure crossing the environmental features to service the projected 70 to 175 jobs for the areas.

4.5.1 Proposed Water Servicing Strategy

The Region has indicated that it will work with the Town to expedite construction and commissioning of required water (and wastewater) servicing to meet the Town's development targets, timing and phasing.

The proposed servicing strategy incorporates the following infrastructure recommendations:

Under Pressure Zone M5L Boundary Alignment

Under the current pressure zone alignment (PGEA P2B within Zone M5L):

- Supply from the proposed Zone M5L 600mm diameter watermain running along Steeles Avenue; and,

- Two (2) 400mm diameter watermain running along Ninth Line and Tenth Line north, supplied by the proposed 600mm diameter Steeles Avenue watermain.
- One (1) 400mm diameter watermain running along the proposed PGEA P2B Collector Road from the proposed Ninth Line 400mm diameter watermain to the proposed Tenth Line 400mm diameter watermain.

The proposed 600mm diameter transmission watermain running along Steeles Avenue supplying the proposed 400mm diameter watermain loop running along Ninth Line, the Proposed Collector Road and Tenth Line is essential to provide sufficient available fire flow to the Employment Lands located near Tenth Line, north of the Proposed Collector Road.

The proposed watermain can be constructed as components of the DC projects for the area. A watermain crossing of the potential GTA West Corridor will be required to provide security of supply and sufficient available fire flow to the PGEA P2B lands located near Tenth Line in the north portion of the Study Area. The ultimate alignment/corridor for the watermain crossing of the GTA West Corridor is flexible and can be aligned with future road alignment through the study area or at the north limit of the PGEA P2B lands. The 400mm diameter watermain will be required to be constructed at sufficient depth under the potential MTO corridor and any future highway structures footings (typically 5 metres or greater), and within a protective casing pipe to MTO and Halton Region standards. Design and construction can be coordinated with MTO to allow for construction of the watermain to proceed ahead of a future highway.

The proposed 400mm diameter servicing loop running along Ninth Line, the Proposed Collector Road and Tenth Line allows for flexibility of future Region transmission watermain alignments. The updated servicing strategy for the area can include future extension of transmission watermain to the north, which can include a north/south crossing of the GTA West Corridor along Tenth Line (within the Future Strategic Employment Areas) – or other configurations to suit development for the broader area to 2051.

The planned 600mm diameter and 400mm diameter watermain crossings of the GTA West Corridor can be relocated further to the north to accommodate development phasing and planning approvals (for both the GTA West Corridor and Future Strategic Employment Lands located north of the PGEA P2B lands) as well as constructability considerations for watermain crossings of a potential GTA West Corridor highway. The watermain crossings of the GTA West Corridor will be required to supply sufficient available fire flow to lands within the PGEA P2B Study Area located east of the GTA West Corridor. To meet the servicing requirements of these lands, ultimate alignment of the planned trunk watermain crossings will need to be further considered in coordination with the GTA West Corridor study and design (as well as part of the Region's Water and Wastewater Master Plan Update). The Proposed Water Servicing Strategy provides for flexibility of the ultimate location and alignment of the trunk watermain crossings the GTA West Corridor. Phasing of the proposed water servicing strategy is detailed further in Section 6.2.

Under Pressure Zone 250 Boundary Alignment

After commissioning of the Ultimate Pressure Zone Boundary Realignment, the Steeles Avenue watermain feed into the PGEA P2B area will be supplemented by a (normally closed) emergency connection to the Trafalgar Road feedermain from a future transmission watermain to be installed along Hornby Road.

4.5.2 Incorporation with the Region's Existing Water and Wastewater Master Plan and Future Municipal Class Environmental Assessment Requirements

The Area Servicing Plan has not been completed to Municipal Class EA requirements, instead building on the approved Approach 2 Sustainable Halton Water and Wastewater Master Plan. For recommended Schedule A, A+ and B projects, the final public notice for the Master Plan can also serve as the Notice of Completion for the Schedule B projects within the Master Plan (and the Secondary Plan Study Area).

Recommended water servicing projects that may potentially cross the GTA West Corridor Environmental Assessment study or significant environmental features are to be considered further at the project-specific approval stage for Revisions to Schedule B projects. The Municipal Class EA process provides for revisions to Schedule B projects due to environmental implications of changes to the project or due to a delay in implementation. Modifications to recommended water servicing projects identified as Schedule B projects can fulfill Class EA requirements as part of integration with Planning Act applications (including, but not limited to, future Zoning By-Law Amendments or Draft Plans of Subdivision). Public consultation can be combined with the required public consultation for Planning Act applications. Required Revisions to Schedule B Projects can reference or build on the previous Master Plan and Secondary Plan studies to demonstrate Phase 1 and Phase 2 of the Class EA process have been satisfied. A Revised Notice of Completion shall be issued with notification of the public's right to request a Part II order within the 30-day review period.

4.5.3 Local Service Watermains

Local 300mm diameter watermain is proposed along the Proposed East-West Collector Road, connecting to the proposed 400mm diameter transmission main at Ninth Line and Tenth Line. This will provide the PGEA P2B area with a large diameter distribution main across the PGEA P2B area for connection of smaller diameter watermains to service future development along future internal roads or direct water service connection for larger site plan applications.

A 300mm diameter watermain running north along Eighth Line, proposed as part of the PGEA P1B, can provide service to development fronting Eighth Line.

The Winston Churchill Boulevard is an arterial corridor and Regional Boundary Road that already has an existing Region of Peel 400mm diameter watermain installed within the right-of-way. A proposed 300mm diameter watermain is shown running along Winston Churchill; which will require Halton Region as well as Region of Peel approval ahead of detailed design and construction. A 300mm diameter watermain at the east limit of the PGEA P2B lands, connected to the proposed Steeles Avenue trunk watermain is required to provide security of supply to the eastern-most portion of the Secondary Plan study area. The opportunity to relocate the proposed watermain, shown on Winston Churchill Boulevard, to future local internal roads under Draft Plan of Subdivision is encouraged to provide the area with services and security of supply and mitigate extensive approval requirements, construction costs and disruption within Winston Churchill Boulevard.

Local watermain alignments and connections to the proposed DC watermains can be developed as part of future Draft Plans of Subdivision (or Zoning By-law Amendment) applications.

Halton Region does not permit service connections to watermains of 400 mm diameter or greater unless a deviation from the Region's policy is approved by the Region. Development fronting Steeles Avenue, Ninth Line and Tenth Line (and the Proposed Collector Road between Ninth Line

and Tenth Line) will require connections to local watermain to service any properties in the area. Design of local watermains through future local roads to reduce (or eliminate) requirements for local watermains to be installed within the same corridor as transmission watermains is encouraged as part of Planning Act Applications.

The proposed Water Servicing Solution for the PGEA P2B lands is shown in Figure 4-3.



Water & Wastewater Area Servicing Plan for the Premier Gateway Phase 2B

Water Infrastructure

- ▲ Pumping Station (PS)
- Storage
- Treatment
- Feeder Main (Diameter ≥ 400mm)
- Local Watermain (Diameter < 400mm)

General Features

- ▭ Premier Gateway Phase 2B
- Highway
- Major Road
- Local Road
- Property Parcel
- ▭ Municipal Boundary
- ▭ Preferred Route and Interchange Locations for GTA West Transportation Corridor Study

Preferred Land Use

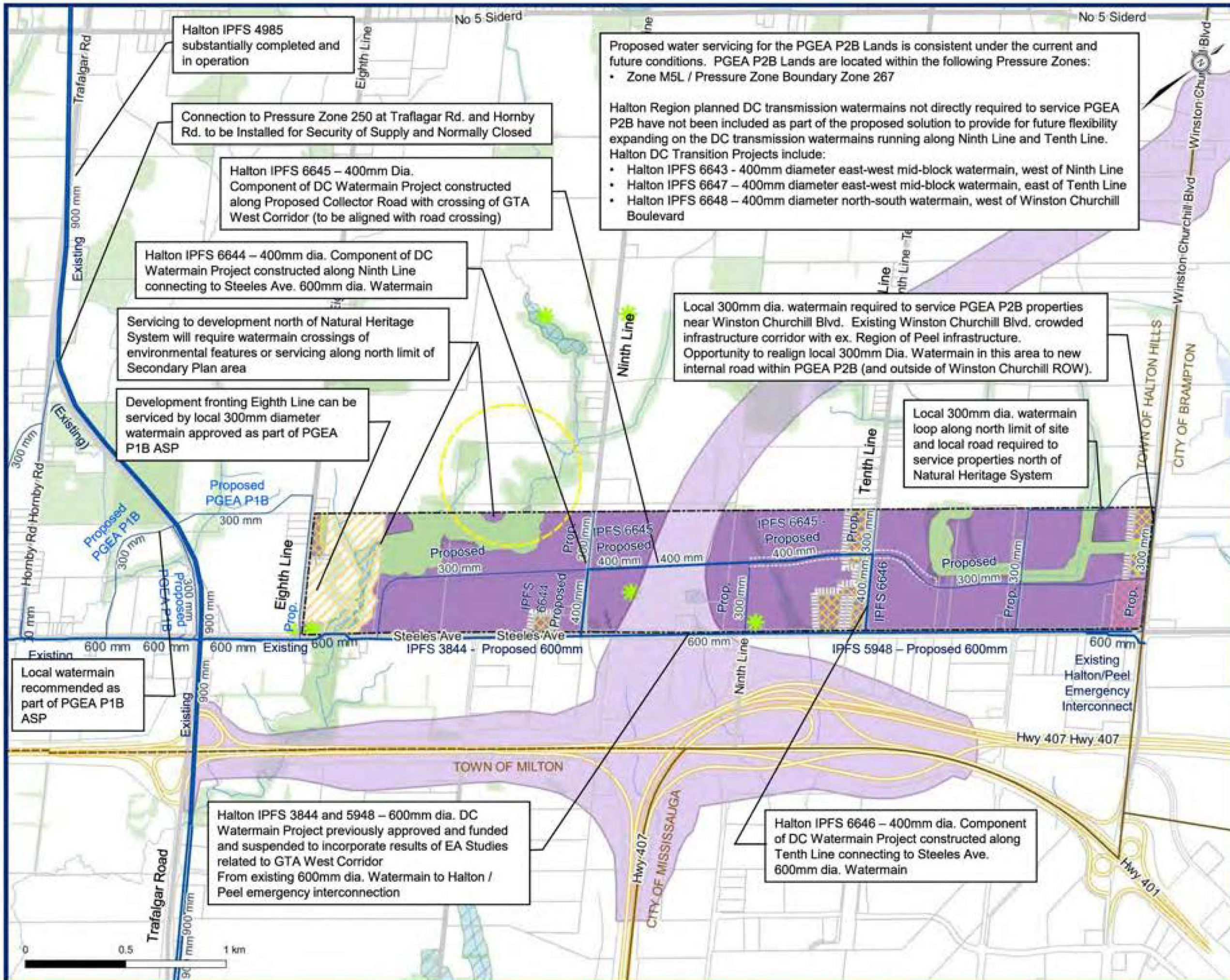
- Natural Heritage System
- Prestige Industrial Area
- Residential Special Policy Area
- Supportive Commercial
- Subject to D09OPA22.001 As Per PL180499
- Headwater Drainage Feature
- Buffer for Existing Residential Uses
- ★ Cultural Heritage Resource
- ▭ Minimum Distance Separation

Figure 4-3

Proposed Water Servicing Solution for PGEA P2B



September 2023
720010-004
Project: EPSO 35917



4.5.4 Water Distribution Modelling Analysis

Halton Region's InfoWater models were utilized to analyse the servicing scheme for the PGEA P2B lands under 2031 conditions. The following scenarios will be run for the analysis of PGEA P2B:

- Maximum Day Demand (MDD);
- Peak Hour Demand (PHD); and,
- Maximum Day Demand plus Fire Flow (MDD+FF)

Model simulations were completed utilizing Halton Region's current model (provided by Halton Region).

Modelling shows that operating pressures in the Study Area range from approximately 45 psi to 85 psi (elevation range for the service area is around 204m to 219m).

With anticipated looping and security of supply of proposed watermain throughout the study area, modelled available fire flow will exceed 300L/s. The water modelling results are summarized in Table 4-6.

Table 4-6: Water Modelling Results for PGEA P2B (Current and Ultimate Pressure Zone Boundary Configurations)

	Demand Condition		
	ADD	MDD	PHD
Range of Service Elevations in Block	~204m to 219m		
Current and Ultimate Pressure Zone Boundary Configuration (PGEA P2B in Zone M5L / TWL 267)			
HGL (m)	~265m	~265m	~265m
Pressure Range	65 to 85psi	65 to 85psi	64 to 84psi
Fire Flow Availability	n/a	~325L/s	n/a
Ultimate Pressure Zone Boundary Configuration (PGEA P2B in Zone 250)			
HGL (m)	~250m	~250m	~249m
Pressure Range	44 to 64psi	44 to 64psi	42 to 62psi
Fire Flow Availability	n/a	~300L/s	n/a

Hydraulic analysis has confirmed that 400mm diameter watermain are required to provide sufficient available fire flow to the Study Area. This is in line with the recommendations of the SH Master Plan.

5 Wastewater

5.1 Existing Wastewater System

The PGEA P2B study area lies within the Mid-Halton Wastewater Treatment Plant (WWTP) catchment area. Existing wastewater flows in the study area and surrounding areas are collected through sewers along Steeles Avenue, which convey flows to two (2) sewage pump stations: Halton Hills #3 Wastewater Pumping Station (WWPS) and Halton Hills #2 WWPS. From these two stations, wastewater flows are conveyed west to Halton Hills #1 WWPS and ultimately conveyed south through a series of trunk sewers and pump stations (the Miller Way Trunk Sewer and Mid-Block WWPS) discharging at the Mid-Halton WWTP. Existing wastewater infrastructure in the area of the PGEA P2B lands is shown in Figure 5-1.

Reference maps from the Sustainable Halton Water and Wastewater Master Plan Update, showing Halton Region's existing wastewater network (at the time of issue of the Sustainable Halton Report) and existing wastewater drainage areas are included in Appendix A.



Water & Wastewater Area Servicing Plan
for the Premier Gateway Phase 2B

- Wastewater Infrastructure**
- ▲ Sewage Pumping Station (SPS)
 - Trunk Sewer (diameter $\geq 450\text{mm}$)
 - Local Sewer (diameter $< 450\text{mm}$)
 - - - Forcemain
- General Features**
- ▭ Premier Gateway Phase 2B
 - Highway
 - Major Road
 - Local Road
 - ▭ Property Parcel
 - ▭ Municipal Boundary
 - ▭ Preferred Route and Interchange Locations for GTA West Transportation Corridor Study
- ➡ Direction of Existing Topography Grade (High Elevations to Low Elevations)

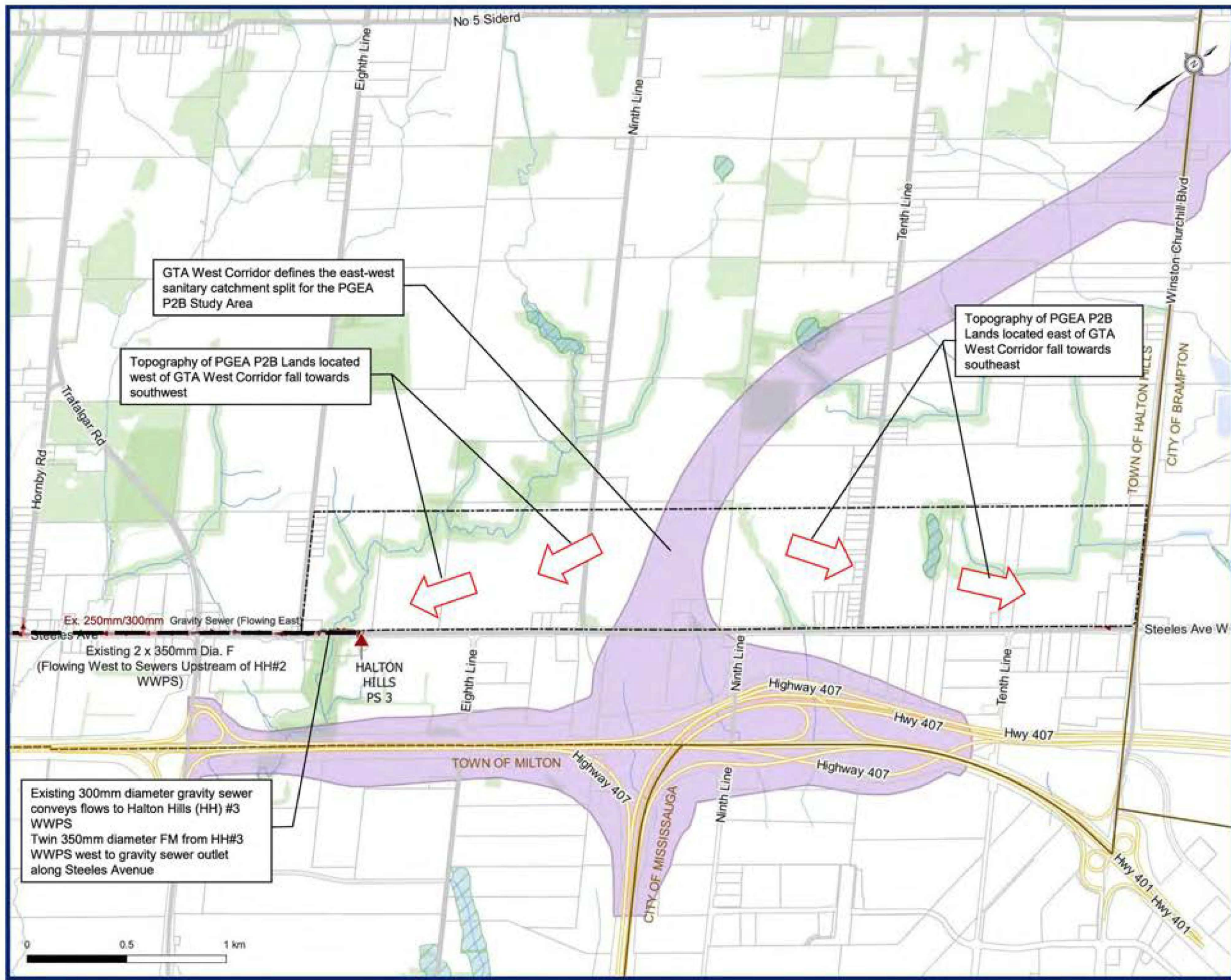


Figure 5-1
Existing Wastewater System

5.2 Planned Wastewater System

5.2.1 Region's Timing and Development Charges Projects

A major trunk sewer (Eighth Line/Trafalgar Trunk Sewer) was identified in the SHWWMP to service growth within Halton Hills, specifically by extending the lake-based wastewater service area to the southern lands of Georgetown. This trunk sewer will be located at the west boundary of the study area and will service PGEA P2B.

Additionally, the 2017 DC Update identified a project to decommission Halton Hills #3 WWPS and free up capacity in the downstream infrastructure (Halton Hills #1 WWPS, Halton Hills #2 WWPS, and internal Milton sewer network).

Table 5-1 summarizes Halton Region's planned wastewater infrastructure projects for the area with timing.

Table 5-1: Halton Region Area Wastewater Projects

Region Project ID	Project Description	Project Municipal Class EA Schedule	Timing	Timing Reference
3863	WWM on Steeles Ave. from West of Ninth Line to HH #3 WWPS (To be transferred to 7553 – Eighth Line Trunk Sewer and HH#3 WWPS decommissioned)	A+	Funded, but not constructed	Sustainable Halton MP notes Project as Funded Project (WM – 2008 MP Projects)
3864	Halton Hills (HH) #4 WWPS at intersection of Steeles Ave. and Winston Churchill Blvd.	B		
3865	WWFM on Steeles Ave. from HH #4 WWPS to 3863 – WWM on Steeles Ave.	A+		
4648	WWM on Steeles Ave. from East of Ninth Line to Winston Churchill Boulevard (3864 - HH#4 WWPS)	A+		
6508	Decommissioning of HH #3 WWPS and connection to new Eighth Line trunk sewer and conversion of site to septage receiving facility	A+	2025 - Under Construction	2020 Halton Region Allocation Program Update
7550	1200mm WWM on 8th Line from No. 5 Side Road to Steeles Avenue	A+		
7552	1200mm WWM on Steeles Avenue from 8th Line to easement crossing Highway 401	A+		
7553	1200mm WWM from ID 7552 on Steeles Avenue to Auburn Road, (crossing Highway 401)	A+		

Steeles Avenue Trunk Sewer and HH#4 WWPS and Forcemain

Halton Region Projects 3863, 3864 and 3865 (the proposed trunk sewer running along Steeles Avenue to service the west portion of the Study Area and the future HH#4 WWPS and forcemain

to service the eastern portion of the PGEA P2B lands) were previously approved and funded. Design of the projects was suspended after implementation of the corridor protection area and policies to protect for the Halton Peel Boundary Area Transportation Study (now the GTA West Corridor Study) and adoption of ROPA 43 (discussed further in Section 2.2.1).

HH #4 WWPS was designed to accommodate flows from the east portion of the PGEA P2B lands as well as Phase 2A lands located south of Steeles Avenue. The proposed SPS was designed in 2012/2013 with a firm capacity of 141 L/s, to service a planned 2031 equivalent population of 6,421 persons and an overall catchment area of 270 Ha. The SPS design is detailed further in the Halton Hills Sewage Pumping Station No. 4 Preliminary Design Report, by Stantec Inc., Revision 1, Draft, January 24, 2013.

Ahead of completion of their ongoing Region-wide Water and Wastewater Master Plan Update, the Region is completing a feasibility study for wastewater servicing of the PGEA P2B Study Area. The feasibility study will consider both gravity sewer and WWPS and forcemain options to service PGEA P2B lands east of the GTA West Corridor (and impacts on trunk servicing infrastructure west of the GTA West Corridor). The Region is looking to complete the feasibility study work by Summer 2023, and if the feasibility study determines that the preferred solution requires a municipal Class EA, the Study is planned for commencement in 2023 and completion by Spring 2024. Design of the preferred solution is anticipated by Summer/Fall 2025, with construction and commissioning completed by Winter 2027.

Eighth Line/Trafalgar Trunk Sewer

Design of the Eighth Line/Trafalgar Trunk Sewer is currently underway. Halton Region has approved financing for the Eighth Line/Trafalgar Trunk sewer as part of their updated 2020 Allocation Program. The 2020 Allocation Program includes a 'full program' that will accommodate new greenfield growth to the year 2022. It is estimated that Halton Region's Eighth Line Trunk Sewer will be commissioned to service area development by 2025.

Also considered was the scenario where the Eighth Line/Trafalgar Trunk Sewer is not commissioned prior to development proceeding within the Study Area lands. Flows from PGEA P2B lands will be conveyed west to HH #3 WWPS and then west to HH#2 WWPS, and HH#1 WWPS (rather than to the Eighth Line/Trafalgar Trunk Sewer).

Region trunk sewers running along Ninth Line, Tenth Line or Winston Churchill Boulevard are not included in the Development Capital Implementation Plan.

The planned wastewater projects for the area are shown in Figure 5-2.

Reference maps from Halton Region's 2017/2022 Development Charges Water and Wastewater Technical Report, showing Halton Region's Wastewater Development Capital Implementation Plan (2017-2031) and Future Wastewater Drainage Areas are included in Appendix A.



Water & Wastewater Area Servicing Plan for the Premier Gateway Phase 2B

- Wastewater Infrastructure**
- ▲ Sewage Pumping Station (SPS)
 - Trunk Sewer (diameter ≥ 450mm)
 - Local Sewer (diameter < 450mm)
 - Forcemain
- General Features**
- ▭ Premier Gateway Phase 2B
 - Highway
 - Major Road
 - Local Road
 - ▭ Property Parcel
 - ▭ Municipal Boundary
 - ▭ Preferred Route and Interchange Locations for GTA West Transportation Corridor Study

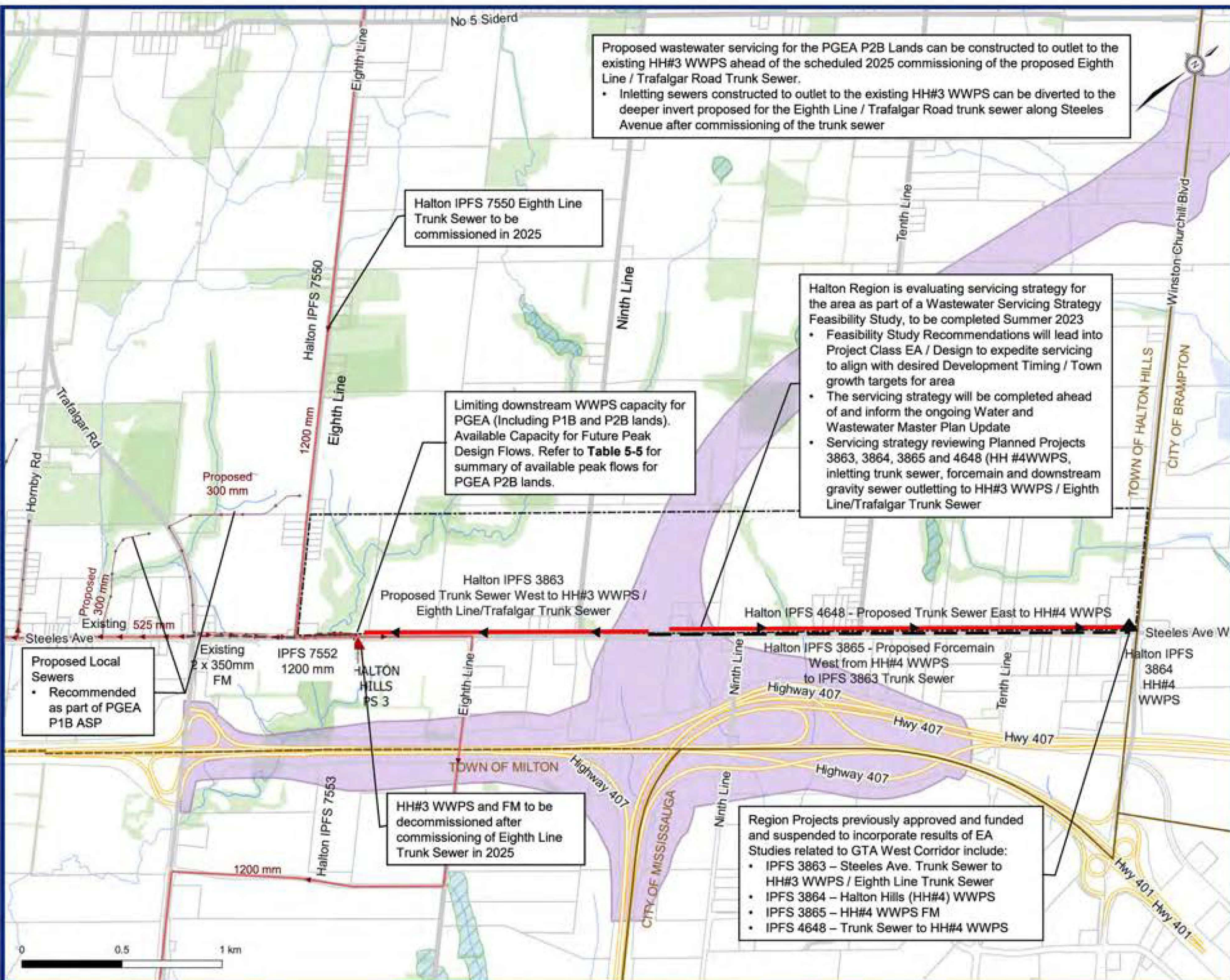


Figure 5-2
Planned Region Wastewater
Projects



5.3 Wastewater Design Criteria and Flows

5.3.1 Wastewater Design Criteria

As noted under Section 4.3.1, it is recommended that the design criteria developed for the Region's 2017 DC Update (and used in the Region's 2022 DC Update) be utilized for this ASP. The 2017/2022 DC Update Design Criteria is representative of existing and ongoing system measures to reduce I/I (which will offset the need to upsize trunk infrastructure).

The recommended design criteria for the PGEA P2B proposed wastewater flows for Treatment Plant and Collection System are summarized in Table 5-2 and Table 5-3.

Table 5-2: Wastewater Design Criteria (Treatment Plant)

Design Criteria	Average Flow	Design Criteria Reference
Residential	360 lpcd	Based on Design Criteria from the 2017 DC Update
Industrial	405 lped	Based on Design Criteria from the 2017 DC Update
Commercial	245 lped	Based on Design Criteria from the 2017 DC Update
Institutional	305 lped	Based on Design Criteria from the 2017 DC Update

Table 5-3: Wastewater Design Criteria (Collection System)

Design Criteria	Dry Weather Flow	Design Criteria Reference
Residential	215 lpcd x wastewater peaking factor	Based on Design Criteria from the 2017 DC Update
Industrial	240 lped x wastewater peaking factor	Based on Design Criteria from the 2017 DC Update
Commercial	145 lped x wastewater peaking factor	Based on Design Criteria from the 2017 DC Update
Institutional	180 lped x wastewater peaking factor	Based on Design Criteria from the 2017 DC Update
Inflow and Infiltration Allowance	0.286 L/s/ha	Based on Design Criteria from the 2017 DC Update

Similar to recommendations for the PGEA P1B ASP completed for Halton Region, it is recommended that Industrial design criteria be applied for the projection of employment wastewater flows throughout the study area. This a conservative and reasonable approach that provides flexibility with regards to the future employment development in the study area. This also provides for a consistent design criteria approach applied to all PGEAs simplifying future comparison and allocation considerations.

Design criteria for wastewater system components is summarized in Table 5-4.

Table 5-4: Design Criteria for Wastewater System Components

Component		Design Criteria
Local Sewers	Roughness Coefficient	$n = 0.013$ for PVC sewers (Based on Halton Engineering Design Guidelines for Area Servicing Plans)
	Capacity	Peak flow (Q) versus Sewer full flow capacity (q_{manning}) less than 85% (Based on criteria established for Halton 2017 Development Charges Water and Wastewater Background Study).

5.3.2 Wastewater Flows

Consistent practice in the SHWWMP and 2017 DC Update is to develop wastewater flows using existing conditions + growth flows. Existing conditions plus growth flows will be evaluated for the Town of Halton Hill's employment projections within the PGEA P2B lands and compared to the flows derived from the 2017 DC Update.

Growth projections and flows for the PGEA (including P2B lands) were updated by the Region as part of the PGEA P1B ASP. The Region's updated growth projections showed that total flows from the PGEA will remain effectively the same, with some realignment within the Mid-Halton WWTP catchment (based on Regional Official Plan Amendment 47 planning projection adjustments to PGEA P1B lands). The treatment plant-level wastewater analysis, evaluation, and recommendations from the 2017 and 2022 DC Updates can be carried forward as the realignment of flows is entirely within the Mid-Halton WWTP catchment area.

As noted in Section 3.2.3, the employment targets for the P2B lands, developed as part of the Secondary Plan's supporting economic study, anticipate growth less than the Region's BPEs. The ASP has considered servicing based on the more conservative (higher total growth projections) established by the Region. The Region's BPEs have been adopted as part of previously approved planning and servicing studies and remain the approved planning projections for the area. Total employment projections will be included as part of the final Secondary Plan.

Long-term servicing for the area for projected growth from 2041 to 2051 is being addressed as part of the Regional Municipal Comprehensive Review, including development of Halton Region's Integrated Growth Management Strategy and the subsequent Halton Region Water and Wastewater Master Plan. The findings of the PGEA P2B ASP will support Halton Region's Municipal Comprehensive Review and MP Update projects.

5.4 Wastewater Servicing Review and Needs Assessment

Assessment of the existing wastewater system included review of existing GIS asset data, current Halton Region wastewater model (InfoSewer) and most recent available design and construction drawings. Hydraulic modelling was undertaken to confirm the existing wastewater flows, capacity and potential required infrastructure upgrades.

5.4.1 Review of Available Capacity Under Scenario 1 Prior to Commissioning of the Eighth Line/Trafalgar Trunk Sewer

Through previous work, including the SHWWMP, Halton Region had noted potential capacity issues downstream of the PGEA lands. Downstream capacity for the PGEA lands was reviewed as part of Halton Region's June 2019 PGEA P1B ASP.

Peak flows from PGEA lands (including projected P1B peak flows) and available peak flow capacity at downstream pumping stations are shown in Table 5-5.

Table 5-5: Downstream Pumping Stations Available Capacities (Prior to Commissioning of Eighth Line/Trafalgar Trunk Sewer)

WWPS	Firm Capacity	Existing Peak Flow (Provided by Halton Region as part of PGEA P1B Study)	Peak Flow Available for all PGEA Lands	Peak Flow from PGEA P1B	Available Peak Flow Capacity for PGEA P2B
HH #3	34 L/s (1 Pump + 1 Standby) 60.3 L/s (ECA)*	11 L/s	23 L/s	14 L/s	9 L/s – 23 L/s
HH #2	195 L/s (3 Pumps)	44 L/s	151 L/s	100 L/s	51 L/s – 151 L/s
HH #1	280 L/s (2 Pumps + 1 Standby)	60 L/s	220 L/s	100 L/s	120 L/s – 220 L/s
Mid-Block WWPS	1,215 L/s (3 Pumps + 1 Standby)	783 L/s	432 L/s	100 L/s	332 L/s – 432 L/s

Region staff have indicated that the available capacity at HH# 3 WWPS is limited to 34 L/s (less than the 60.3 L/s approved under the ECA for the SPS). A 2014 assignment was completed to review the performance of the SPS (pumps designed, installed and tested to meet a specified rated capacity of 53.0 L/s at 14.0m TDH). The study determined that the SPS components (including pumps, valves and appurtenances) were assessed and determined to be working as expected. The 2014 study concluded that the limited available flow from the station (and equivalent capacity) may be due to an issue with the downstream forcemain (potentially a physical restriction, entrapped air, pipe deformation, etc.). The study recommended an inspection of the forcemain to confirm any issues.

To increase the firm capacity to the approved 60.3 L/s, the Region will first need to complete inspection of the forcemain and determine the cause of the flow limitation. Once the flow issue has been resolved, an additional duty/standby pump can be installed (or alternate pump replacement/installation scenario).

Under the current capacity limitations at HH#3 WWPS, there is 23 L/s of peak flow available to development planned to come online ahead of commissioning of the Eighth Line Trunk Sewer commissioning. Assuming previously approved PGEA P1B development proceeds in full ahead of the P2B lands, HH#3 WWPS will only have 9 L/s available peak capacity for PGEA P2B lands. It is anticipated that allocation of the available 23 L/s of peak capacity will be coordinated between the Town and Region to best service development phasing in the area.

Allocation of development of future PGEA P2B lands (as well as PGEA P1B lands) ahead of the 2025 commissioning of the Eighth Line Trunk Sewer will also need to have consideration for downstream pumping station capacity as well as detailed phasing of development of future P1B lands.

5.5 Development of the Proposed Wastewater Servicing Strategy

The overall servicing strategy is based conveying flows from the PGEA P2B lands south to the proposed trunk sewer running along Steeles Avenue from east to west and ultimately outletting to:

- The existing HH #3 WWPS (ahead of commissioning of the Eighth Line Trunk Sewer); and,
- the proposed Eighth Line Trunk Sewer (after proposed commissioning in 2025).

Prior to commissioning of the Eighth Line/Trafalgar Trunk Sewer, PGEA P2B lands will ultimately outlet to HH #3 WWPS, and ultimately through HH #2 WWPS, HH#1 WWPS, Miller Way Trunk Sewer, Mid-Block WWPS and outletting at the Mid-Halton WWTP. After commissioning of the Eighth Line/Trafalgar Trunk Sewer, PGEA P2B lands will outlet to the proposed Eighth Line/Trafalgar Trunk Sewer and ultimately to the Mid-Halton WWTP. Consideration of development of the PGEA P2B lands under pre- and post-commissioning of the Eighth Line/Trafalgar Trunk Sewer ensures that development can proceed independent of the commissioning of the Region's trunk sewer.

The proposed wastewater servicing strategy was developed to meet the short-term needs of anticipated development within the area, with understanding that the long-term servicing strategy for the area will be reviewed and potentially updated as part of the Region's updated Water and Wastewater Master Plan.

The topography of the PGEA P2B area provides for two (2) sanitary sub-catchments that can be delineated at the potential GTA West Corridor (generally mid-block between Ninth Line and Tenth Line). Sanitary conveyance requirements for each of the sub-catchments are summarized in Table 5-6.

Table 5-6: Summary of PGEA P2B Wastewater Sub-Catchments

PGEA P2B Development Scenario	West of GTA West Corridor (Mid-Block between Ninth Line and Tenth Line)	East of GTA West Corridor (Mid-Block between Ninth Line and Tenth Line)
Proposed Sanitary Conveyance	<ul style="list-style-type: none"> • Wastewater flows can be conveyed by local gravity sewers to the southwest, connecting to the planned Steeles Avenue trunk sewer, or via local sewers, directly to the HH#3 WWPS (or Eighth Line/Trafalgar Trunk Sewer) 	<ul style="list-style-type: none"> • Wastewater flows can be conveyed by local gravity sewers to the southeast via local gravity sewers
Required Infrastructure to Connect to the Halton Region Wastewater Network	<ul style="list-style-type: none"> • Lands located west of the GTA West Corridor can drain via future local gravity sewers with future direct connection to existing Region infrastructure. • Ultimately a future Region trunk sewer constructed along Steeles Avenue (draining from west of the GTA West Corridor to existing downstream Region infrastructure) can collect flows from local sewers. 	<ul style="list-style-type: none"> • A future pumping station and forcemain (Region's planned HH#4 WWPS and forcemain) or deep trunk sewer (draining from east to west) is required to convey flows from the lands located east of the GTA West Corridor.

Servicing to the small development areas located in the northwest portion of PGEA P2B (north of the designated Natural Heritage System between Eighth Line and Ninth Line) will require sewer crossings of environmental features or servicing along north limit of Secondary Plan area. It is anticipated that 3.5 Ha of development area within the northwest will require development of Future Strategic Employment Areas to the north to support the construction costs of wastewater (and water) infrastructure crossing the environmental features to service the projected 70 to 175 jobs for the areas.

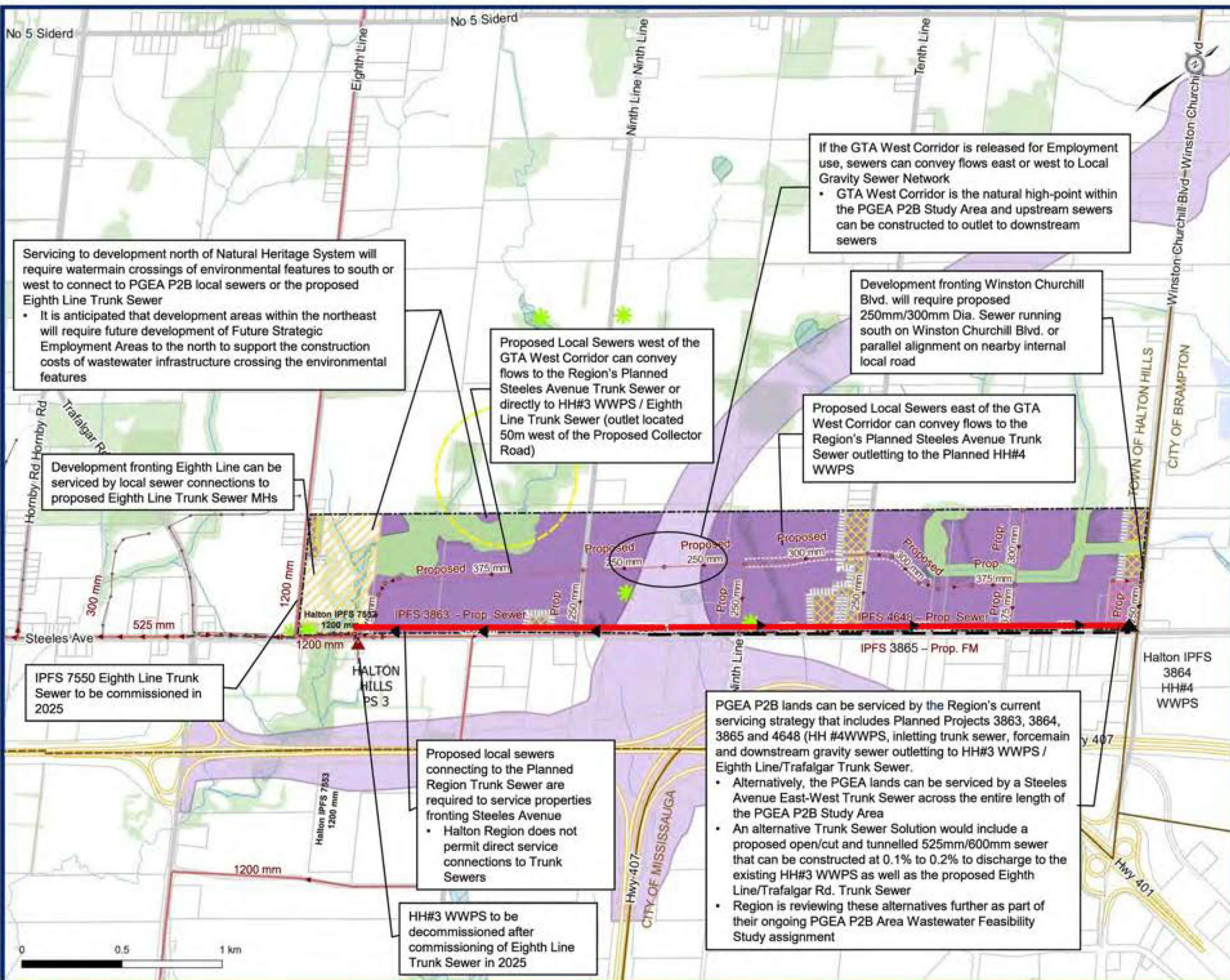
5.5.1 Proposed Wastewater Servicing Strategy

As noted in Section 4.5.1, The Region has indicated that it will work with the Town to expedite construction and commissioning of required water (and wastewater) servicing to meet the Town's development targets, timing and phasing.

The proposed wastewater servicing solution for PGEA P2B (based on approved Region wastewater projects for the area) is shown in Figure 5-3. The Region is reviewing the approved servicing strategy as part of the PGEA P2B Wastewater Servicing Feasibility Study Assignment that is currently underway. The proposed wastewater servicing strategy for the Secondary Plan has been developed to include flexibility to accommodate the ongoing Region work, and this ASP will inform the Region's ongoing studies.



Water & Wastewater Area Servicing Plan
for the Premier Gateway Phase 2B



- Wastewater Infrastructure**
- ▲ Sewage Pumping Station (SPS)
 - Trunk Sewer (diameter ≥ 450 mm)
 - Local Sewer (diameter < 450 mm)
 - Forcemain
- General Features**
- ▭ Premier Gateway Phase 2B
 - Highway
 - Major Road
 - Local Road
 - ▭ Property Parcel
 - ▭ Municipal Boundary
 - ▭ Preferred Route and Interchange Locations for GTA West Transportation Corridor Study
- Preferred Land Use**
- Natural Heritage System
 - Prestige Industrial Area
 - Residential Special Policy Area
 - Supportive Commercial
 - Subject to D09OPA22.001 As Per PL180499
 - Headwater Drainage Feature
 - ▨ Buffer for Existing Residential Uses
 - ★ Cultral Heritage Resource
 - ▭ Minimum Distance Separation

Servicing to development north of Natural Heritage System will require watermain crossings of environmental features to south or west to connect to PGEA P2B local sewers or the proposed Eighth Line Trunk Sewer

- It is anticipated that development areas within the northeast will require future development of Future Strategic Employment Areas to the north to support the construction costs of wastewater infrastructure crossing the environmental features

If the GTA West Corridor is released for Employment use, sewers can convey flows east or west to Local Gravity Sewer Network

- GTA West Corridor is the natural high-point within the PGEA P2B Study Area and upstream sewers can be constructed to outlet to downstream sewers

Development fronting Winston Churchill Blvd. will require proposed 250mm/300mm Dia. Sewer running south on Winston Churchill Blvd. or parallel alignment on nearby internal local road

Proposed Local Sewers east of the GTA West Corridor can convey flows to the Region's Planned Steeles Avenue Trunk Sewer outletting to the Planned HH#4 WWPS

Proposed Local Sewers west of the GTA West Corridor can convey flows to the Region's Planned Steeles Avenue Trunk Sewer or directly to HH#3 WWPS / Eighth Line Trunk Sewer (outlet located 50m west of the Proposed Collector Road)

Development fronting Eighth Line can be serviced by local sewer connections to proposed Eighth Line Trunk Sewer MHs

IPFS 7550 Eighth Line Trunk Sewer to be commissioned in 2025

Proposed local sewers connecting to the Planned Region Trunk Sewer are required to service properties fronting Steeles Avenue

- Halton Region does not permit direct service connections to Trunk Sewers

HH#3 WWPS to be decommissioned after commissioning of Eighth Line Trunk Sewer in 2025

PGEA P2B lands can be serviced by the Region's current servicing strategy that includes Planned Projects 3863, 3864, 3865 and 4648 (HH #4WWPS, inletting trunk sewer, forcemain and downstream gravity sewer outletting to HH#3 WWPS / Eighth Line/Trafalgar Trunk Sewer.

- Alternatively, the PGEA lands can be serviced by a Steeles Avenue East-West Trunk Sewer across the entire length of the PGEA P2B Study Area
- An alternative Trunk Sewer Solution would include a proposed open/cut and tunnelled 525mm/600mm sewer that can be constructed at 0.1% to 0.2% to discharge to the existing HH#3 WWPS as well as the proposed Eighth Line/Trafalgar Rd. Trunk Sewer
- Region is reviewing these alternatives further as part of their ongoing PGEA P2B Area Wastewater Feasibility Study assignment

Figure 5-3
Proposed Wastewater Servicing
Solution for PGEA P2B



The proposed servicing strategy incorporates the following infrastructure recommendations:

Prior to Commissioning of the Eighth Line/Trafalgar Trunk Sewer

Prior to commissioning of the Eighth Line/Trafalgar Trunk Sewer:

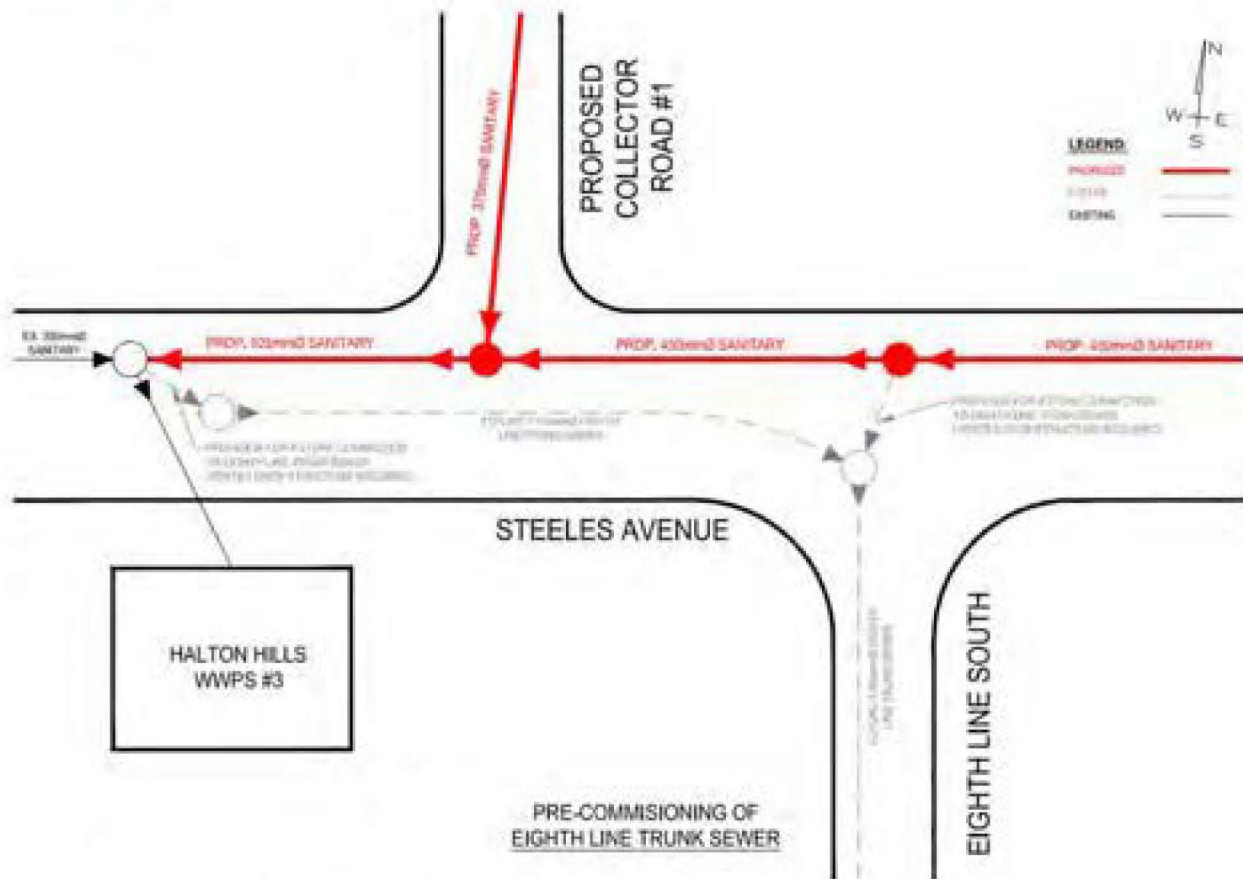
- Conveyance of wastewater flows from lands west of the GTA West Corridor by local gravity sewers and/or Steeles Avenue trunk sewer, outletting to the inlet sewer upstream of HH#3 WWPS; and,
- Conveyance of wastewater flows from lands east of the GTA West Corridor to:
 - The proposed HH#4 WWPS and forcemain; or,
 - Equivalent downstream servicing solution developed as part of the Region's PGEA P2B Wastewater Servicing Feasibility Study.

The Region's ongoing PGEA P2B Wastewater Servicing Feasibility Study is expected to review (at minimum) conveyance of wastewater flows from the east portion of the PGEA P2B lands via a pumping station and forcemain solution (equivalent to HH#4 WWPS and associated forcemain) as well as a deep trunk sewer running east-to-west along Steeles Avenue. A Steeles Avenue trunk sewer can be constructed to meet the servicing requirements of the PGEA P2B area and outlet to the inlet MH invert at the exiting HH#3 WWPS. A trunk sewer alternative would range in depth from approximately 3 metres at Steeles Avenue and Winston Churchill Boulevard to nearly 20 metres through the GTA West Corridor (where ground elevations are highest along the alignment). Profile drawings for the preliminary trunk sewer alternative have been included in Appendix B.

Sizing of the Region's DC projects related to the HH#4 WWPS, inletting Steeles Avenue trunk sewer, forcemain and downstream Steeles Avenue trunk sewer have been sized to accommodate wastewater flows from PGEA P2B (to 2031). Ultimately, the projects may be updated to include more catchment area as part of the Region's ongoing servicing projects being completed in support of the Regional Municipal Comprehensive Review.

The Region's ongoing Wastewater Pumping Station Servicing Strategy Update is considering opportunities for pumping station alternative projects in the area. The recommendations from the Study will be incorporated into Halton Region's Water and Wastewater Master Plan Update.

The outlet to the HH#3 WWPS is shown schematically in Figure 5-4.



**Figure 5-4: Proposed Outlet to HH#3 WWPS
(Ahead of Commissioning of Eighth Line Trunk Sewer)**

After Commissioning of the Eighth Line/Trafalgar Trunk Sewer

After commissioning of the Eighth Line/Trafalgar Trunk Sewer, the proposed servicing strategy for the PGEA P2B lands will be similar, with no capacity constraints for the development of the P2B lands.

If the under-construction trunk sewer is available ahead of construction of the Steeles Avenue / Collector Road 1 sewers, then it is anticipated that lands east of Eighth Line South draining to the proposed 450mm diameter Steeles Avenue west trunk sewer will outlet to the Eighth Line Trunk Sewer at MH 20 (Steeles Avenue and Eighth Line South) via a proposed drop / vortex structure to the Eighth Line trunk sewer. Sewers along Collector Road 1 will outlet to the Eighth Line Trunk sewer along with sewers being diverted from HH#3 WWPS. This scenario is shown in Figure 5-5.

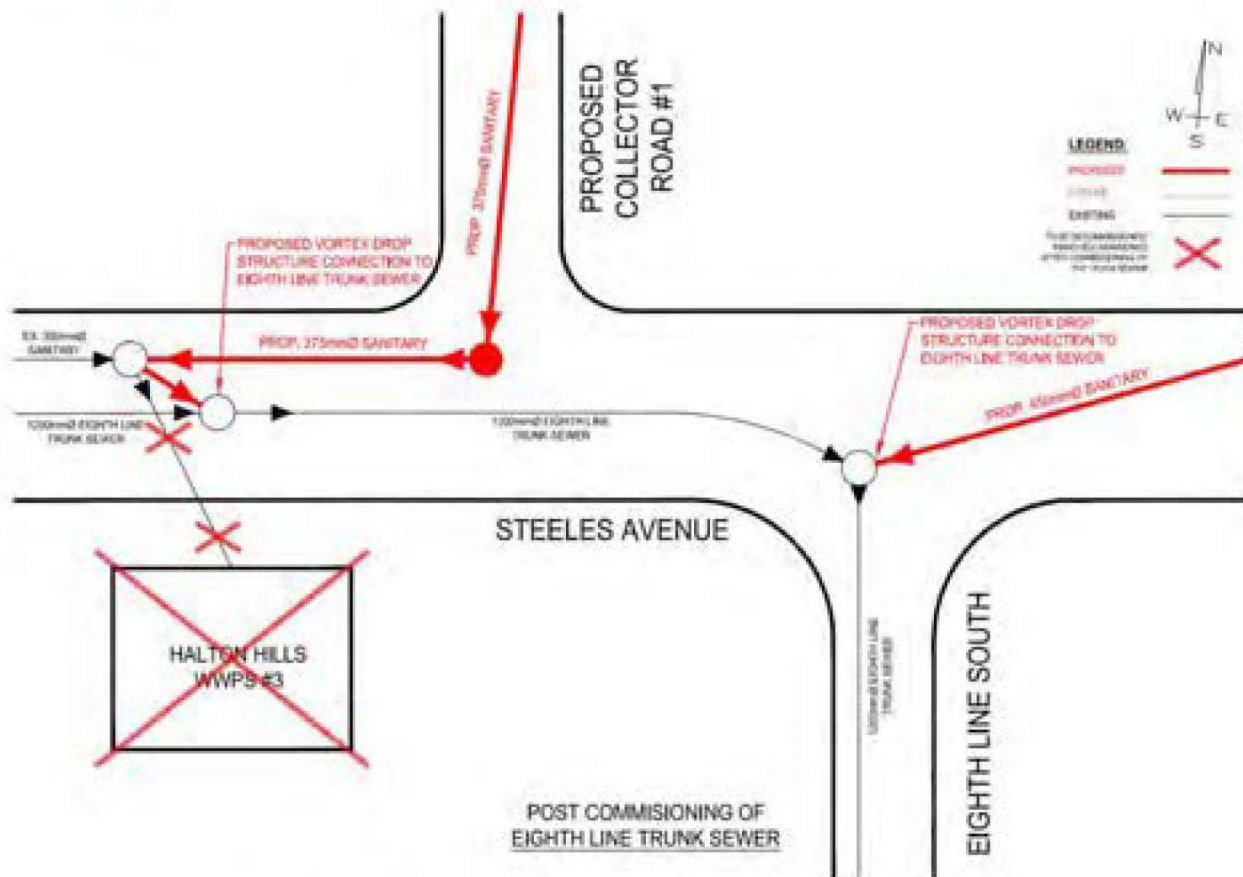


Figure 5-5: Proposed Outlet to Eighth Line Trunk Sewer

Trunk Sewer Alternative to HH#4 WWPS

Alternatively, a Steeles Avenue trunk sewer could be constructed to outlet to the proposed Eighth Line / Trafalgar Road Trunk Sewer. The Steeles Avenue trunk sewer could be constructed at a grade of 0.2% or greater to achieve the appropriate connection invert elevations with the Eighth Line Trunk Sewer. A proposed 600mm sewer could be installed by trenchless methods, within the Steeles Avenue right-of-way. Any future sewer through the GTA West Corridor would require extensive confirmation/coordination with any future GTA West highway structures designs (depth of footings conflict with watermain, etc.)

5.5.2 Incorporation with the Region's Existing Water and Wastewater Master Plan and Future Municipal Class Environmental Assessment Requirements

As noted in Section 4.5.2, the Area Servicing Plan has not been completed to Municipal Class EA requirements, instead building on the approved Approach 2 Sustainable Halton Water and Wastewater Master Plan. For recommended Schedule A, A+ and B projects, the final public notice for the Master Plan can also serve as the Notice of Completion for the Schedule B projects within the Master Plan (and the Secondary Plan Study Area).

Recommended wastewater servicing projects that may potentially cross the GTA West Corridor Environmental Assessment study or significant environmental features are to be considered

further at the project-specific approval stage for Revisions to Schedule B projects. It is anticipated that for recommended Schedule B wastewater projects, Revisions to Schedule B Projects will be required due to environmental implications of changes to the project (including GTA West Corridor crossing impacts) or even due to a delay in implementation. Modifications to recommended wastewater servicing projects identified as Schedule B projects can fulfill Class EA requirements as part of integration with Planning Act applications (including, but not limited to, future Zoning By-Law Amendments or Draft Plans of Subdivision). Public consultation can be combined with the required public consultation for Planning Act applications. Required Revisions to Schedule B Projects can reference or build on the previous Master Plan and Secondary Plan studies to demonstrate Phase 1 and Phase 2 of the Class EA process have been satisfied. A Revised Notice of Completion shall be issued with notification of the public's right to request a Part II order within the 30-day review period.

Incorporation of the Secondary Plan recommended projects into the Region's current Water, Wastewater and Transportation Master Plan may also provide for required fulfillment of Class EA requirements after completion and filing of the ongoing Master Plan.

5.5.3 Local Sewers

Local sewer alignments, depths grades and connections to potential Steeles Avenue trunk sewers have been developed based on the proposed land use concept and proposed road patterns and environmental constraints.

There is opportunity for development within the west sub-catchment to drain by local sewer directly to the existing HH#3 WWPS.

Under the ultimate servicing solution for the area, a local sewer crossing of the GTA West Transportation Corridor is not required. The GTA West Transportation Corridor follows a high ridge within the Study Area and conveying flows from east of the corridor to the west by gravity will drive the depth of the west gravity sewers significantly deeper (to greater than 10 metres depth).

Halton Region does not permit service connections to sanitary sewers of 450 mm diameter or greater unless a deviation from the Region's policy is approved by the Region. Development fronting Steeles Avenue will be required to connect to sections of local sewer that outlet to the proposed Region trunk sewer at manhole connections. Similar to for water service connections for Steeles Avenue fronting properties, alternate sanitary connections to local sewers along future internal roads will be considered, where topography and planned development phasing will allow for local sanitary sewer and service construction.

Local sewer alignments and connections to the proposed trunk sewers can be developed as part of future Draft Plans of Subdivision (or Zoning By-law Amendment) applications.

It is anticipated that local sewers running along future internal roads as well as along existing Town of Halton Hills and Halton Region roads including Ninth Line, Tenth Line, Winston Churchill and potentially Eighth Line and Steeles Avenue will be constructed through Developer front-end agreements.

As noted in Section 4.5.3, Halton Region currently has a policy to allow the Regional development-related projects to be designed and constructed by the development industry which may result in the construction of a project that was not identified in the current or prior years' capital budget.

5.5.4 Wastewater Collection Modelling Analysis

Halton Region's InfoSewer model was utilized to analyse the servicing scheme for the PGEA P2B lands under the two scenarios that consider pre and post commissioning of the Eighth Line Trunk Sewer. The model simulations analysed as part of the Secondary Plan analysis are summarized in Table 5-7.

Table 5-7: Overall InfoSewer Model Scenarios to Evaluate PGEA P2B Development

PGEA P2B Development Scenario	Halton Region InfoSewer Model to be Used	Note
PGEA P2B Build-out Ahead of 2025 Commissioning of Eighth Line Trunk Sewer	<ul style="list-style-type: none"> 2026 Growth Scenario HH#3 WWPS still in service Eighth Line Trunk Sewer not yet commissioned 	<ul style="list-style-type: none"> 2026 Growth Scenario will be used as it is closest growth timing to the 2025 Eighth Line Trunk Sewer commissioning and will not underestimate growth to downstream pumping stations. 2026 Growth Scenario will include PGEA P2B build-out as well as previously approved PGEA P1B build-out
PGEA P2B Build-out After 2025 Commissioning of Eighth Line Trunk Sewer	<ul style="list-style-type: none"> 2031 Growth Scenario Eighth Line Trunk Sewer commissioned HH#3 WWPS decommissioned 	<ul style="list-style-type: none"> 2031 Growth Scenario will be used to evaluate full buildout of PGEA lands under current OP/MP projections 2031 Growth Scenario will include PGEA P2B build-out as well as previously approved PGEA P1B build-out

Sanitary design sheets for flows from the PGEA P2B lands were also completed. The sanitary design sheets were based on engineering standards outlined in Halton Region's Water and Wastewater Linear Design Manual. The sanitary design sheets have been included in Appendix C.

6 Phasing of Servicing

6.1 General

Similar to for previous PGEA Secondary Plans, it is anticipated that the preference will be for the entire study area to come online under a single (initial) phase. This provides the flexibility for development of any parcels within the PGEA P2B lands, and the ability for development phasing/servicing that aligns with development interest.

However, the limited available connections to the Region's existing infrastructure will initially restrict the general phasing of development from Eighth Line progressing east toward the GTA West Corridor.

Development east of the potential GTA West Corridor will require interim servicing to proceed ahead of commissioning of the Region's planned water and wastewater infrastructure for the area.

Also to be considered is Halton Region's policy to allow the Regional development-related projects to be designed and constructed by the development industry which may result in the construction of a project that was not identified in the current or prior years' capital budget. Opportunity to incorporate the construction of required P2B DC watermain with development infrastructure will be reviewed as part of the ASP phasing considerations.

6.2 Water

6.2.1 West of GTA West Corridor

Initial development within the western half of the PGEA P2B lands can be serviced by local watermain connecting to the existing Halton Region 600mm diameter trunk watermain. The existing trunk watermain terminates at the east limit of the HH#3 WWPS property, approximately 50 metres away from the intersection of the Proposed Collector Road and Steeles Avenue. The 600mm Region main can be extended to the Proposed Collector Road and 300mm diameter local watermain constructed northeast along the collector road to service initial development.

Construction of a component of the Region's planned 600mm diameter watermain along Steeles Avenue to west of the GTA West Corridor, along with connection to / construction of the proposed 400mm diameter transmission watermain along Ninth Line will provide sufficient fire flow for all development west of the GTA West Corridor. The proposed 400mm diameter watermain along Collector Road 1 can provide sufficient fire flow to limited development ahead of the looped connection to the extension of the Steeles Avenue 600mm diameter – this can be further reviewed as part of detailed engineering for development applications in the area / fronting the west portion of Collector Road 1.

6.2.2 East of GTA West Corridor

Initial water servicing to development within the eastern portion of the P2B lands will require at minimum commissioning of the Region's planned 600mm diameter Steeles Avenue watermain. Design and construction of the 600mm diameter watermain was suspended to accommodate the EA studies associated with the GTA West Corridor. It is not anticipated that the Steeles Avenue trunk watermain project will be resumed ahead of the completion of the GTA West Corridor EA Study, and the watermain project will be considered further as part of the Region's ongoing Water and Wastewater Master Plan Update.

Design of the future watermain will require consideration for depth through the GTA West Corridor and potential conflict with possible future overpass structure footings and other structures. If the

preferred alternative developed from the GTA West Corridor EA leads to conceptual and detailed design of a new highway interchange south of Steeles Avenue (including associated overpass/underpass structures in the area of Steeles Avenue), then significant coordination between future water and wastewater infrastructure crossings will be required. It is unlikely that trunk watermain design and construction would be able to proceed ahead of design of potential MTO works in the area.

As the Steeles Avenue trunk watermain may not be constructed to meet the Secondary Plan's anticipated development timing, alternative initial supply to the eastern portion of the PGEA P2B lands was considered. Construction and commissioning of the proposed 300mm/400mm diameter watermain along the Proposed Collector Road (ahead of commissioning of the Steeles Avenue trunk watermain) was evaluated. The intent was to determine if a watermain along the Proposed Collector Road could provide sufficient available fire flow to potential east development areas fronting/near the Proposed Collector Road. Crossing of the GTA West Corridor lands is still required, including significant coordination with the MTO to complete the works. A dead-end 300mm/400mm diameter watermain along the Proposed Collector Road can provide approximately 90 L/s of available fire flow to areas east of the GTA West Corridor. This interim servicing and available fire flow will not meet the Region's minimum requirement of 15,000 L/min (250 L/s) available fire flow for Employment Lands.

Looped watermain across the GTA West Corridor will be required to provide sufficient available fire flow to proposed Employment Lands east of the GTA West Corridor. Even a scenario where the planned 600mm diameter watermain crossing from Steeles Avenue is relocated to the Proposed Collector Road cannot by itself provide the required available fire flow to employment lands located east of the GTA West Corridor. An additional 400mm diameter crossing is still required and can be located at the north limit of the PGEA P2B lands, or to align with a future road crossing through the Future Strategic Employment area lands (if planning timing and approvals can be aligned with development requirements within the east portion of the PGEA P2B lands).

Relocation of the planned 600mm diameter watermain crossing of the GTA West Corridor to north of Steeles Avenue to potentially provide for a less complex crossing of the highway corridor (clear of any potential interchange/overpass/underpass structures that may be constructed within the area of Steeles Avenue) can be explored. A proposed strategy that includes the required 600mm diameter and 400mm diameter trunk watermain crossings of the GTA West Corridor can be applied across various crossing locations throughout the study area. As the ultimate location of the watermain crossings is flexible, it can be adapted to the future requirements of study and development timing, approvals and constructability considerations.

Connection to the Region of Peel's system at Winston Churchill Boulevard could also be considered for supply to the east portion of the PGEA P2B lands. This alternative has not been reviewed with Halton Region or the Region of Peel but could be explored for feasibility if it is determined that potential delays to the timing of the planned Steeles Avenue Trunk Watermain commissioning will significantly impact planned development within the eastern portion of the PGEA P2B lands. At minimum, a connection to the Region of Peel system would require confirmation that the Peel water system has the capacity to supply the area, an agreement between Halton Region and the Region of Peel to supply water to the area and metered connection chamber(s). Halton Region and the Region of Peel do not currently have any existing agreements to provide water supply for normal operations. Feasibility analysis for this alternative is outside of the scope of this assignment. It has only been included as an alternative for

consideration based on the expectation that the timing of the Steeles Avenue trunk watermain may not meet the requirements of desired development timing for the PGEA P2B lands.

Proposed phasing and future considerations for interim water servicing of PGEA P2B lands is shown in Figure 6-1.



Water & Wastewater Area Servicing Plan
for the Premier Gateway Phase 2B

Water Infrastructure

- ▲ Pumping Station (PS)
- Storage
- Treatment
- Feeder Main (Diameter ≥ 400mm)
- Local Watermain (Diameter < 400mm)

General Features

- Alternate Watermain Alignment
- Premier Gateway Phase 2B
- Highway
- Major Road
- Local Road
- Property Parcel
- Municipal Boundary
- Preferred Route and Interchange Locations for GTA West Transportation Corridor Study

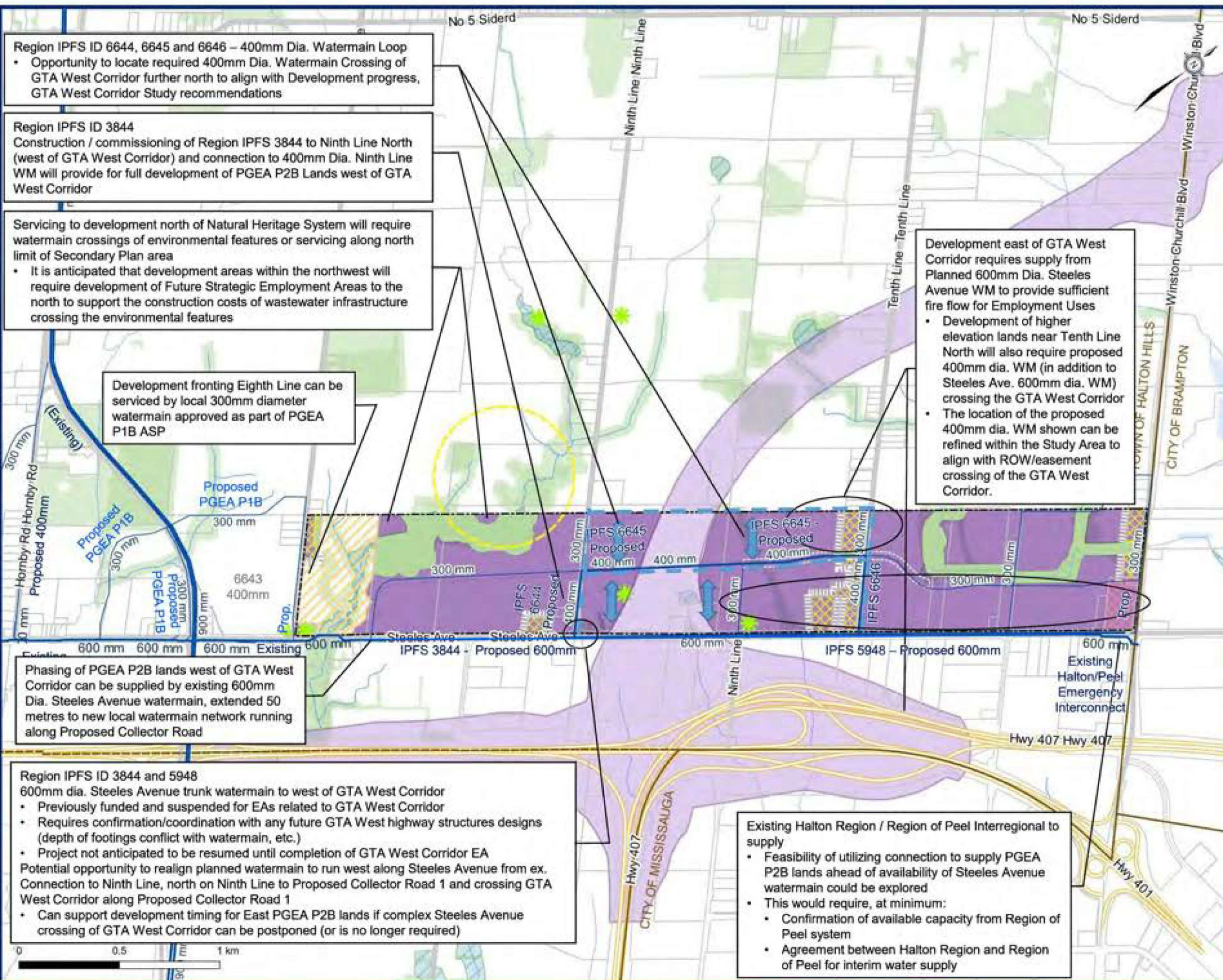
Preferred Land Use

- Natural Heritage System
- Prestige Industrial Area
- Residential Special Policy Area
- Supportive Commercial
- Subject to D09OPA22.001 As Per PL180499
- Headwater Drainage Feature
- Buffer for Existing Residential Uses
- Cultural Heritage Resource
- Minimum Distance Separation

Figure 6-1
Proposed Phasing and Future
Considerations
Interim Water Servicing
PGEA P2B Lands



September 2023
720010-004
Projection: EPSG:26917



6.3 Wastewater

6.3.1 Prior to Commissioning of Eighth Line/Trafalgar Trunk Sewer

Development within all PGEA lands, including P2B as well as P1B will need to consider the downstream pumping station capacity constraints prior to commissioning of the Eighth Line/Trafalgar Trunk Sewer. Existing pumping station constraints are discussed further in Section 5.4.1. Allocation will be required to ensure that peak wastewater flows for future development within the PGEA does not exceed the existing capacity at HH#2 and HH#3 WWPS.

6.3.2 West of GTA West Corridor

Development within the western half of the PGEA P2B lands can be serviced by local sanitary sewers initially outletting to the HH#3 WWPS and ultimately to the Eighth Line/Trafalgar Trunk Sewer after commissioning.

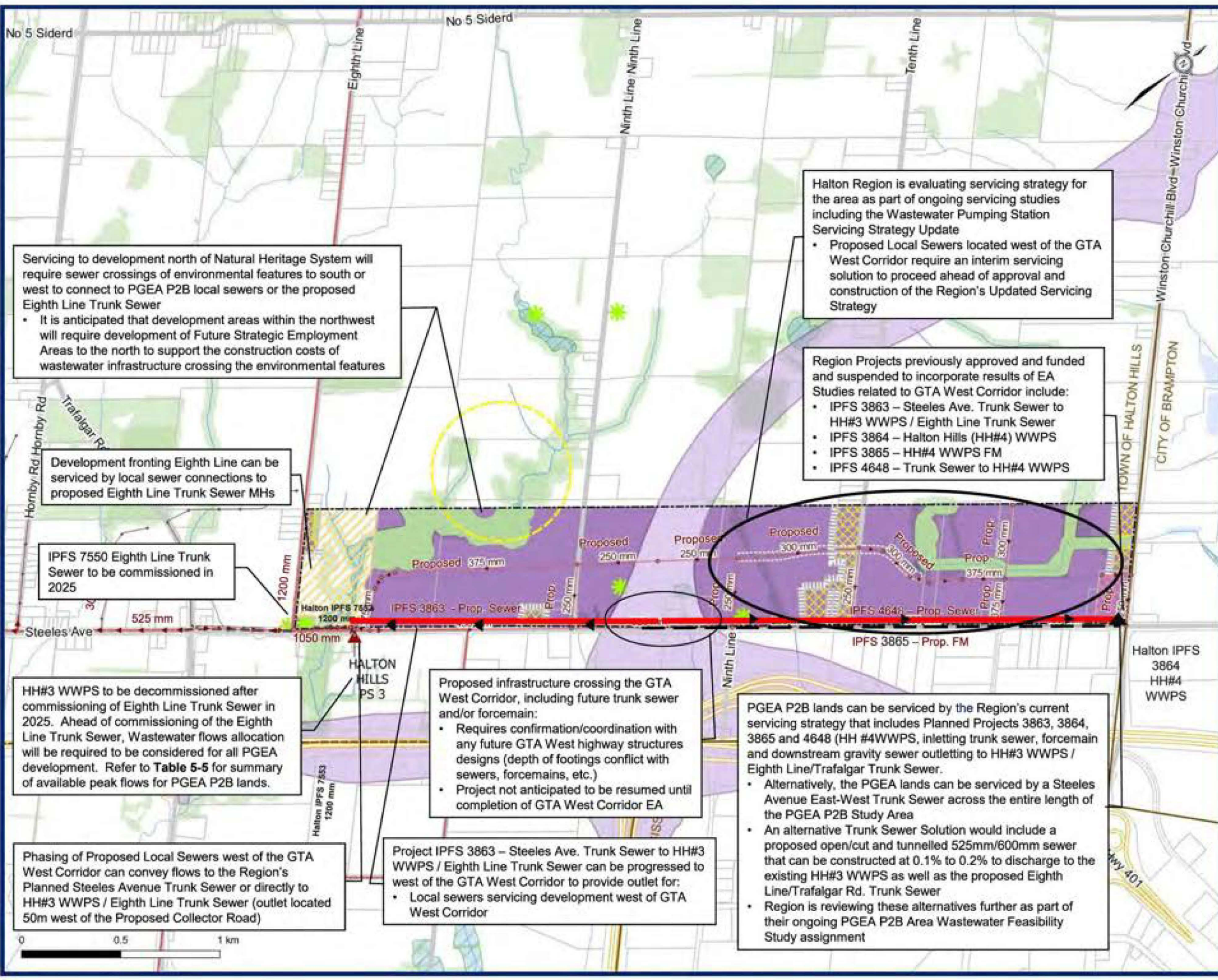
6.3.3 East of the GTA West Corridor

The Region is currently working to expedite the construction and commissioning of wastewater servicing to meet the development timing and needs of lands located east of the GTA West Corridor. The Region solution will incorporate the flexibility to cost effectively meet servicing needs of initial development within the Secondary Plan area.

Proposed phasing and future considerations for interim wastewater servicing of PGEA P2B lands is shown in Figure 6-2.



Water & Wastewater Area Servicing Plan for the Premier Gateway Phase 2B



- Wastewater Infrastructure**
- ▲ Sewage Pumping Station (SPS)
 - Trunk Sewer (diameter ≥ 450mm)
 - Local Sewer (diameter < 450mm)
 - Forcemain
- General Features**
- ▭ Premier Gateway Phase 2B
 - Highway
 - Major Road
 - Local Road
 - ▭ Property Parcel
 - ▭ Municipal Boundary
 - ▭ Preferred Route and Interchange Locations for GTA West Transportation Corridor Study
- Preferred Land Use**
- Natural Heritage System
 - Prestige Industrial Area
 - Residential Special Policy Area
 - Supportive Commercial
 - Subject to D09OPA22.001 As Per PL180499
 - Headwater Drainage Feature
 - ▭ Buffer for Existing Residential Uses
 - ★ Cultral Heritage Resource
 - ▭ Minimum Distance Separation

Servicing to development north of Natural Heritage System will require sewer crossings of environmental features to south or west to connect to PGEA P2B local sewers or the proposed Eighth Line Trunk Sewer

- It is anticipated that development areas within the northwest will require development of Future Strategic Employment Areas to the north to support the construction costs of wastewater infrastructure crossing the environmental features

Development fronting Eighth Line can be serviced by local sewer connections to proposed Eighth Line Trunk Sewer MHs

IPFS 7550 Eighth Line Trunk Sewer to be commissioned in 2025

HH#3 WWPS to be decommissioned after commissioning of Eighth Line Trunk Sewer in 2025. Ahead of commissioning of the Eighth Line Trunk Sewer, Wastewater flows allocation will be required to be considered for all PGEA development. Refer to Table 5-5 for summary of available peak flows for PGEA P2B lands.

Phasing of Proposed Local Sewers west of the GTA West Corridor can convey flows to the Region's Planned Steele's Avenue Trunk Sewer or directly to HH#3 WWPS / Eighth Line Trunk Sewer (outlet located 50m west of the Proposed Collector Road)

Proposed infrastructure crossing the GTA West Corridor, including future trunk sewer and/or forcemain:

- Requires confirmation/coordination with any future GTA West highway structures designs (depth of footings conflict with sewers, forcemains, etc.)
- Project not anticipated to be resumed until completion of GTA West Corridor EA

Project IPFS 3863 – Steele's Ave. Trunk Sewer to HH#3 WWPS / Eighth Line Trunk Sewer can be progressed to west of the GTA West Corridor to provide outlet for:

- Local sewers servicing development west of GTA West Corridor

Halton Region is evaluating servicing strategy for the area as part of ongoing servicing studies including the Wastewater Pumping Station Servicing Strategy Update

- Proposed Local Sewers located west of the GTA West Corridor require an interim servicing solution to proceed ahead of approval and construction of the Region's Updated Servicing Strategy

Region Projects previously approved and funded and suspended to incorporate results of EA Studies related to GTA West Corridor include:

- IPFS 3863 – Steele's Ave. Trunk Sewer to HH#3 WWPS / Eighth Line Trunk Sewer
- IPFS 3864 – Halton Hills (HH#4) WWPS
- IPFS 3865 – HH#4 WWPS FM
- IPFS 4648 – Trunk Sewer to HH#4 WWPS

PGEA P2B lands can be serviced by the Region's current servicing strategy that includes Planned Projects 3863, 3864, 3865 and 4648 (HH #4WWPS, inletting trunk sewer, forcemain and downstream gravity sewer outletting to HH#3 WWPS / Eighth Line/Trafalgar Trunk Sewer.

- Alternatively, the PGEA lands can be serviced by a Steele's Avenue East-West Trunk Sewer across the entire length of the PGEA P2B Study Area
- An alternative Trunk Sewer Solution would include a proposed open/cut and tunnelled 525mm/600mm sewer that can be constructed at 0.1% to 0.2% to discharge to the existing HH#3 WWPS as well as the proposed Eighth Line/Trafalgar Rd. Trunk Sewer
- Region is reviewing these alternatives further as part of their ongoing PGEA P2B Area Wastewater Feasibility Study assignment

Figure 6-2
Proposed Phasing and Future Considerations
Interim Wastewater Servicing
PGEA P2B Lands

7 Cost Estimates

7.1 Water and Wastewater Servicing Cost Estimates

7.1.1 Water and Wastewater DC Projects Cost Estimates Approach

As noted in Section 4.2.3 and Section 5.2.1, Region water and wastewater development charges projects for the area were previously funded and suspended to consider the previous and current EA Studies related to the GTA West Corridor.

Planned water and wastewater servicing for the area has been considered as part of ongoing Halton Region servicing studies, including the Region's Integrated Growth Management Study and Wastewater Pumping Station Servicing Strategy Update. The water and wastewater servicing strategy for the area will be updated as part of the Region's ongoing Water and Wastewater Master Plan Update project.

The cost estimate unit rates are based on the Region's 2022 Development Charges Update (Water/Wastewater) and are generally inline with the Association for the Advancement of Cost Engineering (AACE) Class 4 estimates. AACE Class 4 estimates are:

- completed in support of project planning and initial preliminary design;
- are based on sufficient knowledge of site conditions adequate to identify high level risk;
- based on historical costs of similar projects and historical average unit costs for work activities; and,
- have an expected accuracy range of -20% to +30%.

For the Class 4 cost estimates, the risk related to construction considerations including environmental feature crossings, pumping station decommissioning and MTO coordination (as well as other approval authorities and utilities) is intended to be incorporated directly into the unit rate. The costs have been included primarily for reference purposes.

The cost estimate approach is detailed further in the 2022 Development Charges Update – Water/Wastewater Cost Estimation Approaches Technical Memorandum, October 2021, included in Appendix D.

7.1.2 Water Projects

Updated cost estimates for the Region's anticipated water development charges projects and local watermain required to service the PGEA P2B lands have been developed based on the unit rates included in the Region's 2022 Water and Wastewater DC Update. Total cost estimates are summarized in Table 7-1.

**Table 7-1: Cost Estimate for Proposed Development Charges Water Infrastructure
Required for PGEA P2B Development**

Water	Total Project Construction Cost Estimate (2021\$)	Total Project Cost Estimate (Including Property, Engineering and Associated Construction Fees) (2021\$)
Halton Region Project 3844 and Halton Region Project 5948 - 600 mm diameter watermain on Steeles Avenue, from Eighth Line to Peel Interregional Connection at Winston Churchill Boulevard (Approximately 3.8 km of watermain)	\$18.6 M	\$25.8 M
Halton Region Project 6644 – 300 metres of 400mm diameter watermain on Ninth Line from Steeles Avenue to PGEA P2B Proposed Collector Road	\$0.5 M	\$0.7 M
Halton Region Project 6645 – 1.5 km of 400mm diameter watermain on PGEA P2B Proposed Collector Rod from Ninth Line to Tenth Line (with potential crossing of GTA West Corridor)	\$5.6 M	\$7.7 M
Halton Region Project 6646 – 400 metres of 400mm diameter watermain on Tenth Line from Steeles Avenue to PGEA P2B Proposed Collector Road	\$0.6 M	\$0.8 M
Sub-Total Halton DC Projects Costs	\$25.3 M	\$35.0 M
Local Watermain Costs	\$5.7 M	\$7.8 M
Total Water Servicing Costs for HH PGEA P2B Area	\$31.0 M	\$42.8 M

Detailed cost estimates for the proposed local 300mm diameter watermain running along the proposed collector roads have been included in Appendix D. Cost estimates for local watermain have also been based on the unit rates included in the Region's 2022 Water and Wastewater DC Background Study. Distribution watermain cost estimates include an estimate of \$5,000/150m (\$33.33/m) construction cost estimate for hydrants based on Halton Region design standards and recent comparable construction pricing.

7.1.3 Wastewater Projects

Updated cost estimates for the Region's anticipated wastewater development charges projects and local sewers required to service the PGEA P2B lands have been developed based on the unit rates included in the Region's 2022 Water and Wastewater DC Update. Cost estimates are project based, recognizing that construction and commissioning of individual servicing components will be required to be adaptable to meet associated development phasing within the PGEA P2B lands.

Total cost estimates are summarized in Table 7-2.

Table 7-2: Cost Estimate for Proposed Development Charges Wastewater Infrastructure Required for PGEA P2B Development

Component	Total Project Construction Cost Estimate (2021\$)	Total Project Cost Estimate (Including Property, Engineering and Associated Construction Fees) (2021\$)
Halton Region Project 3863 – Trunk Sewer on Steeles Avenue from East of Ninth Line to Eighth Line (HH #3 WWPS / 7553 – Eighth Line Trunk Sewer)	\$1.8 M (Pre-Eighth Line Trunk Commissioning) \$1.0 M (Post-Eighth Line Trunk Commissioning)	\$2.5 M (Pre-Eighth Line Trunk Commissioning) \$1.4 M (Post-Eighth Line Trunk Commissioning)
Halton Region Project 3864 - Halton Hills (HH) #4 WWPS at intersection of Steeles Avenue and Winston Churchill Boulevard	\$8.0 M	\$11.1 M
Halton Region Project 3865 – Forcemain(s) on Steeles Avenue from HH #4 WWPS to the Proposed Halton Region Trunk Sewer (Project 3863) (Estimated as 2x300mm Dia. Forcemains (from Stantec's January 2013 Preliminary Design Report))	\$8.3 M	\$11.6 M
Halton Region Project 3865 – Trunk Sewer on Steeles Avenue from East of Ninth Line HH#4 WWPS (Project 3864) (Estimated firm capacity of 141 L/s to service PGEA P2A + P2B Lands East of GTA West Corridor (from Stantec's January 2013 Preliminary Design Report))	\$3.0 M	\$4.1 M
Sub-Total Halton DC Projects Costs	\$20.3 M – \$21.1 M	\$28.2 M – \$29.3 M
Local Sewer Costs	\$10.6 M	\$14.6 M
Total Wastewater Servicing Costs for HH PGEA P2B Area	\$30.9 M - \$31.8 M	\$42.8 M - \$43.9 M

Cost estimates for the HH#4 WWPS, inlet sewer, forcemain(s) and downstream gravity sewer are based on approved designs and updated based on the Region's 2022 DC cost estimates methodology. Ultimately, the projects may be updated to include more (or less) catchment area as part of the Region's PGEA P2B Servicing Strategy Feasibility Study as well as the ongoing Water and Wastewater Master Plan Update. Projects 3863 – 450mm/525mm diameter trunk sewer has been conservatively estimated using the 2022 DC unit rate for 525mm diameter open-cut gravity sewer for the entire length (the location for the increase in pipe size will be confirmed as part of the updated of the detailed design of the gravity sewer for the area).

The ultimate location of the proposed pumping station and upstream catchment area can have significant impacts on the cost estimate, including adjusted requirements for wet well depth and pump and energy requirements. The 2022 DC cost estimate approach for pumping stations is based on peak flow only. Additionally, if a temporary pumping station is constructed by developers ahead of commissioning of the Region's updated wastewater servicing strategy for the area, then there may be cost efficiencies if construction of a prefabricated pumping station is determined to be the preferred alternative.

The January 2013 Preliminary Design Report for HH#4 WWPS estimated the SPS works only to cost \$4.6 M (2013\$). A SPS servicing only the P2B lands east of the GTA West Corridor is

estimated to cost \$5.3 M (Region DC Formula - $\$19,420.36 \times 73 \text{ L/s}$ (required firm capacity) + \$1,982,631).

Cost estimates for the Eighth Line Trunk Sewer and the HH #3 WWPS decommissioning have not been included in the cost estimate. These projects benefit the P2B lands and the broader development area but are not considered to be required for initial development of the PGEA P2B lands and will be completed independent of the development of the Study Area.

The cost estimate for an equivalent trunk sewer servicing the PGEA P2B lands, along Steeles Avenue from Winston Churchill Boulevard, crossing the GTA West Corridor and outletting to the existing HH#3 WWPS / Eighth Line Trunk Sewer is shown in Table 7-3.

Table 7-3: Cost Estimate for Alternative Equivalent Tunnelled Steeles Avenue Trunk Sewer Required for PGEA P2B Development

Component	Total Project Construction Cost Estimate (2021\$)	Total Project Cost Estimate (including Property, Engineering and Associated Construction Fees) (2021\$)
Equivalent Steeles Avenue trunk sewer from Winston Churchill Boulevard, crossing the GTA West Corridor, and outletting at the Ex. HH#3 WWPS / Proposed Eighth Line Trunk Sewer (Assumed 600mm Dia. to align with smallest diameter available microtunnelling boring machine (MTBM))	\$50.8 M	\$70.6 M

Detailed cost estimates are included in Appendix D.

8 Conclusion

8.1 General

The PGEA P2B ASP confirms that the planned Employment Area can ultimately be serviced by the Region's proposed upgrades to water and wastewater linear infrastructure along Steeles Avenue, Ninth Line and Tenth Line and crossing the GTA West Corridor.

The Region's previously approved and funded water and wastewater Master Plan projects, including the Steeles Avenue trunk watermain and the HH#4 WWPS, forcemain and downstream trunk sewer remain suspended while the Province's GTA West Corridor is completed.

Additionally, the Region's planned water and wastewater projects for the area are being considered further as part of studies supporting the Regional Municipal Comprehensive Review, including the Integrated Growth Management Strategy and the Wastewater Pumping Station Servicing Strategy. Ultimately the servicing strategy for the area will be updated as part of the Region's PGEA P2B Servicing Strategy Feasibility Study as well as the Water and Wastewater Master Plan Update. Recommendations from the PGEA P2B ASP will help inform the recommended water and wastewater projects for the area.

Halton Region does currently have a policy to allow the Regional development-related projects to be designed and constructed by the development industry - which may result in the construction of a project that was not identified in the current or prior years' capital budget. Opportunity to incorporate the construction of required PGEA P2B water and wastewater infrastructure by the development industry can be considered as part of future Zoning By-law Amendments or Draft Plans of Subdivision.

8.2 Water

Ultimate water servicing to the PGEA P2B lands generally requires commissioning of the Region's planned 600mm diameter Steeles Avenue trunk watermain with a 400mm diameter watermain loop along Ninth Line, the Proposed Collector Road and Tenth Line as well as 300mm diameter watermain running along the Proposed Collector Road with connections to the Steeles Avenue trunk at key locations. This will provide the future Employment Lands with suitable operating pressures and sufficient available fire flow.

Employment Lands located west of the GTA West Corridor can initially be serviced by 300mm diameter watermain network connected to a short (50 metre) extension of the 600mm diameter Steele Avenue trunk main that terminates east of Eighth Line and Sixteen Mile Creek. Buildout of the Planned 600mm diameter Steeles Avenue watermain to west of the GTA West Corridor combined with the 400mm diameter Ninth Line transmission watermain will provide sufficient available fire flow to all development located west of the GTA West Corridor.

Prior to development of lands located east of the GTA West Corridor, construction of the Steeles Avenue trunk watermain to east of the GTA West Corridor will be required. The future 300mm diameter watermain network can be supplied from the Steeles Avenue trunk watermain. A looped watermain, including the planned 600mm diameter watermain and an additional 400mm diameter watermain crossing of the GTA West Corridor will be required prior to development of the east portion of the PGEA P2B lands. The only existing Halton Region water infrastructure located east of the GTA West Corridor is an emergency interregional connection to the Region of Peel's system, located at Steeles Avenue and Winston Churchill Boulevard. If uncertainty around timing of the GTA West Corridor study and associated impact on Region water infrastructure construction for the area will significantly impede development plans for the east portion of the

PGEA P2B lands, then feasibility of alternative water supply to the area will need to be further explored with Halton Region. This could include relocation of the planned Steeles Avenue 600mm diameter watermain to north of the potential GTA West Corridor interchange/overpass/underpass structures within the area of Steeles Avenue. The ultimate location of the watermain crossings is flexible and can be adapted to the future requirements of study and development timing, approvals and constructability considerations.

8.3 Wastewater

Ultimate wastewater servicing to the PGEA P2B lands will require a trunk sewer running west from the GTA West Corridor to the existing HH#3 WWPS / Eighth Line/Trafalgar Trunk Sewer and a pumping station and forcemain to service the Employment Lands located east of the GTA West Corridor (or an equivalent solution that can include a deeper trunk sewer running across the entire width of Steeles Avenue from Winston Churchill Boulevard to Eighth Line).

Ahead of the anticipated 2025 commissioning of the proposed Eighth Line/Trafalgar Trunk Sewer, there are capacity constraints at the downstream HH#3 WWPS and HH#2 WWPS that must be considered for allocation of proposed development across all PGEA lands (including P2B and previously approved P1B lands).

Wastewater flows from future development west of the GTA West Corridor can be conveyed by local gravity sewer along internal roads to the outlet at the existing HH#3 WWPS. Construction of the planned Steeles Avenue trunk sewer to west of the GTA West Corridor can also be progressed to service the entire PGEA 2B lands west of the GTA West Corridor.

Lands located east of the GTA West Corridor will require the ultimate servicing solution to be developed, constructed and commissioned by the Region (including the updated HH#4 WWPS and forcemain or equivalent proposed trunk sewer).

The Region is currently undertaking a Feasibility Study and Provisional Municipal Class EA to support development of an updated servicing strategy that can be constructed to align with the development timing needs of initial phases of development (as well ultimate phasing for the broader service area). This ASP will inform the Region's servicing strategy (the Region's feasibility study is anticipated to be completed in Summer 2023).

**ONTARIO LAND TRIBUNAL - CASE
NO(S): OLT-24-000369 - MAY 08,
2025**

Ontario Land Tribunal
Tribunal ontarien de l'aménagement
du territoire



ISSUE DATE: May 08, 2025

CASE NO(S).:

OLT-24-000369

PROCEEDING COMMENCED UNDER subsection 17(36) of the *Planning Act*, R.S.O. 1990, c. P.13, as amended

Appellant:	2607503 Ontario Inc.
Appellant:	First Gulf Halton Steeles Limited and Sun Life Assurance Company of Canada
Subject:	Proposed Official Plan Amendment
Description:	To facilitate development of land for employment purposes
Reference Number:	OPA 50
Property Address:	Lands within the Premier Gateway Employment Area Phase 2B Secondary Plan
Municipality/UT:	Halton Hills/Halton
OLT Case No.:	OLT-24-000369
OLT Lead Case No.:	OLT-24-000369
OLT Case Name:	First Gulf et al. v Halton (Region)

Heard: May 7, 2025 by Video Hearing

APPEARANCES:

Parties

2607503 Ontario Inc.

First Gulf Halton Steeles Limited and Sun Life Assurance Company of Canada

Counsel

R. Cheeseman

M. Bassani

Regional Municipality of Halton

K. Yerxa

Town of Halton Hills

S. Floras

**MEMORANDUM OF ORAL DECISION DELIVERED BY S. BRAUN ON MAY 7, 2025
AND ORDER OF THE TRIBUNAL**

[Link to Order](#)

BACKGROUND

[1] This was a hearing to consider a proposal to settle appeals by 2607503 Ontario Inc. (“Sigma Group”) and First Gulf Halton Steeles Limited and Sun Life Assurance Company of Canada (“First Gulf”), (collectively, “Appellants”), arising from the decision of the Regional Municipality of Halton (“Region”) to approve Official Plan Amendment 50 (“OPA 50”).

[2] OPA 50 is a Secondary Plan to establish a comprehensive planning framework for Phase 2B of the urban employment area identified in the Town of Halton Hills (“Town”) Official Plan (“Town OP”) as the Premier Gateway Employment Area. The Secondary Plan Area is approximately 254 hectares in size, bounded by Steeles Avenue to the south, Winston Churchill Boulevard to the east, Eighth Line to the west and a line parallel to, and approximately 0.6 kilometres north of, Steeles Avenue to the north. The Secondary Plan Area represents a significant component of the Town’s 2031 future employment growth and is intended to help ensure the long-term financial stability of the Town.

[3] OPA 50 was adopted by Town Council on October 10, 2023 through Zoning By-law No. 2023-0089 and was approved, with modifications, by the Region on March 5, 2024. The appeals were submitted on March 22, 2024, and thereafter, changes to the *Planning Act* (“Act”) removed planning responsibilities from the Region. The Region remained a Party to these proceedings and participated in discussions aimed at

resolving the appeals, which ultimately led to the settlement proposal presently before the Tribunal.

THE HEARING

[4] Bronwyn Parker, a Registered Professional Planner and Director of Planning Policy for the Town, was qualified by the Tribunal to provide land use planning opinion evidence. She provided a sworn Affidavit and oral evidence in support of the proposed settlement.

[5] For context, Ms. Parker noted that the Secondary Plan Area is bisected by the Highway 413 Focused Analysis Area and Northwest GTA Transmission Corridor (“Highway Corridor”), and as such, the policies of OPA 50 specify that any development in the area requires approval of the Ministry of Transportation to avoid interference with the Highway Corridor.

[6] The Tribunal heard that the appeals of OPA 50 by Sigma Group and First Gulf centered primarily around a future east-west collector road to be located approximately 300 metres north of Steeles Avenue, extending west from Winston Churchill Boulevard, across the Highway Corridor and through the north section of the First Gulf lands, before turning southerly through the Sigma Group lands and ending at Steeles Avenue east of East Sixteen Mile Creek. In the view of Sigma Group and First Gulf, the location of that collector road would divide their lands, thereby precluding intended development of those lands with proposed warehouse uses.

[7] However, Ms. Parker explained that, following the adoption of OPA 50, the urban boundaries in the Region and Town Official Plans were expanded to include additional employment areas through Regional Official Plan Amendment 49 (“ROPA 49”) as part of Bill 162, *Get it Done Act* (“Bill 162”). This provided a reason for the Town to revisit the location of the east-west collector road, independent of the issues raised in the two appeals.

[8] Following the passage of Bill 162, the Town recommended study of an alternative alignment of the collector road, and to that end, the Town, Region, and Appellants formed a working group to arrive at terms of reference for traffic and servicing studies. As part of those studies, concept plans provided by the Appellants showing the proposed warehouse development were considered.

[9] The traffic study considered an alternative alignment of the east-west collector road, curving to the north in the vicinity of the future Highway 413, continuing west to Eighth Line and Trafalgar Road via another collector road, to be established in the additional ROPA 49 employment area lands. While a specific alignment for that collector road was not identified between Eighth Line and Ninth Line, an approximate envelope was established which is capable of accommodating a range of potential alignments. Overall, the study concluded that the foregoing would result in acceptable traffic operations, provided that traffic signals are installed or turn lanes constructed when intersections are developed. A servicing study similarly concluded the feasibility and availability of alternative water and wastewater servicing from Steeles Avenue, without the proposed east-west collector road.

[10] The study conclusions were presented to Town Council in a confidential solicitor's report on March 24, 2025, with Town staff indicating that the submitted studies were acceptable, subject to minor edits with final studies to be submitted to the Town and Region for acceptance. Town Council endorsed the report and issued a direction to support the implementation of the study's conclusions and settlement of the appeals of OPA 50. Regional staff received the same direction.

[11] Final versions of the traffic and servicing studies were circulated on April 24, 2025, which, *inter alia*, addressed Region and Town staff concerns with access points for development on the Appellants' lands as shown on the concept plans. Region and Town staff required the inclusion of a caveat making clear that the concept plans are not approved in the settlement of these appeals, that only one full access onto Ninth Line may be considered by the Region, and any access onto Regional Roads within OPA 50 will be required to comply with a policy in OPA 50 (H.8.9.2.7) which addresses future access to the Regional Road Network.

[12] Ms. Parker testified that all of the modifications proposed to OPA 50 (as reflected in Attachment 1 hereto) address the issues raised in the two appeals and incorporate all aspects of OPA 50, as approved. She opined that the most important modification is the addition of a new policy (H8.9.2.2), which discusses the technical analysis undertaken in relation to the realignment of the collector road and the conclusions of same as outlined at paragraphs [7] and [8] above, and specifies that the collector road envelope will be subject to further study as part of a future Secondary Plan exercise for the additional ROPA 49 employment area lands.

[13] Overall, she opined that the proposed modifications to OPA 50 meet all necessary legislative tests, noting in particular, consistency with all employment policies found in s. 2.8 of Provincial Planning Statement, 2024. She further noted that the proposed settlement assists in the integration and coordination of planning for transportation and servicing and furthers the purpose and intent of the Town OP.

[14] On the uncontested planning evidence presented, the Tribunal finds the proposed modifications to OPA 50 are representative of good planning in the public interest and meet all necessary legislative tests. In making its findings the Tribunal has given regard to the original decision of the Region, as well as the subsequent decisions of the Town and the Region to endorse the proposed settlement.

ORDER

[15] **THE TRIBUNAL ORDERS THAT** the appeals by First Gulf Halton Steeles Limited and Sun Life Assurance Company of Canada and 2607503 Ontario Inc. are allowed in part and Official Plan Amendment 50 is hereby modified and approved in accordance with **Attachment 1** to this Order, and that Official Plan Amendment 50, as modified, is hereby in full force and effect.

"S. Braun"

S. BRAUN
VICE-CHAIR

Ontario Land Tribunal

Website: www.olt.gov.on.ca Telephone: 416-212-6349 Toll Free: 1-866-448-2248

The Conservation Review Board, the Environmental Review Tribunal, the Local Planning Appeal Tribunal and the Mining and Lands Tribunal are amalgamated and continued as the Ontario Land Tribunal ("Tribunal"). Any reference to the preceding tribunals or the former Ontario Municipal Board is deemed to be a reference to the Tribunal.

ATTACHMENT 1

**Town of Halton Hills
Premier Gateway
Employment Area
Phase 2B Secondary Plan**



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PREMIER GATEWAY EMPLOYMENT AREA PHASE 2B SECONDARY PLAN

H8.1 PURPOSE

The purpose of the Premier Gateway Employment Area Phase 2B Secondary Plan is to develop land use designations and policies for a portion of the Premier Gateway Employment Area needed to accommodate employment lands needs for the 2031 planning horizon. The Secondary Plan will establish a more comprehensive planning framework for the area in support of the general policy framework provided by the Official Plan. The policies will guide new employment uses as well as address the interface between new uses and the existing uses and Natural Heritage System. The Plan incorporates the recommendations of the Phase 2B Scoped Subwatershed Study including the provision of updated Natural Heritage System mapping. It integrates transportation system updates, servicing policies, and land use compatibility directions, and addresses cultural heritage resources, surrounding agricultural uses and urban design. It is also intended to help ensure the long-term financial sustainability of the Town.

H8.2 STRUCTURE OF THE SECONDARY PLAN

All of this part of the document, consisting of the following text and Schedule H8 constitutes Amendment No. 50 to the Official Plan for the Town of Halton Hills. The appendices contain additional information that supports the policies of this Plan but do not form an operative part of the Plan.

H8.3 LOCATION

The Secondary Plan affects all lands as shown on Schedule H8, which is generally bounded by:

- a) Steeles Avenue to the south
- b) Winston Churchill Boulevard to the east
- c) Eighth Line to the west
- d) A line parallel to, and approximately 0.6 kilometers north of, Steeles Avenue to the north.

The Secondary Plan area contains approximately 254 hectares (628 acres) of land.

H8.4 VISION STATEMENT

The Secondary Plan Area will be a high quality, comprehensively planned, prestige employment area that forms a key component of the Premier Gateway Employment Area. The area will provide the majority of the Town's future employment growth. It will diversify and enhance the Town's economy, increase assessment, and expand local employment opportunities. Development will leverage Halton Hills' strategic location in close proximity to major transportation networks and shipping hubs.

The area will provide quality prestige employment uses that align with the Town's strategic objectives. It will be a leading and thriving Green Economy Innovation and Employment Hub that focuses on advanced manufacturing, food and beverage processing, clean technology and renewable energy, research and development, office, major hospitality and tourism, and related employment uses. The permitted uses will reinforce a more intensive economic base that will amplify synergies between the Town's economic development, land use planning and climate change objectives.

Visually attractive and sustainable development that reflects the Town's leadership in climate change and its 2030 Net-Zero target will improve resiliency. The refined and enhanced *Natural Heritage System* will further support the Town's climate change and sustainability programs.

Access and connectivity will be improved, and attractive and comfortable streetscapes will encourage active transportation and support future public transit. Development should proceed on full municipal services that meet the needs of businesses and existing residents.

H8.5 GUIDING PRINCIPLES

- H8.5.1 Prioritize the development of prestige employment uses that diversify and enhance the Town's non-residential assessment base, generate higher density employment, support economic resiliency, raise the Town's economic competitiveness, and provide expanded local employment opportunities, aligned with the strategic objectives of the Town's Economic Development and Tourism Strategy, Foreign Direct Investment (FDI) Attraction Strategy and Business Concierge program, as updated.
- H8.5.2 Attract and serve as the primary location for the Town's target sectors of advanced manufacturing, food and beverage processing, clean technology and renewable energy, and agri-business as well as higher density employment uses such as, research and development, office, major hospitality and tourism related uses.
- H8.5.3 Encourage eco-industrial activities including, but not limited to:

Premier Gateway Employment Area Phase 2B Secondary Plan, September 2023

- a. Mutually supportive business relationships between industries through resource sharing;
 - b. Re-use of industrial by-products;
 - c. Sharing of services and facilities, including transportation and demand management strategies;
 - d. Sustainable and energy efficient net-zero climate resilient building infrastructure that focuses on renewable energy and co-generation opportunities including district energy, and supports an integrated energy distribution system;
 - e. Integration of climate change mitigation and adaptation techniques; and
 - f. Leveraging partnerships between public and private organizations that support the area emerging as a major regional Green Economy Innovation and Employment Hub.
- H8.5.4 Attract a post-secondary education institution to locate in the area to support, generate and drive innovation, research and workforce development in order to support a leading and thriving Green Economy Innovation and Employment Hub.
- H8.5.5 Encourage development of multi-tenant industrial malls that can accommodate tenants of various sizes in order to support the growth of emerging and innovative uses.
- H8.5.6 Support accessory retail and service commercial uses that assist existing and future businesses and reinforce a diversified economic base.
- H8.5.7 Facilitate the creation of high quality development and an attractive public realm through the use of urban design guidelines and other measures, including zoning regulations. Ensure development is consistent with the other phases of the Premier Gateway Employment Area in order to provide a cohesive and coordinated employment area. Allow individual areas to develop their own specific character while ensuring they fit with the general vision for the Employment Area.
- H8.5.8 Encourage high standards of built form, site design and landscape treatments with a strong visual identity in a sustainable environment that supports the area's gateway location. Ensure a consistent level of quality on both public and private lands with the highest quality of development at key locations.
- H8.5.9 Collaborate with the Regions of Halton and Peel to ensure that the Winston Churchill Boulevard and Steeles Avenue intersection reflects its gateway role by promoting a distinct visual presence and sense of arrival as well as providing a comfortable and engaging environment.

- H8.5.10 Strengthen the *Natural Heritage System*, located within both the Sixteen Mile Creek and Credit River watersheds, by protecting, enhancing and where possible restoring the natural heritage features and functions, and by connecting it with the *Natural Heritage System* within the Town.
- H8.5.11 Respect the existing low density residential and institutional uses within and adjacent to the Secondary Plan area, recognize their right to continue to exist and allow office and home-based businesses. Prohibit new residential uses and restrict institutional uses in order to create a stable operating environment for businesses. Provide appropriate buffering to minimize impacts on sensitive land uses from adjacent employment uses, while recognizing that the primary and long term use of the area is for employment.
- H8.5.12 Collaborate with the Region of Halton, Halton Hills Community Energy Corporation and other private and public stakeholders to ensure the expeditious provision of soft and hard infrastructure including a leading edge telecommunication service that is required to attract and retain innovative employment uses in the Green Economy Innovation and Employment Hub and to supply municipal services to existing residential uses in advance of, or in conjunction with, new development.
- H8.5.13 Recognize that the proposed Highway 413 which traverses through the Secondary Plan area may enhance exposure and visibility but may also impact internal connectivity and existing sensitive land uses. Ensure transportation, servicing and land use frameworks do not preclude or negatively affect the planning and/or implementation of the proposed Highway 413 while providing alternative options should it not proceed.
- H8.5.14 Create a safe, efficient and integrated transportation network that provides public road access to all parcels, meets the needs of all modes of travel and promotes active transportation. Facilitate connections to major transportation corridors as well as urban development areas within the Town to maximize connectivity to key markets and major areas of population.
- H8.5.15 Ensure appropriate interfaces between new employment uses and adjacent existing agricultural areas to allow existing farming operations to continue to operate effectively and efficiently.
- H8.5.16 Integrate significant existing cultural heritage resources into future land use development through retention of heritage attributes that express the resource's cultural heritage value.
- H8.5.17 Collaborate with the Region of Halton to monitor and phase development in an orderly manner in order to address the cost-effective and timely supply of major

capital projects including municipal services and transportation network upgrades as the area develops.

- H8.5.18 Promote sustainable site and net-zero climate resilient building design and construction techniques to reduce energy and water consumption, encourage alternative modes of transportation, protect and enhance the natural environment, protect air and water quality, and improve waste management.

H8.6 GENERAL POLICIES

The general policies of the Official Plan will apply to the Secondary Plan area, particularly:

- a) General Development Policies, particularly Subdivision of Land, Urban Design, Cultural Heritage Resources, Transportation, Public Parkland, Community Facilities and Services, and Development Phasing Strategies; and,
- b) Plan Implementation and Administration, particularly Secondary Plans and More Detailed Plans, Site Plan Control, Pre-Consultation and Complete Application Requirements, and Interpretation
- c) The development of employment land uses in this Plan is intended to occur in a timely and orderly manner. Build out of the Phase 2B lands is anticipated to occur over a ten to fifteen year period although the policies address a twenty year time frame.

H8.6.1 Employment Targets

- H8.6.1.1 The employment target for the Secondary Plan area at full build out is 3570 jobs assuming a density of 25 jobs per net hectare.

- H8.6.1.2 In order to achieve this target, compact development within the *Prestige Industrial* and *Business Commercial* Areas will be encouraged. It will also be important to ensure that critical linkages such as access to markets, service providers, and available labour pools are maintained.

- H8.6.1.4** It is the policy of this Plan to prohibit new residential uses and other non-employment uses, including major retail uses, in the *Prestige Industrial Area* and *Business Commercial Area* designations, except:
- a) to recognize existing uses within the *Prestige Industrial Area* and *Business Commercial Area* as shown on Schedule H8 of this Plan;
 - b) for institutional uses which have provided a detailed study that sets limits on, and establishes criteria for, such uses based on the following principles:
 - i) the use is small scale, and collectively such uses within the *Prestige Industrial Area* and/or *Business Commercial Area* designations do not change the character of that designation;
 - ii) the location and design of the use addresses land use compatibility in a manner based upon Regional Guidelines and the policies in section H8.13;
 - iii) the use is located near the periphery of the Premier Gateway Employment Area;
 - iv) such uses do not collectively displace employment from the *Prestige Industrial Area* and *Business Commercial Area* designations to an extent that the supply of land within the *Prestige Industrial Area* and *Business Commercial Area* designations are insufficient to meet the employment target contained in Tables A1A (Population and Employment Targets) and F10 (Regional Phasing) of the Official Plan, and,
 - v) do not contain a residential component or function where individuals reside on a temporary or permanent basis, such as long-term care facilities, retirement homes, or boarding schools.
 - c) Notwithstanding H8.6.1.2 b), post-secondary education facilities will be permitted provided they undertake a land use compatibility study that has been approved by the Town in consultation with the Region, which indicates that they will not adversely affect the overall viability of the employment area.
- H8.6.1.5** It is the policy of this Plan to permit supportive commercial and secondary uses and services that enhance the Premier Gateway as set out in *Business Commercial* and the *Prestige Industrial Area* designations. Such uses are particularly encouraged to locate in the *Business Commercial Area* designation. The policy framework for these secondary uses was informed by a *Supportive Commercial Needs Assessment* study which provided estimates of the amount of supportive commercial floor space and land area which might be required. As the Secondary Plan area develops, the *Supportive Commercial Needs Assessment* study should be used as a guide in evaluating individual applications that include permitted secondary uses.

H8.7 LAND USE DESIGNATIONS

H8.7.1 Prestige Industrial Area

H8.7.1.1 The Secondary Plan area is part of a larger Premier Gateway Employment Area that is located in close proximity to Highways 401 and 407 and the proposed Highway 413 where other lands are also designated *Prestige Industrial Area* and are intended to form an economically competitive, high quality employment area.

H8.7.1.2 Within the Secondary Plan, where lands are identified as *Prestige Industrial Area* on Schedule H8 to this Plan, the predominant use of the land will be for employment uses. Advanced manufacturing, food and beverage processing, clean technology and renewable energy, research and development and office will be encouraged.

H8.7.1.3 Development will be designed to reflect and enhance the prominent location of the area through the creation of well-designed and sustainable buildings, structures and landscaping in a visually attractive environment.

H8.7.1.4 The main permitted uses within the *Prestige Industrial Area* will be limited to:

- a) business and professional offices in free-standing buildings or as part of an industrial mall;
- b) industrial uses, such as manufacturing, assembling, processing, fabricating, warehousing, wholesaling and distribution facilities located primarily within wholly enclosed buildings. Outdoor storage will not be permitted. For the purposes of this Secondary Plan, trailer waiting spaces are not considered outdoor storage;
- c) computer, electronics and data processing facilities;
- d) clean technology and renewable energy;
- e) research and development facilities;
- f) post-secondary education facilities excluding residential accommodation subject to policy H8.6.1.4 c);
- g) printing and associated service establishments; and,
- h) industrial malls, containing one or more of the main uses permitted in this designation.

New residential uses are prohibited. No use will be permitted that causes, or is likely to cause air pollution, offensive odours, ground or surface water pollution, or noise in excess of Provincial regulations or guidelines.

H8.7.1.5 Permitted Secondary Uses

The following secondary uses, which are intended to be ancillary to and supportive of the main permitted uses, may be permitted in the *Prestige Industrial Area*:

- a) administrative office uses accessory to, and located on the same lot as a permitted industrial use;
- b) limited commercial uses which are ancillary to the main permitted uses, such as service commercial uses excluding personal service uses, take-out or full-service restaurants, financial institutions, convenience stores, and commercial fitness centres. *Major retail* uses will not be permitted;
- c) commercial or trade school facilities;
- d) public parks and recreational facilities, in accordance with Section F7 (Parkland) of this Plan;
- e) limited private parks and recreational facilities located on lands adjacent to Arterial or Collector roads;
- f) limited retail sales of goods and materials manufactured, or substantially assembled, on the premises;
- g) limited institutional in accordance with the requirements of Part H8.6.1.4;
- h) motor vehicle service stations; and,
- i) uses that legally existed at the date of adoption of this Plan and expansions or alterations thereto which conform to the intent of this Plan and the implementing Zoning By-law.

H8.7.1.6 Development Policy for Secondary Commercial Uses

The development of new secondary commercial uses will be limited to those uses that serve the *Prestige Industrial Area* and as noted in H8.7.2.1, these uses will be encouraged to locate in the *Business Commercial Area*. Secondary commercial uses are subject to the following:

- a) Individual retail and service commercial uses permitted in section H8.7.1.5 b) will not exceed 400 sq m in total floor area within the *Prestige Industrial Area* unless they are located within a freestanding office building where they will be located on the ground floor and will not exceed 750 sq m in total floor area. The total retail and service commercial space on individual sites designated *Prestige Industrial Area* will not exceed 2000 sq m in total floor area.
- b) Full-service restaurants, commercial fitness centres and financial institutions may be integrated into industrial malls and/or free-standing office buildings, or may be developed as free-standing buildings provided that the building design is consistent and compatible with the overall design of the adjacent development.
- c) Take-out restaurants and convenience stores will be integrated into industrial malls or free-standing office buildings.

- d) Two or more full-service restaurants may be developed in a "campus" development at the intersection of arterial roads with arterial or collector roads, subject to a comprehensive site development plan in order to achieve well-designed and integrated development, including:
 - i) integrated internal circulation and parking areas;
 - ii) coordinated access points, in order to minimize the number of access points to abutting roads;
 - iii) compatible building design and location;
 - iv) complementary landscaping; and,
 - v) consistent signage and lighting facilities.
- e) A high quality of building and landscaping design will be required for all commercial uses to ensure compatibility with adjacent development.

H8.7.1.7 The maximum height of all buildings or structures within the *Prestige Industrial Area* will not exceed six storeys although this policy does not apply to signs, utility towers or other non-habitable structures. The greatest heights will be encouraged to locate adjacent to Steeles Avenue and Winston Churchill Boulevard. Where heights in excess of 3 storeys are proposed adjacent to institutional and existing residential uses, appropriate transitions and buffering will be provided in accordance with the Premier Gateway Employment Area Urban Design Guidelines.

H8.7.1.8 To fulfill the vision and guiding principles of the Secondary Plan, which encourage a variety of higher density employment uses, prior to considering any application, Council shall be satisfied that:

- a) the development provides a land use which contributes to achieving the required density targets as prescribed by Provincial and Regional Policy;
- b) the development is planned to encourage shared use of land, secondary uses such as office spaces, efficient use of multi-storey buildings (full or partial), and to maximize the space to encourage higher employment densities;
- c) elements of the public realm which will serve both employees and the community alike will be improved as a condition of development/ redevelopment; and
- d) low density development such as warehousing/storage does not preclude intensification in the future. Therefore, a plan must be submitted that demonstrates how the site and buildings could feasibly transition to higher density employment uses in accordance with market demands/trends and in keeping with the Town's objective to pursue investment that generates the greatest community benefits, including a higher number of jobs per sq. ft. of gross floor area.

H8.7.2 Business Commercial Area

H8.7.2.1. The purpose of the Business Commercial Area is to provide services that are important to support the primary employment function of the Prestige Industrial Area and help to promote a gateway function. These include such uses such as hotels, business and professional offices and limited service commercial uses. Hotels are only permitted in the Business Commercial Area and while ancillary service commercial uses are also permitted within the Prestige Industrial Area designation, they will be encouraged to locate in the Business Commercial Area. Development will be designed to reflect and enhance the visible location of the land within the Business Commercial Area through the creation of well-designed and sustainable buildings, structures and landscaping in a visually attractive environment. The designation of new Business Commercial Area sites will only be permitted as part of a Municipal Comprehensive Review.

H8.7.2.2. Permitted Uses within Business Commercial Area are limited to:

- a) hotels including full service hotels with conference, exhibition, banquet and amenity facilities;
- b) business and professional offices in free-standing buildings or as part of an industrial mall;
- c) research and development facilities;
- d) limited retail and commercial uses which are ancillary to and serve the main permitted uses including banks, financial institutions and services; take-out restaurants subject to the policies in section H8.7.2.3 c), full service restaurants, convenience stores and commercial fitness centres. Limited retail and commercial uses do not include major retail or supermarkets, specialty food stores, department stores, general merchandise stores, fashion retailers, and personal service uses;
- e) motor vehicle service stations;
- f) printing and associated service establishments;
- g) industrial malls, containing one or more of the uses permitted in this designation;
- h) uses that legally existed at the date of adoption of this Plan and expansions or alterations thereto, which conform to the implementing Zoning By-law.

H8.7.2.3 Development within the Business Commercial Area permitted by H8.7.2.2 is subject to the following:

- a) All individual retail and service commercial uses permitted in Section H8.7.2.2 d) will not exceed 750 sq m in total floor area within the Business Commercial Area designation. The total retail and service commercial space

on individual sites designated Business Commercial Area will not exceed 2500 sq m in total floor area.

- b) Full-service restaurants, financial institutions and commercial fitness centres may be integrated into industrial malls and/or free-standing office buildings, or may be developed as free-standing buildings provided that the building design is consistent and compatible with the overall design of the adjacent development.
- c) Take-out restaurants and convenience stores will be integrated into industrial malls or free-standing office buildings.
- d) Two or more full-service restaurants may be developed in a "campus" development, subject to a comprehensive site development plan in order to achieve well-designed and integrated development, including:
 - i) integrated internal circulation and parking areas;
 - ii) coordinated access points, in order to minimize the number of access points to abutting roads;
 - iii) compatible building design and location;
 - iv) complementary landscaping; and,
 - v) consistent signage and lighting facilities.
- e) Building and landscaping design will be required to meet the objectives of the Urban Design Guidelines to ensure appropriate design considerations along the Regional Major Arterial, Local Minor Arterial and Local Collector Road frontages.

H8.7.2.4 The maximum height of any buildings or structures within the Business Commercial Area will not exceed six storeys except for hotels and free-standing office buildings which may be permitted up to ten storeys. This policy does not apply to signs, utility towers or other non-habitable structures. Where heights in excess of 3 storeys are proposed adjacent to institutional and existing residential uses, appropriate transitions and buffering will be provided in accordance with the Urban Design Guidelines.

H8.7.3 Residential Special Policy Area

H8.7.3.1 While it is the intent of the Secondary Plan that over time, existing uses will be replaced by uses contemplated in accordance with the land use designations and provisions of the Secondary Plan, the Residential Special Policy Area identifies locations where existing residential uses might not redevelop in the short term. The Residential Special Policy Area is an overlay to the *Prestige Industrial Area* designation and new development in the Residential Special Policy Area on Schedule H8 will be in accordance with the underlying land use designation. To recognize this situation, the following special policies will apply to those lands:

- a) The existing residential uses may be recognized in the Implementing Zoning By-law. Any new development will require rezoning which will be reviewed by the Town in the context of the Secondary Plan policies.
- b) In addition to the uses permitted in the underlying land use designation on Schedule H8, within the Residential Special Policy Area, the Town may rezone lands to permit the use of the existing residential dwelling, including additions to such dwellings, for office uses, home occupations or other uses which are compatible with the adjacent residential uses and uses permitted by the underlying employment land use designation.
- c) Where development is proposed on lands abutting a building being used for residential purposes in the Residential Special Policy Area, the Town may require the provision of landscaping and other buffering on the boundary of the residential use in accordance with the provisions in section H8.13.

H8.7.4 Natural Heritage System

H8.7.4.1 The goal of the *Natural Heritage System* is:

- a) to protect areas which have been identified as having environmental significance or which contain natural hazards such as valleylands, wetlands, watercourses and other hazardous lands, based on the functional recommendations of the Phase 2B Scoped Subwatershed Study and applicable Conservation Authority Regulations, and
- b) to ensure that the *Natural Heritage System* will be resilient to the impacts of the adjacent urban development and climate change by achieving enhanced ecological and hydrologic features and functions.

H8.7.4.2 The *Natural Heritage System* reflects the systems approach taken to ensure the protection, preservation and enhancement of the natural heritage features, and to maintain biological diversity and ecological functions of the area. The *Natural Heritage System* is a critical component of a healthy community and helps to define the character of the area.

H8.7.4.3 Lands within the *Natural Heritage System* will be protected and enhanced over time, and the following uses may be permitted in accordance with the objectives of the Secondary Plan and subject to, other policies of this Plan, applicable Zoning By-laws, and Conservation Authority Regulations:

- a) existing agricultural operations;
- b) single detached dwellings on existing lots;
- c) passive non-motorized recreational uses, such as nature viewing and pedestrian trail activities, only on publicly owned lands;
- d) forest management, wildlife management and fisheries management;
- e) archaeological activities;

- f) essential transportation and utility facilities; and,
- g) essential watershed management and flood and erosion control projects carried out or supervised by a public authority.

H8.7.4.4 Where any land within the *Natural Heritage System* designation is held under private ownership, this Plan will not be construed as implying that such areas are free and open to the general public. The Town will seek to obtain dedication of these lands through the development review process.

H8.7.4.5 Where new development is proposed on a site, part of which is designated *Natural Heritage System*, such lands shall not be considered acceptable as part of the dedication for park purposes as required by the *Planning Act*.

H8.7.4.6 The land in the *Natural Heritage System* shown on Schedule H8 is based on a systems approach to protecting and enhancing natural features and functions and consists of the following components:

- 1) *Key Features*, which include:
 - a) habitat of endangered and threatened species,
 - b) significant wetlands,
 - c) significant woodlands,
 - d) significant valleylands,
 - e) significant wildlife habitat,
 - f) significant areas of natural and scientific interest,
 - g) fish habitat,
- 2) *Enhancements to the Key Features*,
- 3) *Linkages*,
- 4) *Buffers*,
- 5) Watercourses that are within Conservation Halton or Credit Valley Conservation Regulation Limits or drainage features that provide a linkage to a wetland or a significant woodland,
- 6) Wetlands other than those considered significant, and
- 7) Natural Hazards which include flooding hazards, erosion hazards and/or hazardous lands as determined, mapped and refined from time to time by Conservation Halton or Credit Valley Conservation.

Key Features that have been identified are included in the *Natural Heritage System* shown on Schedule H8. The boundaries and/or characterization of these features may be refined through the Subwatershed Impact Study identified in Section H8.8. The Subwatershed Impact Study may also identify other *Key Features* that are not within the *Natural Heritage System* shown on Schedule H8 and if found, these unmapped *Key Features* will be protected through appropriate development approvals in accordance with Section H8.7.4.7.

- H8.7.4.7 A systems based approach to implementing the *Natural Heritage System* will be applied by:
- a) Prohibiting development and site alteration within significant wetlands, significant habitat of endangered and threatened species, and fish habitat except in accordance with Provincial and Federal legislation or regulations;
 - b) Not permitting the alteration of any components of the *Natural Heritage System* unless it has been demonstrated that there will be no negative impacts on the natural features or their ecological functions, and hydrologic functions if applicable;
 - c) Refining the boundaries of the *Natural Heritage System*, including additions, deletions, and/or boundary adjustments, during site specific applications through a Subwatershed Impact Study in accordance with Section H8.8 accepted by the Town, applicable Conservation Authority and the Region, an individual Environmental Impact Assessment (EIA) accepted by the Town and the Region, or similar studies based on terms of reference accepted by the Town in consultation with the Region. Such refinements may occur without an amendment to the Secondary Plan on a site-specific basis;
 - d) Restoring and enhancing, where feasible, natural features and areas within the *Natural Heritage System*. Where areas are not in a natural state, they should be naturalized and planted with native, non-invasive species.
 - e) Requiring the proponent of any development or site alteration, including public works, that are located within the *Natural Heritage System* or on adjacent lands within 120 m of the *Natural Heritage System* to carry out an Environmental Impact Assessment (EIA), unless:
 - i) the proponent demonstrates to the satisfaction of the Town, applicable Conservation Authority and the Region that the proposal is minor in scale and/or nature and does not warrant an EIA; or
 - ii) the proponent demonstrates to the satisfaction of the Town, Conservation Authority and the Region that a Subwatershed Impact Study, prepared in accordance with Section H8.8, adequately addresses the purpose of an EIA.
 - f) The purpose of an EIA is to demonstrate that the proposed development or site alteration will result in no negative impacts to that portion of the *Natural Heritage System* or unmapped *Key Features* affected by the development or site alteration by identifying components of the *Natural Heritage System* as listed in Section H8.7.4.6 and their associated ecological functions, and by assessing the potential environmental impacts, requirements for impact avoidance and mitigation measures, and opportunities for enhancement. The EIA, will identify any *Key Features* on or near the subject site that are not mapped on Schedule H8.

- H8.7.4.8 The *Natural Heritage System* may include additional lands beyond identified natural heritage features and their buffers, which are identified through an approved Subwatershed Impact Study as being a *Key Feature* and/or providing additional enhancement to the system. These enhancements provide for increased ecological services such as wildlife habitat and movement opportunities, increased buffering to natural heritage features, and restoration opportunities including additional vegetation plantings.
- H8.7.4.9 The *Natural Heritage System* in the Secondary Plan area connects with the natural heritage system within the Town. This connectivity is important to support natural processes and provide corridors that allow species to move and adapt, thereby ensuring a healthy and resilient ecosystem that protects biodiversity for the long term.
- H8.7.4.10 Buffers for natural heritage features have been included within the *Natural Heritage System* illustrated on Schedule H8, so additional buffers should not be required except where unmapped *Key Features* have been identified through an approved Subwatershed Impact Study and/or requirements to address any Endangered or Threatened species may be applicable. The following buffer widths are provided for general guidance but the final buffer widths, which may be smaller or larger, will be determined in the Subwatershed Impact Study based on an analysis of the feature sensitivity and proposed adjacent land uses in accordance with Section H8.7.4.10.1.
- a) **Watercourse Corridors:**
Generally greater of 15 metres from the greatest hazard (Regional Storm flood plain, meander belt or stable top of bank) or 30 metres from the bankfull channel of cold, cool and warm water sportfish watercourses. The hazard limits shall be determined as per policy H8.7.4.18 Watercourses and Natural Hazards. Where a trail is planned, it may be located within the buffer subject to section H8.7.4.20.
 - b) **Woodlands:**
Generally 30 metres from the drip line for significant woodlands and 10 metres for other woodlands. Where a trail is planned, it may be located within the buffer subject to section H8.7.4.17.
 - c) **Wetlands:**
Generally 30 metres from the wetlands limits. The size and significance of the wetlands are subject to wetland delineation and the Ontario Wetland Evaluation System as part of the Subwatershed Impact Study.
 - d) **Significant Valleylands:**

The East Sixteen Mile Creek valleyland is considered significant and is included in the *Natural Heritage System*. The valleyland boundaries will be defined by the long term stable top of slope and natural heritage features that overlap with the top of slope boundary. The overlapping surveyed feature boundaries and appropriate buffers will determine the Significant Valleyland boundary as part of the Subwatershed Impact Study.

- e) Significant wildlife habitat
Generally 30 metres from the boundary of the habitat unit.
- f) Significant habitat of endangered or threatened species
Generally 30 metres from the boundary of the habitat unit or as determined through the permitting process under the Endangered Species Act, 2007.
- g) Fish Habitat
Generally 30 metres from the boundary of the habitat unit.
- h) Linkages
Generally 60 metres when associated with a wetland area

H8.7.4.10.1 The Phase 2B Scoped Subwatershed Study, as interpreted in accordance with the policies of Section H8.7.4.10, represents an initial assessment and recommendation of buffer widths based on general information on land uses contemplated adjacent to the *Natural Heritage System*.

The final buffer width is to be determined through a Subwatershed Impact Study or Environmental Impact Assessment at the development stage when additional information is available to determine the nature of adjacent uses and related impacts on the system and may include refinements to the buffer widths.

Through the Subwatershed Impact Study or Environmental Impact Assessment buffer widths may be refined through the consideration of the nature of abutting land uses in relation to the adjacent key features and the significance of those key features. In particular, the intensity of the abutting land uses as illustrated through specific plans for such uses (i.e. grading, setbacks, maintenance, servicing, built form including height, location of buildings and structures and other activities) will be evaluated. In all cases, the final buffer widths must have regard for the following:

- Maintain or improve the level of certainty regarding buffer function effectiveness post refinement including factors such as the timing of planting and certainty regarding the nature of the abutting development;
- Achieve no negative impacts to the *Natural Heritage System* key features

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and their ecological functions;

- The ecological functions, characteristics, significance and sensitivity of the *Natural Heritage System* key features which will ultimately determine the final buffer width through the Subwatershed Impact Study or Environmental Impact Assessment; and
- Adhere to the relevant goals, objectives and policies of this Plan, Regional Official Plan and relevant Provincial policies to the satisfaction of the Town, and Region, and in consultation with the applicable Conservation Authority.

H8.7.4.11 *Natural features and areas including Linkages within the Natural Heritage System, should be restored and enhanced, where feasible, and where areas are not in a natural state, they should be naturalized and planted with native, non-invasive species. The requisite planting and seeding plans will be established at the time of subdivision or site plan approval.*

H8.7.4.12 *Through the Subwatershed Impact Study or detailed design stage, areas not suitable for development that are created through the irregular *Natural Heritage System* boundary, should be included in the *Natural Heritage System* and naturalized to provide an enhancement to the proposed system. Where the provision of enhancement measures as well as open space uses such as stormwater management and Low Impact Development are to be provided, they will be encouraged to locate on lands adjacent to the *Natural Heritage System*.*

H8.7.4.13 *Headwater Drainage Features*

H8.7.4.13.1 *The location of Headwater Drainage Features identified through the Phase 2B Scoped Subwatershed Study for protection or conservation, and the width of the associated buffers will be determined as part of the Subwatershed Impact Study.*

H8.7.4.13.2 *The Phase 2B Scoped Subwatershed Study management recommendations dealing with drainage features indicates that some headwater drainage features may be suitable for relocation or compensation if they are classified as "Mitigation" or "Conservation", are not regulated by a Conservation Authority, and are outside of the *Natural Heritage System*. The hazard limits shall be determined as per policy H8.7.5 Watercourses and Natural Hazards. Where relocation occurs, the feature should be moved to an area where its form and function can be replicated and where it is contiguous with other natural features where possible, in order to provide overall benefit to the natural heritage feature and the wildlife that use and inhabit them.*

Final determination as to whether the relocation is appropriate for headwater drainage features will be determined through the Subwatershed Impact Study submitted in accordance with H8.8 as part of a development application and may be further supported by a design report, or inclusion within a Natural Channel Design Brief for the subject lands at the detailed design stage which may include:

- a) Details related to the natural channel design principles applied to the design of the feature;
- b) Fluvial geomorphological analysis of the proposed feature design;
- c) Rationale for selection of plantings within the riparian zone and floodplain;
- d) Details regarding any enhancements proposed within the adjacent feature;
- e) Detailed hydrologic and hydraulic analyses of proposed feature and hydraulic structures to demonstrate impacts to floodplains, and freeboard under proposed conditions, maintenance of riparian storage post-development;
- f) Detailed groundwater analysis to demonstrate that the groundwater seepage function can be replicated in proposed condition;
- g) Detailed assessment of impacts of proposed feature to aquatic habitat and fish species; and
- h) Detailed design of the drainage for proposed feature and corridor.

H8.7.4.13.3 As noted in H8.7.4.15.2, a headwater drainage feature located within the Mullet Creek watershed is associated with a wetland. It is included in the Natural Heritage System and as such, is not intended to be removed. The Subwatershed Impact Study prepared as part of the development application will be required to further assess this wetland in accordance with H8.7.4.6 and the requirements of the management recommendations of the Phase 2B Scoped Subwatershed Study to determine the exact configuration and width of the headwater drainage feature and to address the opportunity to also utilize the feature for flood protection purposes.

H8.7.4.14 Woodlands

H8.7.4.14.1 The Woodlands associated with the Eastern Sixteen Mile Creek valley were considered significant at the time of the Phase 2B Scoped Subwatershed Study and included in the Natural Heritage System. Retention of other woodlands, hedgerows and individual trees will be encouraged where possible. Tree removal must take into consideration the Migratory Birds Convention Act and the Endangered Species Act with regards to breeding birds and Species at Risk bats.

H8.7.4.14.2 The Subwatershed Impact Study will identify or refine the boundaries of woodlands and implement measures to protect and significant woodlands as described in the recommendations of Phase 2B Scoped Subwatershed Study including refining the width of buffer in accordance with Section H8.7.4.10.1.

H8.7.4.15 Wetlands

H8.7.4.15.1 Wetlands within the Secondary Plan area may require further evaluation as part of the Subwatershed Impact Studies as per the Ontario Wetland Evaluation System. Where wetlands exist, they have been incorporated into the *Natural Heritage System*.

H8.7.4.15.2 An unevaluated wetland was identified to the east of Tenth Line and north of the proposed collector road. This wetland is regulated by Credit Valley Conservation and will require a development setback based on delineation and evaluation in the Subwatershed Impact Study. In accordance with H8.8.3, the Subwatershed Impact Study prepared as part of the development application will further assess this feature in conjunction with the headwater drainage feature that will connect the wetland to the Mullet Creek.

H8.7.4.15.3 Within the lands located east of Ninth Line, north of Steeles Avenue that are part of the Usgar drainage area, the Scoped Subwatershed Study has identified two vegetation communities that require further assessment during the Subwatershed Impact Study. The general locations of these two areas are shown on Schedule H8. They have not been included in the *Natural Heritage System* based on their lack of provincial or regional significance or their connection to other natural heritage features or areas as determined by the Phase 2B Scoped Subwatershed Study. If either of these areas are determined to be wetlands, the potential for them to be removed and replicated elsewhere may be considered through the Subwatershed Impact Study to the satisfaction of the Town Halton Region and Conservation Halton. The potential removal and replication shall have regard for the recommendations in the Phase 2B Scoped Subwatershed Study. The Corridor Protection Area of the proposed Highway 413 traverses this area and these potential wetlands will also be evaluated through that Environmental Assessment process.

H8.7.4.16 Significant Wildlife Habitat

- H8.7.4.16.1 Significant Wildlife Habitat has been identified within the Secondary Plan area and included in the *Natural Heritage System*. Further assessment and mitigation requirements for Significant Wildlife Habitat are required in the Subwatershed Impact Study. Any additional Significant Wildlife Habitat identified as part of a Phase 2B Subwatershed Impact Study should be considered for incorporation into the *Natural Heritage System* where appropriate along with appropriately sized buffers to ensure its protection and enhancement.

H8.7.4.17 Species at Risk

- H8.7.4.17.1 Species at Risk were observed within the Secondary Plan area. Further assessment and mitigation requirements for Species at Risk habitat will be addressed through the Subwatershed Impact Study. Boundaries of any habitat for endangered and threatened species identified as part of the Subwatershed Impact Study should be delineated in consultation with the Ministry of Environment, Conservation and Parks in accordance with provincial requirements under the ESA, 2007. The boundaries of this habitat and any buffers deemed necessary through the Subwatershed Impact Study should be incorporated into the *Natural Heritage System* where appropriate, to ensure its protection and enhancement.

H8.7.4.18 Watercourses and Natural Hazards

- H8.7.4.18.1 Watercourses and natural hazard lands within the Secondary Plan area are part of the *Natural Heritage System*.
- H8.7.4.18.2 The delineation and regulation of watercourses and natural hazard lands is administered by either Conservation Halton or Credit Valley Conservation. The approximate limits of these lands are included in the *Natural Heritage System* as shown on Schedule H8 but may be subject to change until such time as the limits are delineated through a Subwatershed Impact Study (SIS). The studies and resulting delineation of the watercourses, natural hazards and their associated regulatory allowances and buffers will be to the satisfaction of the Town and the applicable Conservation Authority and new development will be located outside these limits.
- H8.7.4.18.3 As part of the development approval process, watercourses, natural hazards and their associated regulatory allowances and buffers will be zoned to an appropriate natural areas zoning category.

H8.7.4.19 Road Crossings and Alignments

- H8.7.4.19.1** New roads, are intended to be located outside of the *Natural Heritage System* including its buffers in order to reduce impacts on the natural heritage features and only essential road infrastructure may be permitted after all other alternatives have been considered through an appropriate study. The approach to be taken in addressing road locations will be to first avoid impacts if possible, and then minimize, mitigate and finally compensate for any resulting impacts. Crossings and alignments within or in close proximity to the *Natural Heritage System* will need to be assessed through an Environmental Assessment, Subwatershed Impact Study, or an equivalent comprehensive study to address negative impacts. Best management practices to facilitate fish and wildlife passage across road crossings and appropriate buffers from alignments, will be addressed as part of the detailed design.

H8.7.4.20 Trails

- H8.7.4.20.1** A trail system should be established along the edge of the *Natural Heritage System*, including linkages, where lands are being put into public ownership to allow public use and access. The trails and associated activities will not negatively impact ecologically sensitive areas. Where possible, the trail system should be constructed at the start of development, to provide immediate access and connection with the natural environment, while discouraging the establishment of footpaths and ensuring that proper regard is given to avoiding trespassing on private properties. For all trails proposed within *Natural Heritage System* buffers and enhancement areas, trail siting should be located outside the Conservation Authority Regulated Area where possible and close to the development side of the buffer to provide as much area as possible for naturalization plantings between the development and key natural heritage features. Adjacent landowners potentially affected by the trails will be consulted.
- H8.7.4.20.2** Fencing should be considered along the edge of the *Natural Heritage System* irrespective of ownership in order to restrict access from people and pets. Public access to the *Key Features* should be discouraged and trails should avoid ecologically sensitive features.

H8.8 SUBWATERSHED STUDY

- H8.8.1** The Phase 2B Scoped Subwatershed Study was undertaken in conjunction with preparation of this Secondary Plan in order to characterize the terrestrial and aquatic ecology, stream systems and surface water and groundwater resources, refine the *Natural Heritage System*, achieve a greater level of detail for natural

hazards, assist with development of the land use plan, determine the expected impacts from development and address the associated requirements and opportunities for mitigation and management. All new development in the Secondary Plan area will generally comply with the recommendations of Premier Gateway Phase 2B Scoped Subwatershed Study except for the land at 8029 Eighth Line identified on Schedule H8 as subject to settlement decision D090PA22.001 at the Ontario Land Tribunal under file PL180499 which is subject to a separate Scoped Subwatershed Study. No amendments to the Secondary Plan will be required to implement the recommendations of the Phase 2B Scoped Subwatershed Study.

- H8.8.2 A Subwatershed Impact Study will be required in support of a complete development application unless it has been demonstrated to the satisfaction of the Town and applicable Conservation Authority in consultation with the Region that either the proposal is minor in nature and does not warrant a study, or the issues have been or will be addressed through other studies such as an Environmental Impact Study and that matters related to the *Natural Heritage System* are addressed in accordance with H8.7.4.7.
- H8.8.3 The goal of the Subwatershed Impact Study will be to achieve a greater level of detail in the integration of land use, servicing, stormwater management and the *Natural Heritage System*. The study area for the Subwatershed Impact Study should be based on subcatchment areas but could be modified or consolidated subject to the approval of the Town, and the applicable Conservation Authority in consultation with the Region of Halton.
- H8.8.4 Greater detail regarding the matters to be addressed by the Subwatershed Impact Studies is outlined in Appendix A and should be incorporated into the Terms of Reference for the Subwatershed Impact Study. The Terms of Reference will be refined on an individual basis and subject to the approval of the Town, in consultation with the applicable Conservation Authority and the Region of Halton.

H8.9 TRANSPORTATION

The Secondary Plan area will provide a safe, integrated, holistic, multi-modal transportation network that is well connected to other lands within the Premier Gateway Employment area, and provides easy access to major population areas within the Town as well as key transportation corridors. As a result of this work, a road network as well as a number of transportation infrastructure improvements were identified as required to support the development of the Secondary Plan area. All future development will be tied to the provision of these required infrastructure improvements. The Transportation policies of Section F6 of this Plan are applicable to the Secondary Plan area and in addition, the following policies:

H8.9.1 Highway 413 and the Northwest GTA Transmission Corridor

- H8.9.1.1** The Highway 413 Focused Analysis Area and the Northwest GTA Transmission Corridor Narrowed Area of Interest are to be protected as planned corridors for the purpose(s) for which they are identified.
- H8.9.1.2** Development within the Highway 413 Focus Analysis Area and Northwest GTA Transmission Corridor Narrowed Area of Interest shown on Schedule H8 of this Plan will not preclude or negatively affect the planning and/or implementation of the planned corridors for the purpose(s) for which they are identified.
- H8.9.1.3** Lands shown on Schedule H8 as being within the Highway 413 Corridor Protection Area, have an underlying Prestige Industrial Area designation. When the final location of Highway 413 and Northwest GTA Transmission Corridor has been determined by the Province, all lands not required for Highway 413 or the Northwest GTA Transmission Corridor may develop in accordance with the Prestige Industrial Area designation and policies of this Plan without an amendment to this Secondary Plan.

H8.9.2 Road Network

- H8.9.2.1** The road network will provide for the safe and efficient movement of people and goods. Road right of way widths shown on Schedule B2 to this Plan and Map 4 of the Region of Halton Official Plan or the most up to date Regional Transportation Master Plan requirements, will be provided in accordance with Table F6 (Function of Transportation Facilities) of this Plan and in accordance with Policy 173 (5) of the Region of Halton Official Plan.
- The location and general alignment of the new collector roads as shown on Schedule H8 are conceptual and approximate, and subject to study as may be required by the Town and/or the Region but are necessary to ensure appropriate overall functioning of a transportation system that can support development of the area. Where the east-west collector road crosses Highway 413 and the Northwest GTA Transmission Corridor, the final alignment will be subject to approval by the Province and may be modified to reflect the locational needs of the Highway and Transitway stations. Any proposed additions or deletions to collector roads identified will require an amendment to this plan.
- H8.9.2.2** Following adoption of this Secondary Plan by Council, through Regional Official Plan Amendment 49 additional employment lands were incorporated into the Urban Area immediately to the north of the Premier Gateway Phase 2B Secondary Plan Area. As a result, additional technical analysis was undertaken with respect to the transportation network with a focus on the location of the

east-west collector road. Through this analysis, it was deemed appropriate to realign the collector road west of Highway 413 onto lands to the north in accordance with the east-west collector road envelope as shown on Appendix B to this Plan. The east-west collector road envelope will be subject to further study as part of a future Secondary Plan exercise for the additional employment lands.

HS.9.2.3 All roadway and driveway spacings will conform to standard roadway engineering practices and will be approved by the respective roadway jurisdiction. The provision of additional local roads, where deemed necessary, may be determined as part of the development approval process. The road network will be designed to augment the existing system, improve connectivity within the Secondary Plan area and to adjacent areas, provide opportunities to relieve pressure on key intersections, facilitate access to individual properties, and reduce driveway accesses onto arterial roads.

Access to Regional Roads shall be in accordance with the most current Halton Region Access Management Guidelines and by-laws.

Only one full movement intersection will be permitted on Ninth Line within the Secondary Plan area.

As part of the Staging and Monitoring Plan for the Secondary Plan area, an Access Management Strategy must be developed to ensure interim and ultimate access during implementation is achieved through landowner coordination and in conformity with the Town's and Region's requirements (for example, Access Management Guideline and By-law NO. 32-17).

- H8.9.2.4 The Town will work with the Region of Halton to ensure that Steeles Avenue functions efficiently as a major route into and through the Secondary Plan area. Given the future projected traffic volumes, improvements will be required at all Steeles Avenue intersections within the Secondary Plan in order to address the capacity issues. The Region of Halton will be undertaking a Municipal Class Environmental Assessment for improvements to Steeles Avenue from Trafalgar Road to Winston Churchill Boulevard. A range of multi-modal options for corridor improvements will be considered, such as widening of the roadway, cross-sectional requirements, active transportation, transit infrastructure improvements, intersection improvements, and overall traffic operations.
- H8.9.2.5 As part of any development application, a Traffic Impact Study may be required in order to demonstrate
- a) that adequate network capacity exists to accommodate the proposed development;
 - b) the need for new collector road segments;
 - c) the need for road and/or intersection improvements (such as traffic signals, turn lanes, access construction, illumination etc.) and the timing for such improvements;
 - d) the development supports the overall function of the road network for the Phase 2B Secondary Plan area;
 - e) any additional requirements as determined by the Town and/or Region.
- The Traffic Impact Study shall be completed in accordance with the Town and Region's Transportation Impact Study Guidelines.
- H8.9.2.6 The Winston Churchill Boulevard and Steeles Avenue intersection should be designed to reflect its role as major gateway to Halton Hills. As such, the intersection should include enhanced streetscape design, landscaping, signage and lighting in accordance with all jurisdictional guidelines, as applicable

- H8.9.2.7 Future access to the Regional Road Network will be restricted and/or controlled, should be limited to the connections with the road network identified for the Secondary Plan area, and will be based on a Traffic Impact Study. Access to Regional roads will be subject to Regional Access Management Guidelines, and By-Law 32-17, a By-Law to Prohibit, Restrict, and Regulate Access to the Regional Road System.
- H8.9.2.8 A new east-west collector road with several new north-south collector road connections is shown through the Secondary Plan area on Schedule H8 as conceptual and subject to further traffic study and coordinated with the Region as it relates to water and wastewater... A scope of work for the Transportation Impact Study must be completed in accordance with the Town's and Region's most up to date guidelines and policies. The location of the road must be coordinated with the Region as it relates to water and wastewater services to ensure that the infrastructure aligns with the Area Servicing Plan and the Wastewater Servicing Feasibility Study for the Secondary Plan area.
- A terms of reference for a Transportation Impact Study is required and must be approved by the Town and the Region in advance if undertaking any analysis. All studies must be completed in accordance with the Town's and Region's most up to date guidelines and policies.
- H8.9.2.9 The new east-west collector road is intended to cross Tenth Line in a relatively central location between Steeles Avenue and the northern boundary of the Secondary Plan. The location of the new east-west collector road will be subject to further review at the time of development applications taking into consideration the existing residential houses on the west side of Tenth Line and the final location of the Natural Heritage System east of Tenth Line.
- H8.9.3 Active Transportation**
- H8.9.3.1 Development will be based on facilitating the use of walking, cycling and lightly motorized modes of travel within the Secondary Plan area in order to assist the Town in achieving its environmental stewardship goals and to encourage increased activity levels regardless of age, fitness, or ability as a component of healthy living by those who live and work within the Town.
- H8.9.3.2 The transportation system will be designed to maximize connectivity, safety and comfort for pedestrians, cyclists, those with accessibility issues and those using lightly motorized modes of travel both within the Secondary Plan area and between the Secondary Plan area and other activity areas within the Town.

- H8.9.3.3 The Town's Active Transportation Master Plan, which supports and promotes an increased level of cycling within the Town, has identified a two-tiered cycling network that provides for both on-road opportunities such as bike lanes and off-road opportunities such as trails and multi-use paths.
- H8.9.3.4 The Halton Region Active Transportation Master Plan also identifies regional cycling and walking networks for Regional Roads. Detailed facility types will be confirmed through a future Municipal Class Environmental Assessment.
- H8.9.3.5 In addition to the infrastructure identified, Council will also:
- a) require the provision of sidewalks as new roads are created or existing roads are redeveloped in the Secondary Plan area;
 - b) ensure the provision of safe and convenient on-site pedestrian connections and adequate bicycle storage facilities in the review of development applications;
 - c) require barrier-free design of all new pedestrian facilities; and
 - d) ensure that lands for bicycle, pedestrian and lightly motorized facilities are included with the land requirements for roads.

H8.9.4 Transit

H8.9.4.1 Although the Town does not have a full transit system, all development within the Secondary Plan area will be designed to promote transit supportive land use in order to facilitate its use when full services are provided. Future transit stops should be designed to capture the majority of development within a 10-minute walk.

H8.9.4.2 The Region of Halton has identified Steeles Avenue as a Transit Priority Corridor in their Mobility Management Strategy for Halton and the Defining Major transit Requirements for Halton. As the Region undertakes further work, the form and function of the corridor may be updated.

H8.9.5 Transportation Demand Management

H8.9.5.1 In support of the creation of a multi-modal transportation system, the Town will actively work with developers and businesses to develop and implement Transportation Demand Management measures that aim to reduce motorized vehicular trips and that promote and enable the use of active transportation modes, car-sharing and/or carpooling, use of existing Steeles Avenue public transit and other transit routes when they become available, and travel during off-peak hours.

HB.9.6 Parking

- HB.9.6.1 Surface parking will be minimized in order to reduce large expanses of hard surfaces and to encourage the use of alternative means of transportation. Where surface parking is provided, it should be appropriately landscaped and screened from the street in accordance with F.2.2.5, with clearly defined pedestrian connections between the parking area and the building entrances. Consideration will also be given to addressing the Premier Gateway Employment Area Urban Design Guidelines and the Town's Green Development Standards.

HB.10 SERVICING

HB.10.1 Functional Servicing Study

- HB.10.1.1 A Functional Servicing Study will be prepared as part of the subdivision or site plan approval process, which will generally include:
- a) Location (including depth) and preliminary sizing of wastewater sewers;
 - b) Location and preliminary sizing of storm sewers;
 - c) Location and preliminary sizing of water mains;
 - d) Preliminary site grading plan;
 - e) Preliminary analysis of where infrastructure will be constructed below the water table and identification of mitigation measures to be used for dewatering during construction and post construction;
 - f) Location and preliminary sizing of stormwater management facilities;
 - g) Location where Low Impact Development Best Management Practices may be considered;
 - h) Location and preliminary sizing of hydraulic structures (i.e. bridges and culverts);
 - i) Preliminary channel grading plans and supporting analyses; and
 - j) Assessment of riparian storage for existing channel and preliminary channel designs.

The Region may accept a deviation to allow local water and wastewater servicing within Steeles Avenue. Detailed justification must be included in the report for any local water and wastewater mains (and associated servicing connections) to be installed within the Steeles Avenue right-of-way.

The report will be prepared in accordance with the approved Area Servicing Plan to the satisfaction of the Town, Region and applicable Conservation Authority in accordance with the policies in Section C8 and G.12 of this Plan.

H8.10.2 Municipal Water and Wastewater Services

- H8.10.2.1 The Region is responsible for the development of water treatment and distribution as well as wastewater collection and treatment infrastructure. All water treatment, water distribution, wastewater collection and wastewater treatment infrastructure servicing the Secondary Plan area shall be developed in accordance with the Water and Wastewater Master Plan and Development Charges Background Study, as further refined by the Area Servicing Plan.
- H8.10.2.2 All new development will occur on the basis of full municipal services. Extension of the Region of Halton's water distribution and wastewater collection systems will be required to support development within the Secondary Plan area in accordance with the approved Area Servicing Plan. Notwithstanding the requirement for development to proceed on the basis of full municipal services, due to the significant economic benefits associated with development in the Phase 2B Secondary Plan Area and in lieu of the Region's water and wastewater services being available, temporary private water and wastewater services may be considered on a limited basis subject to the requirements of the Region's Urban Services Guidelines and other considerations.
- H8.10.2.3 New servicing to the Secondary Plan area will generally be developed from west to east to allow for an efficient and cost-effective progression of services. Private front-end construction of water and waste water services to accommodate development west of the GTA West Corridor ahead of the implementation of the Region's planned infrastructure may be considered on a case-by-case basis. The availability of water flows and adequate pressure and wastewater system capacity must be confirmed with the Region. The design of front-ended infrastructure must be to the satisfaction of the Region.
- H8.10.2.4 The extension of water and waste water services will need to cross the proposed Highway 413 and the construction of a looped watermain system will be required.
- H8.10.2.5 Existing houses in the area are currently serviced by individual water wells. Houses within the Secondary Plan area will be eligible for connection to full municipal services as part of the provision of municipal services to the employment area in accordance with the Region's Urban Services Guidelines. The use of Best Management Practices to protect local aquifer and water supply wells will be required for all development within the Secondary Plan area.
- H8.10.2.6 The extension of municipal services on the lands north of the East Sixteen Mile Creek may not be permitted to cross the main branch of the East Sixteen Mile

Creek given the significance of the *Natural Heritage System* in that area. This could delay development of the lands north of the East Sixteen Mile Creek until servicing is available to be extended from lands to the north which are outside of the Secondary Plan area.

HB.10.3 Stormwater Management

- HB.10.3.1 The Stormwater Management Facilities shown on Schedule HB represent the general locations for stormwater management facilities within the Secondary Plan Area and relocation of these facilities or refinement of the number of facilities will not require an amendment to the Secondary Plan. These locations are intended to maintain the supply of runoff to receiving systems and to minimize requirements for operation and maintenance by the Town. Stormwater Management facilities will accommodate the ultimate drainage from arterial roads. Notwithstanding the location of these designations, such facilities will be permitted in all designations except the *Natural Heritage System* where only those components of such facilities that are essential for conveying stormwater to the receiving watercourse may be permitted. Stormwater Management Facilities will be located and designed to the satisfaction of the Town, the Region, and the applicable Conservation Authority, and should be integrated with the open space system. Final details such as location and design criteria for stormwater management facilities, including Low Impact Development are to be determined through the Subwatershed Impact Study and Functional Servicing Study taking into account any proposed watercourse and/or headwater drainage feature alterations.
- HB.10.3.2 All new development will be supported by a Stormwater Management (SWM) report unless waived by the Town through a pre-consultation process. The purpose of the report is to demonstrate capability and compliance with the principles and criteria in the Phase 2B Scoped Subwatershed Study as well as the Subwatershed Impact Study. It will identify the specific stormwater management infrastructure to be implemented, and the details related to its design, maintenance and subsequent monitoring requirements.
- HB.10.3.3 In preparing stormwater management plans, a treatment train approach is recommended in order to address requirements for water quality, erosion control, flood control, thermal mitigation and water budget. Low Impact Development Best Management Practices such as bioswales/biofilters with underdrains, infiltration trenches, rain gardens and perforated pipes will be encouraged in addition to wet end-of-pipe facilities. Consideration will be given to minimizing the size of storm water management facilities through the use of such measures as roof top storage, parking lot storage, infiltration galleries, modified pond design

standards, and other appropriate measures to the satisfaction of the Town in consultation with the applicable Conservation Authority.

- H8.10.3.4 Where regulatory storm controls are required, and the Town has determined that a publicly owned facility is not feasible, the Town and the applicable Conservation Authority will evaluate the use of privately owned regulatory storm control facilities on a case-by-case basis. If the Town and the applicable Conservation Authority determine that a privately owned regulatory storm control pond or other appropriate stormwater management system may be permitted, the owner will be required to address the terms for its construction and maintenance through the development application process.
- H8.10.3.5 The stormwater management plans for land within the proposed Highway 413 corridor will not be integrated with the rest of the lands within the Secondary Plan area and will be determined through the Environmental Assessment and detailed design of the highway.
- H8.10.3.6 Stormwater management facilities for the lands within the area covered by Settlement Decision D090PA22.001 at the Ontario Land Tribunal under file PL180499 are subject to a separate Scoped Subwatershed Study and will be addressed through a site specific development application process.

H8.10.4 Utilities

- H8.10.4.1 The Town will confirm with all utility providers that adequate servicing networks are, or will be established to serve the anticipated and existing development, and that these networks can be phased in a manner that is cost-effective and efficient. The Town will encourage the establishment of high speed internet services to the area.

H8.11 AGRICULTURE

- H8.11.1 The full range of existing and new agricultural, agriculture-related, and secondary uses/ on-farm diversified uses are permitted within the Secondary Plan Area until lands are developed in accordance with the policies in the Secondary Plan.
- H8.11.2 Nothing in this Plan will limit the ability of farmers to carry out normal and reasonable farm practices in accordance with the Farming and Food Production Protection Act. All new development, including development on existing lots of record, will be sensitive to existing agricultural uses, including minimizing potential

impacts and will be required to address Minimum Distance Separation One formula, as amended, with respect to setbacks from agricultural operations.

H8.12 CULTURAL HERITAGE

- H8.12.1 The Secondary Plan area was historically an agricultural area and the cultural heritage resources reflect that history.
- H8.12.2 It is an objective of the Town to conserve significant cultural heritage resources and to ensure that all new development and any site alteration conserves significant cultural heritage resources. They shall be maintained and integrated into new development, where appropriate and feasible. The Town will exercise the powers and apply the tools provided by legislation, including the Ontario Heritage Act, in implementing and enforcing the cultural heritage policies of the Town.
- H8.12.3 The following properties within the Secondary Plan area are currently listed on the Town's Municipal Heritage Register and shown on Schedule H8 as locations of Cultural Heritage Value:
- Hornby Presbyterian Cemetery at 8021 Eighth Line (corner of Steeles Avenue);
 - Theodore Brain House at 15145 Steeles Avenue
- H8.12.4 Another cultural heritage resource has been identified as a candidate for conservation at 15625 Steeles Avenue and is shown on Schedule H8 as a location of Cultural Heritage Value. This cultural heritage resource retains historical and associative, design and architectural and/or contextual value associated with the late-nineteenth-century farmstead with a farmhouse, two barns and agricultural fields. The property should be included on the Town's Municipal Heritage Register and should be prioritized for research and evaluation for potential designation under the Ontario Heritage Act.
- H8.12.5 In evaluating development applications, the Town:
- a) Will encourage the use or adaptive reuse of cultural heritage resources, or key components of such resources, whenever possible as part of the new development in situ, or on an alternate site; or
 - b) May, where resources which are not designated and are not to be conserved, request the documentation of such resources in a cultural heritage evaluation report with a detailed property history, architectural description and photographic recording.

The Town may impose, as a condition of any development approvals, the implementation of appropriate measures to ensure the conservation of any affected cultural heritage resources, and where appropriate, their integration into new development.

- H8.12.6 The Town will require a Cultural Heritage Impact Assessment be prepared in accordance with section F5.1.2 of this Plan to determine the resource's specific heritage significance and to establish appropriate conservation plans and/or mitigation measures be prepared where development or redevelopment is proposed:
- a) on, within, adjacent to, or in the immediate vicinity of any designated cultural heritage resource; or
 - b) on a property listed on the Town's Heritage Register.

The Town may require a Cultural Heritage Impact Assessment be prepared in accordance with section F5.1.2 of this Plan to determine the resource's specific heritage significance and to establish appropriate conservation plans and/or mitigation measures for any development or redevelopment proposal that affects any of the cultural heritage resources mentioned above in policies H8.12.3 and H8.12.4.

- H8.12.7 The conservation of cultural heritage resources should be integrated with the conservation strategies for natural heritage features and environmentally sensitive areas where feasible.
- H8.12.8 New development should be compatible with, legible from and subordinate to cultural heritage resources in their context (both those within and those adjacent to the Secondary Plan area) through consideration of such matters as, but not limited to, building height, massing, scale, setbacks, stepbacks, roof line and profile and architectural character and expression.
- H8.12.9 Development that impacts areas identified for potential archaeological significance will require assessment in accordance with Provincial requirements and the Regional Archaeological Management Plan to determine the presence of archaeological resources and, if required, provide recommendations for mitigation and conservation. Development and site alteration may be permitted after the significant archaeological resources have been conserved by removal and documentation, or preservation on site. Where significant archaeological resources must be preserved on site, only development and site alteration, which maintains the heritage integrity of the site, will be permitted.

H8.13 LAND USE COMPATIBILITY

The Official Plan recognizes that residential and institutional uses are sensitive to noise, vibration, odour, dust or other emissions, particulates or contaminants from industrial, transportation and utility sources. Schedule III has identified that the need for buffering

and/or mitigation must be addressed adjacent to existing residential uses. Where new facilities are proposed in proximity to existing sensitive uses, a proponent will be required to undertake the necessary impact analysis and implement, as a condition of approval, the appropriate abatement measures, in accordance with Ministry of Environment, Conservation and Parks guidelines and the Halton Region Land Use Compatibility Guidelines. The potential for impacts (Air and Noise) associated with new facilities will be addressed by provincial permitting and review tools such as Environmental Compliance Approvals, Environmental Activity and Sector Registry, or Environmental Assessments.

H8.13.1 Noise and Vibration

H8.13.1.1 Noise can be expected to vary significantly within the Secondary Plan area and will occur from both stationary sources (the employment and commercial land uses) and transportation sources within and in the vicinity of the Secondary Plan area.

H8.13.1.2 A Noise and/or Vibration Impact Study will be required in accordance with Ministry of Environment, Conservation and Parks guidelines and the Halton Region Land Use Compatibility Guidelines to the satisfaction of the Town in consultation with the Region and will address Environmental Protection Act requirements with respect to existing sensitive land uses. The Noise Impact and/or Vibration Study will consider potential impacts and address mitigation measures through building design, setbacks and/or other measures such as berms and fencing. The Study will be provided early in the site plan approval process to assist in determining the most efficient way to mitigate noise emissions. Changes such as building orientation can significantly affect noise impacts at nearby sensitive land uses and address applicable Ministry of Environment, Conservation and Parks noise criteria without the need for other costly or operationally restrictive mitigation measures.

H8.13.1.3 Design measures to avoid queuing or traffic congestion such as strategic siting of entrances and exits and a reasonable setback from sensitive land uses may be required as part of site plan approval in order to limit nuisance effects associated with the truck traffic.

H8.13.2 Air Quality

H8.13.2.1 Air quality studies will be required in support of Class II industrial development in accordance with Ministry of Environment, Conservation and Parks guidelines and the Halton Region Land Use Compatibility Guidelines unless it can be demonstrated using a screening that no potential air quality effects are likely.

Odour may be caused by discharges from a variety of sources including stationary sources such as buildings, outdoor sources, or fugitive sources such as equipment leaks. As a result, odour assessment, and odour management plans and control measures that are specific to the facility that is being proposed should be required at the Site Plan application stage to avoid odour release and off-site effects.

- H8.13.2.2 Effective fugitive dust management will be required in support of Class II industrial development (including warehouse/distribution facilities) in accordance with Ministry of Environment, Conservation and Parks guidelines. A Best Management Plan outlining procedures and practices to prevent nuisance effects and deposition of fugitive dust should be prepared in support of development applications.

H8.13.3 Other Compatibility Issues

- H8.13.3.1 All development applications will be required to identify means to minimize and mitigate light pollution and should consider International Dark Sky guidelines. The Town may consider developing a strategic lighting master plan that addresses both private lighting of facilities and municipal lighting of roadways and supporting facilities such as future transit stops in order to address:
- Light intrusion or light trespass of unwanted light onto adjacent properties and the Natural Heritage System;
 - Timing of lighting; and
 - Light intensity, spectrum, clutter and glare.
- H8.13.3.2 Construction activities can be a source of emissions and should be managed to control effects from these emissions, with consideration of scheduling, monitoring and mitigation.
- H8.13.3.3 Effective communication with residents during planning and construction phases will be encouraged and the Town may consider establishing a public liaison committee to encourage resident participation.
- H8.13.3.4 New or expanding sensitive land uses, if proposed within the Secondary Plan area, will require the completion of a land use compatibility assessment to ensure that the use is appropriate and the location and design of the use can minimize and mitigate any adverse impacts on industrial, manufacturing or other uses that are particularly vulnerable to encroachment. Land use compatibility requirements in the Provincial Policy Statement must be achieved for development in proximity to major facilities.

H8.14 URBAN DESIGN

H8.14.1 Urban Design Policies

- H8.14.1. The Secondary Plan area is intended to reflect its role as a high profile gateway into the Town and to ensure that future development reflects high quality architectural and urban design standards that support the corporate image envisaged for it. All development within the Secondary Plan area will address the Urban Design objectives and policies in section I2 of this Plan, which are intended to ensure an attractive public realm, a comfortable and engaging environment and a consistent level of quality on both public and private lands. The highest quality of development will occur at key locations, particularly at the Winston Churchill Boulevard and Steeles Avenue intersection, in order to promote a distinct visual presence and sense of arrival.
- a) The view of the Town from the proposed Highway 413 will be significant in presenting an image of the community for both residents and visitors. In order to enhance that image, the following policies apply to lands that abut the proposed Highway 413:
- i) The Town will expect a high level of architectural design and finish on the facades of buildings that are visible from the highway;
 - ii) Properties that abut the highway must be designed to have two "faces", one to the internal street network, and one to the highway;
 - iii) High quality, well maintained landscaping, will be required along all lot lines that abut the highway; and
 - iv) Parking and loading facilities will be permitted between the highway right of way and the building, but these must be limited in nature, and screened from view of the highway.
- H8.14.1.2 The zoning by-law and site plan approval process will assist in controlling development along Winston Churchill Boulevard and Steeles Avenue to ensure a high quality of site design. In particular:
- a) buildings will be designed to front on these roads;
 - b) parking including the parking of transport trucks and trailers and delivery trucks regularly used in the business operation will be at least partially screened from these streets by berms and landscaping;
 - c) the majority of service and loading facilities will be screened from these roads; and,
 - d) safe and functional vehicular and pedestrian access will be provided.

H8.14.1.3 Development standards, including zoning provisions and urban design guidelines, will be coordinated with the other phases of the Premier Gateway Employment Area in order to provide a cohesive built environment and an overall unified employment area.

H8.14.1.4 Development will support sustainable site and building design and construction practices.

H8.14.2. Urban Design Guidelines

H8.14.2.1 The Premier Gateway Employment Area Urban Design Guidelines have been prepared to establish a planning framework that will help the Town achieve its goal to create a dynamic, accessible, and economically competitive employment area that includes sustainable development practices and have been updated to include the Phase 2B lands. The Premier Gateway Employment Area Urban Design Guidelines provide guidance on the creation of high quality streetscape design and urban design which is supportive of transit and active transportation.

H8.14.2.2 The Premier Gateway Employment Area Urban Design Guidelines are based on the following principles:

- a) Enhancing Natural Features and mitigating impacts;
- b) Achieving a Strong Visual Identity for Trafalgar Road, Steeles Avenue and Winston Churchill Boulevard;
- c) Developing Nodes at three Gateway Locations;
- d) Ensuring a High Quality of Built Form and Landscape Development;
- e) Providing Flexible Development to Accommodate Evolving Needs;
- f) Encouraging the Protection and Adaptive Reuse of Significant Cultural Heritage Resources; and
- g) Encouraging the Construction of Sustainable Buildings.

H8.14.2.3 The Premier Gateway Employment Area Urban Design Guidelines address matters related to site plan design, built form design, and environment and open space.

H8.15 SUSTAINABLE DEVELOPMENT

H8.15.1 This Plan is intended to facilitate development by addressing such matters as: energy and water; ecology; air quality; resiliency; transportation; and innovation. References are made throughout this Secondary Plan to where sustainable development practices may be considered or encouraged. Further guidance is provided in the Premier Gateway Urban Design Guidelines and the Town's Green Development Standards.

- H8.15.2 The Green Development Standards Section in the Official Plan addresses how development will have been deemed to have met the goals for sustainable or green development.

H8.16 PHASING

- H8.16.1 Development should occur in a logical and orderly manner to ensure that major capital projects including municipal services and transportation network upgrades are provided in a cost-effective and timely manner as the area develops. Development is generally expected to occur from west to east as municipal servicing becomes available.
- H8.16.2 Development on full municipal services will only be permitted to proceed if servicing capacity is available. Until commissioning of the proposed Eighth Line/Trafalgar Trunk Wastewater Sewer occurs, there are downstream capacity constraints that may impact servicing allocation to proposed development within the Premier Gateway Employment Area lands.
- H8.16.3 Prior to the development of land east of the proposed Highway 413, construction of the Steeles Avenue trunk watermain east of the proposed Highway 413 will be required, as well as a looped watermain system that includes the crossing of the proposed Highway 413 in association with the planned east-west collector road. If the timing of the Steeles Avenue trunk watermain is delayed, the feasibility of an alternative water supply may be considered.
- H8.16.4 Lands located east of the proposed Highway 413 will require the ultimate waste water servicing solution to be constructed by the Region to discharge into local sewers, the component of Steeles Avenue trunk sewer constructed to west of the proposed Highway 413 or directly into the Eighth Line Trunk Sewer.
- H8.16.5 Updated Municipal Class Environmental Assessments or Amendments to the existing approved Municipal Class Environmental Assessments may be required for projects impacted by the proposed Highway 413. Future Municipal Class Environmental Assessments may be completed through Integration with Planning Act Applications.
- H8.16.6 A more detailed implementation plan for servicing and transportation infrastructure improvements should be prepared once the Region has completed the updates to the Water and Waste Water Master Servicing Plan and Transportation Master Plan in order to articulate any updates to the required improvements or their timing. The availability of water and wastewater services may also be impacted by decision related to the proposed Highway 413.

H8.16.7 Private front-end construction of water and waste water services to accommodate development ahead of the Region of Halton planned implementation may be considered as part of development applications.

H8.16.8 Landowners within the Secondary Plan Area may be required to enter into a private cost sharing agreement or agreements amongst themselves to address the distribution of costs of development for the provision of servicing or transportation infrastructure. Cost sharing agreements should consider best management practices.

H8.17 IMPLEMENTATION

H8.17.1 The Secondary Plan will be implemented and interpreted in accordance with the provisions in the Official Plan pertaining to the administration and implementation of the Plan.

H8.17.2 The Town, at its discretion, may request a peer review of any of the reports, plans and/or studies required in support of complete applications.

H8.17.3 Glossary

The following terms are defined to assist in understanding the intent of this Plan.

Major Retail means a large scale or large format stand alone retail stores or retail centres that have the primary purpose of commercial activities.

Trailer Waiting Spaces means an on-site designated commercial vehicle waiting spaces that are restricted in scope and/or extent in the Zoning By-law. Trailer Waiting Spaces do not include loading spaces. No other commercial vehicle parking is permitted, as it would be considered outdoor storage.

Regulatory allowance means the distance from a hazard prescribed in the Conservation Authority's Regulation.

H8.17.4 For the purposes of the Plan, any italicized terms in this Plan reference defined terms in the Region of Halton Official Plan.

Premier Gateway Employment Area Phase 2B Secondary Plan, September 2023

SCHEDULE H8

Premier Gateway Employment Area
Phase 2B Secondary Plan Land Use
Plan



APPENDIX A
Subwatershed Impact Study
Matters to Be Addressed

The Subwatershed Impact Study will:

- a) Update the characterization of features that are recommended to be integrated in the *Natural Heritage System*;
- b) Refine the natural heritage and natural hazard limits reflecting the *Natural Heritage System* objectives and other intentions of the subwatershed study (i.e. final staking of *Natural Heritage System* features and buffers, calculation of riparian storage volumes, etc.);
- c) Delineate the location of key *Natural Heritage System* features, and Conservation Halton and Credit Valley Conservation regulated areas based on site specific topographic survey information and detailed technical studies regarding watercourses, flooding hazards (eg. floodplain analysis), erosion hazards (eg. physical top of bank staking, geotechnical slope stability and/or meander belt erosion assessments) and wetlands (wetland limit staking and Ontario Wetland Evaluation System). This includes addressing policy H8.7.4.15.3 where applicable;
- d) Conduct feature-based wetland and pond water balance assessments for those features that have been identified as Significant Wildlife Habitat and/or associated with watercourses or any "protection" or "conservation" rated headwater drainage features;
- e) Confirm presence of turtle nesting and wintering habitat, snake hibernacula, and amphibian breeding habitat in areas not surveyed in the Phase 2B Scoped Subwatershed Study;
- f) Conduct Species at Risk surveys and follow Endangered Species Act regulations in consultation with the Ministry of Environment, Conservation and Parks, where required;
- g) Confirm the watercourse constraint ranking and Headwater Drainage Feature classification and which may include a Natural Channel Design Brief if relocation is being proposed;
- h) Determine the preferred servicing plan;
- i) Determine the road layout;
- j) Verify and refine, as appropriate, the location and sizing criteria for stormwater management facilities;
- k) Define phasing in areas of multiple ownership;
- l) Establish and fulfill terrestrial and aquatic natural heritage and surface water and groundwater monitoring requirements including duration, methodology and purpose for the components being monitored for pre, during and post development and define cost sharing for monitoring programs;
- m) Refine the meander belt width delineation, hazard setbacks and regulatory setbacks;
- n) Refine the limits of the *Natural Heritage System* with respect to updated watercourse corridor widths;
- o) Develop further characterization of the groundwater resources associated protection measures, and establish mitigation techniques to ensure pre to post water balance is provided to maintain hydrological form and function long term;

- p) Verify the appropriate management of the water budget based on the proposed development and stormwater management plan;
- q) Determine the detailed road alignment and configuration of watercourse and valley crossings, determine the potential impacts on these features and establish the appropriate mitigation measures to ensure the form and function is maintained;
- r) Prescribe site specific standards and preliminary design for landscaping, implementation and the management of corridors, wetland creation areas, buffers, and restoration areas;
- s) Develop strategies to enable construction phasing while allowing rescue of biota from small isolated habitats, and maintenance of the *Natural Heritage System* resources and functioning through the construction period;
- t) Develop the use of Low Impact Development measures;
- u) Undertake a preliminary analysis of the stream corridor dimensions required to maintain the pre-development riparian strategy; and
- v) Determine the location and design of any trail system.

As stated in section H8.8.3, the applicant will consult with the Town, the applicable Conservation Authority and the Region of Halton in order to finalize the Terms of Reference before undertaking a Subwatershed Impact Study (SIS).

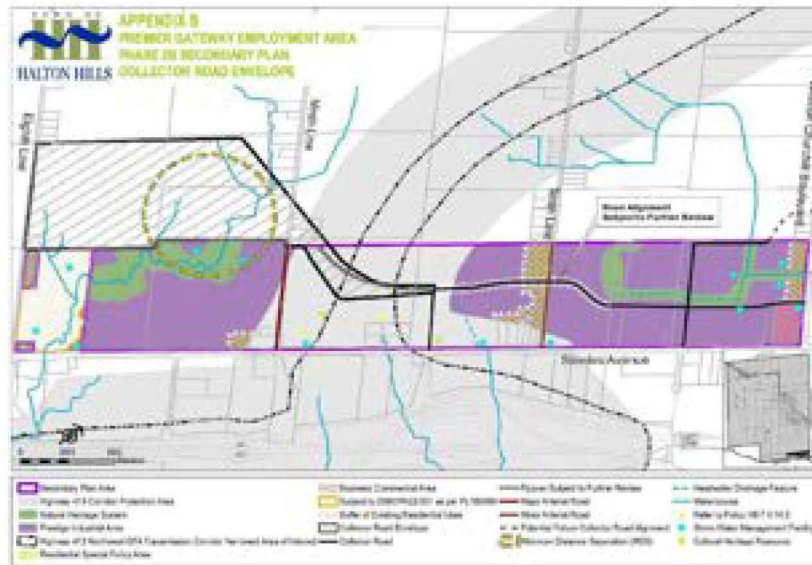
It is noted that within Sixteen Mile Creek, the flood hazard limits will be based on the Subwatershed Study modelling, refined as necessary through a SIS or other subsequent detailed study, until such a time that the CH Sixteen Mile Creek Flood Hazard Mapping (FHM) Study is substantially completed. Flood hazard delineations may be required to use the updated modelling from the Sixteen Mile FHM study, once it becomes available, and the stormwater design strategy will use the criteria within the Town's Subwatershed Study, refined as necessary, by the SIS. Once the Sixteen Mile Creek FHM is substantially complete, applicants may be required to use the updated modelling from this study to demonstrate the proposed stormwater management strategy will have no unacceptable impacts to flood hazards.

Premier Gateway Employment Area Phase 2B Secondary Plan, September 2023

APPENDIX B

Premier Gateway Employment
Area Phase 2B Secondary Plan
Collector Road Envelope

Premier Gateway Employment Area Phase 2B Secondary Plan, September 2023



Premier Gateway Employment Area Phase 2B Secondary Plan, September 2023

Schedule 1 to OPA 50



HALTON REGION ACCESS MANAGEMENT GUIDELINE

ACCESS MANAGEMENT GUIDELINE

Dated: January 2015



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1.0 INTRODUCTION

This Access Management Guideline document provides the context for access approval for Halton Regional Roads. Its purpose is to provide direction to developers, planners, and consultants on Halton Region's best practices in access management. It also provides a reference for the review and approval of new and redevelopment access applications.

The purpose of the Access Management Guideline document is to provide a framework for access control that will maintain a high level of service for through-traffic, while providing reasonable access to abutting properties. The overall goals of the Guidelines are to reduce collisions, reduce energy consumption, preserve the long-term integrity of the traffic movement function while balancing the needs of all road users.

The degree of access control is directly related to the functional classification (See Appendix A) of the individual road and is generally comprised of the following:

- Access to land development via the arterial/collector road system
- Physical control of left turn movements at mid-block locations
- Appropriately spaced right-in / right-out accesses

1.1 ACCESS MANAGEMENT – BEST PRACTICES

Access Management allows the Region the authority to effectively manage the provision of access to the public road system for new development or redevelopment. The major objective is to provide a safe access which is consistent with the function and operation of the public road system and access needs of the adjacent land uses. In addition, it achieves the necessary balance between traffic movement and land use access by careful control of the location, type and design of driveways and intersections. As indicated in Transportation Association of Canada – 1999 Geometric Design Guide for Canadian Roads (TAC Manual) “access management provides a systematic means of balancing the access and mobility requirements of roads. Access management is the process that manages access to land development while simultaneously preserving the flow of traffic on the surrounding public road system in terms of safety, capacity and speed.”

The Traffic Engineering Handbook, 6th Edition indicates that direct access to/from arterials should be discouraged or denied except where no alternative access exists. In addition, access to a property should be obtained from the local road system where possible. Where access must be provided from an arterial road, it should be proven to be safe without affecting the capacity of the roadway and balance the needs of all road users using criteria established within this Guideline

Before permitting an access, generally, a raised centre median should be in place to physically restrict the access and reduce turning conflicts and potential collisions. Auxiliary turning lanes should be incorporated with all types of accesses. As indicated in the, US Department of Transportation - Access Management Guideline for Activity Centres, left turn lane and right turn lanes are essential from both a safety and capacity perspective. It has been noted that the

frequency of and severity of rear end collisions will be reduced when left and right turn lanes are in place.

Sufficient sight distances for vehicles using the access is also essential. Vehicles should be able to enter and exit the access safely with respect to vehicles using the access and vehicles on the adjacent roadway. Sight distances should be met for all access types using the standards indicated from the TAC Manual and current Regional practices.

Notwithstanding the above information, a Transportation Impact Study may also be required prior to determining the location of an access. The goal of a Transportation Impact Study is to assess the potential affects of additional traffic being generated by a proposed development on the surrounding transportation network to determine what provisions are needed for safe and efficient site access and traffic flow and to address other related issues for transit and active transportation.

This Guideline is developed based upon existing state of the practice referencing technical guidelines, including the Region's Right-of-Way Guidelines, Traffic Impact Study Guidelines, 1999 TAC Geometric Design Guide for Canadian Roads, Transportation Engineering Handbook, Access Management Guidelines for Activity Centres (US DOT), MTO design guides, the Highway Capacity Manual, American Association of State Highway & Transportation Officials (AASHTO) and Transportation Research Board (TRB) publications, and other sources.

1.2 AUTHORITY FOR ACCESS MANAGEMENT

Halton Region has the authority to regulate access through a number of legislative processes. Access can be controlled under the legislative authority of the Planning Act and the Municipal Act. Halton can use the following planning tools as described in these acts:

- Official Plan Policies
- Site Plan and Subdivision Process
- Entrance Permits
- Proactive Corridor / Access Reviews

Official Plan

The Regional Official Plan provides a future vision of growth, land use and infrastructure in the Region. It is a policy document that sets out the basic goals, objectives and directions for long-term growth and development in the Region. It guides public and private decisions regarding development patterns, land use servicing, economic matters, and transportation infrastructure investment. The Access Management strategy outlined in this report builds upon the Region's policies contained in its Official Plan.

The Regional Official Plan sets the framework for implementing a policy to manage development. The Official Plan addresses access functions and sets general practice for access approval within the context of sound engineering practice.

Site Plan and Subdivision Plan Approval

Site Plan and Subdivision approval processes are carried out under the regulations of the Planning Act, which implement the land use policies of the Official Plan and Zoning By-laws. The Site Plan or Subdivision Plan process uses a set of drawings that illustrate the physical arrangements of property improvements, such as driveways, drainages, etc. Site Plan and Subdivision Agreements regulate in detail, items such as parking requirements, layout, access location, and access configuration. The Site Plan and Subdivision Plan approval process often involves items regarding access and access management for Regional roads.

Entrance Permit

Municipal Act allows municipalities to control the construction of accesses onto a public roadway through the issuance of permits.

An entrance permit is a site-by-site permission for access and a tool to regulate the location of the access prior to construction. Entrance permits allow the Region to protect the interests of both individuals and the community as a whole. By reviewing and approving plans before any work is done, the Region can ensure that the owner of the access comply with:

- The Official Plan which controls the functions and uses of access;
- The Guidelines and Standards, which sets requirements for design and materials; and agreements through site plan or subdivision relating to site configuration or required improvements.

1.3 ACCESS RESTRICTIONS

The Region reserves the right to temporarily permit a full movement access onto a Regional road, until such time that the roadway is reconstructed with a centre median at which time the full moves access would be restricted to a right in/out only. In these cases, this condition will be registered on the Title of the property and the Owner shall agree that it shall not initiate any action, suit or any other restrictions of access to and from the site; including, but not limited to any action for injurious affection.

Median breaks to allow for a full movement access connection shall not be permitted at locations where there are existing raised medians or HOV/transit facilities on the roadway. The Region maintains its right to modify and close accesses in order to ensure safe and efficient operation of the Regional road system. The right of municipalities to maintain safety through access management has been recognized in the Ontario courts.

1.4 TIMING

It is the intent of Halton Region that there is concurrency between the impacts of a development and the implementation of required infrastructure. Every effort must be made to have all road and traffic improvements are to be in place prior to occupancy of the development. It is recommended that no development occupancy shall be permitted prior to completion of all road infrastructure work and inspection.

2.0 ACCESS TYPE

When all options for access to the subject land from local and non-arterial roads are not feasible, the following is a list of types of accesses which may be acceptable, with conditions, on Regional Roads:

- Right in/out – Section 2.1;
- Full Movement – Section 2.3;
- Partial Movement (Left in/out) – Section 2.4;
- Mutually Shared Driveway – Section 2.5;
- Emergency – Section 2.6;
- Construction and Temporary – Section 2.7; and
- Rural Driveway – Section 2.8.

2.1 RIGHT IN/OUT ACCESS

Right in/out accesses should be considered where necessary and only when the minimum spacing requirements can be achieved. (See Access - Spacing Section 3.2) Where possible, the developer will be responsible to construct a centre median with a minimum of 45 metres on either side of the curb extension with the objective to provide a continuous centre median. At locations where a centre median can not be constructed due to limited roadway width, adjacent access constraints, the developer will be required to build a directional island to physically restrict the movements at the access point. As a temporary measure, the Region will also consider the installation of flexible delineators when a Roads Capital project is imminent.

At locations where restricting access to a right in/out is not conducive to the surrounding area, due to existing adjacent accesses or other physical constraints, the Region, at its discretion may require the Developer/Owner to enter into an access agreement to restrict an access in the future, in conjunction with a future road widening project or under the direction of Regional Council for public safety and capacity reasons. (See Access Restrictions – Section 1.3)

Due to the current trend in Service Stations, it is necessary to provide specific requirements as it relates to the type and location of access. With the development of new service stations, only one right in/out access shall be considered, on each frontage to a Regional road, at the location furthest from the intersection with preference to achieve access at the furthest limit of the property. At rebuilds of existing service stations, all efforts will be made by staff to incorporate only one right in/out access at the furthest limit of the property frontage.

2.2 FULL MOVEMENT ACCESS

Full movement accesses are currently permitted, where necessary, when minimum spacing requirements can be achieved. (See Access Spacing – Section 3.2) A full movement access will only be considered when the safety and capacity of the roadway will not be adversely affected.

In addition to adequate spacing between intersection or full movement access, it is necessary to ensure that proper sight distances are achieved utilizing standards indicated in the TAC Manual for Geometric Design Guide for Canadian Roads and confirmed by field measurements.

The provision of future traffic control signals shall be considered in conjunction with a full movement access. The cost for the installation of traffic control signals shall be 100% paid by the Developer/Owner, unless identified in Halton's Capital Works program.

2.3 PARTIAL MOVEMENT ACCESS (LEFT IN, RIGHT IN/OUT)

Partial moves access (left in, right in/out) shall only be considered when an adequate left turn lane can be constructed to accommodate the necessary storage and taper for the proposed turning volume. In addition, the partial movement access shall ensure that satisfactory back to back left turn lanes with the adjacent intersections can be accommodated.

Although typical spacing for partial moves accesses will vary depending on the volume of each roadway and the necessary left turn storage and taper requirement, every effort should be made to maximize the spacing for these proposed accesses. As such a minimum spacing is recommended to be 235 metres obtained given that there are sufficient provisions made to provide for storage for left turning vehicles when considering this type of access. Access spacing is to be measured stop bar to stop bar.

In addition, a centre median shall be designed and built in such a way to physically restrict the left out movement.

2.4 MUTUALLY SHARED DRIVEWAY

Mutually shared or joint access requirements are used to connect major developments and to improve driveway spacing, which allows intensive development of a corridor, while maintaining efficient traffic operations, and safe and convenient access to businesses.

For commercial and high density developments, the Region will require the developers to establish shared commercial driveways when there are more than one business development at a given location, or a series of adjacent developments proposed over time. A shared entrance is beneficial and helps preserve the traffic carrying capacity of the arterial.

Halton Region supports and encourages the use of mutually shared driveway arrangements. Shared driveways must be registered on Title of both properties in order to protect the interests of both property owners in the event that either of the properties is sold.

2.5 EMERGENCY ACCESS

Emergency access is typically required when a development (particularly a subdivision) has only one connection to the local road network. Unlike typical access driveways, these emergency access driveways are infrequently, if ever, used and can be dealt with quite differently.

For emergency accesses to be effective, the design considerations must be addressed:

- The travel surface must be designed to withstand large truck loads (i.e. Fire trucks),
- The access must be clearly marked as such,
- The access must be controlled (bollards, chains, etc.) to ensure the access does not become a common access,
- The access width should be sufficient to accommodate large trucks (6 metres), and
- The design must make the emergency access serviceable (snow removal).

2.6 CONSTRUCTION AND TEMPORARY ACCESS

A construction or temporary access design must be considered like any other access when it comes to safety and design. Even though the access may be temporary, sufficient truck and car volumes may use the access.

There are three aspects to construction and temporary accesses that will be considered:

- Location - Often access from a Regional road is requested for construction purposes for new developments. Every effort will be made to locate the construction or temporary access in the location of the future street or driveway. Locating the access in these areas would ensure that the Access Design Guidelines have been met because the permanent access locations would have been previously reviewed.
- Maintenance. Maintenance must be undertaken to ensure that both road and boulevard are maintained and damages repaired.
- Duration. The access will have an annual renewal permit. A renewal process would ensure that the construction access has been maintained, and that unused construction accesses can be closed.

A construction or temporary access should be designed and installed in conforming to the Entrance Permit conditions. Appropriate mitigation measures must be implemented to prevent mud tracking from the development onto Regional roads. Securities, insurance and conditions will be provided as part of the Entrance Permit.

2.7 RURAL DRIVEWAY

An application for a rural driveway to a Regional Road will be evaluated on a site by site basis. In general the following will be required:

- The access will have a minimum platform of 6 metres and 1.5 metres of side slope;
- If a culvert is required, the culvert shall be new, corrugated, 16-gauge galvanized steel spiral, polyethylene or approved equivalent;
- The diameter and length shall be determined on a site by site basis;
- Culverts shall be backfilled with a minimum compacted cover of 300 mm of Granular 'A'. The remainder of the access, within Halton Region's right-of-way, shall have a minimum compacted thickness of 200 mm of Granular 'A';
- Driveway sideslopes shall be top dressed with a minimum 100 mm of topsoil and sodded and graded to a maximum of 3:1 ratio from the access driveway platform to the ends of the culvert invert; and
- Fencing, gates, curbs, lighting, headwalls, etc. shall not be constructed or installed within Halton Region's right-of-way limits.

Appropriate mitigation measures must be implemented to prevent mud tracking from the development onto Regional roads. Securities, insurance and conditions will be provided as part of the Entrance Permit.

3.0 ACCESS CONTROL CONSIDERATIONS

The access management process involves an approval and a design stage. Access approval involves the weighing of the merits and operational implications of an access to determine where access will be permitted and what form of access (i.e. Number of movements) will be allowed.

The following Halton Region documents should be reviewed in the access approval process:

- Guidelines for the Preparation of Transportation Impact Studies
- Right-of-Way Guidelines
- Design Guidelines for Proposed Road and Traffic Signal Works on Region of Halton Roads

Each proposed access must be shown to maintain the safety and efficiency of the arterial roadway. Despite the potential merits of an access, operational implications must be considered when evaluating a proposed access to a Regional road. It is the Region's common practice to request a Transportation Impact Study when evaluating a proposed access location.

3.1 DIRECT ACCESS vs INDIRECT ACCESS

The number of direct access points to the Regional Road network must be effectively managed to reduce delay, minimize turning conflicts, and maintain an acceptable level of safety for motorists, pedestrians and cyclists.

Therefore, before direct access to a Regional Road will be permitted, alternate access opportunities must be explored and the need for access to the Regional system must be demonstrated. The following are valid criteria used to consider direct access to a Regional Road:

- Land parcels are otherwise landlocked;
- Environmental and utility considerations/impacts;
- A site possesses unique constraints which negate any other access opportunities, such as the lot depth, the footprint of existing buildings, grades, or minimal frontage onto local streets; and
- Alternate access creates unacceptable traffic operational conditions on, or in close proximity to, the Regional Road.

A Transportation Impact Study is required to support all proposed direct access locations.

3.2 ACCESS SPACING

There are various reports and conclusions in the Traffic Engineering field that discuss the spacing recommendations for different types of access. The general spacing guidelines for right in/out access is 115 metres to 140 metres between accesses. Where spacing is not conducive to the land use, physical or economical reasons, a spacing of 30 metres to 60 metres may be considered

Although typical spacing for partial moves access (left in, right in/out) will vary depending on the volume of each roadway and the necessary left turn storage and taper requirement, every effort should be made to maximize the spacing for these proposed accesses. The Access Management Guideline for Activity Centres – US Department of Transportation indicates a minimum spacing of 235 metres should be obtained and with the provision to provide sufficient storage and taper for left turning vehicles when considering this type of access.

The general spacing guidelines for a full movements access is 300 metres to 400 metres. This range of spacing is based on the speed of the roadway, traffic signal coordination and storage requirements for left turning vehicles. Full movement accesses should be located at a point to allow enough spacing to the nearest signalized intersection to avoid any possible interference with intersection queues.

Beyond accommodating traffic movements, the demands on Regional roads are changing in that they also need to address multiple roles related to other users including transit riders, cyclists, and pedestrians. Regional streets are also an integral element in promoting high quality urban design, serving as entryways to communities and encouraging the development of pedestrian-friendly and transit-oriented neighbourhoods.

Halton Region has created Access Spacing guidelines (see Table 1) that correspond to the Region's Right-of-Way Guidelines which group the functional classification of roadways into three categories, Rural/NHS, Corridor and Node. The three categories reinforce an urban structure model that directs growth away from rural and natural heritage areas and towards identified urban growth areas within the Regional Official Plan.

Rural/Natural Heritage System (R)

Rural lands are designated areas for agriculture and protection of infrastructure that supports farming and Natural Heritage Areas (NHS) are lands designated for natural area conservation. In planning for new urban areas, the Region is seeking to minimize development of prime agricultural lands. Regional Roads in Rural/NHS lands should respect the rural character of the area.

Corridors (C)

Corridors are urban growth areas identified along major roads, arterials or higher order corridors that have the potential to provide a focus for higher density mixed-use development and employment use consistent with planned transit service levels. The design and physical appearance of corridors contribute directly to livability and economic success and therefore should offer a positive community environment and convenient access for residents and businesses to a variety of goods and services.

Corridors will generally vary in use along their length and their design needs to reflect the change in surroundings. Over time, corridors could include a mix of uses such as: sidewalk-fronting shops or businesses, offices, civic uses appropriately scaled and designed public spaces and a broad mix of residential forms and densities. Corridors that travel through employment lands are to provide for development of quality business environment and include a range of offices, industrial-type buildings and services supporting employment such as business related retail and restaurants located in buildings with doors and windows that front the street.

Node (N)

Nodes are defined as compact, transit-oriented, pedestrian/cyclist friendly and mixed use/residential neighbourhood centers that are areas of more intensive urban uses within a community. They provide area residents with a hub to meet a variety of daily needs (goods and services) and serve as a social focus for the community and as concentrations of office employment uses. Nodes are generally located at the intersections of major corridors within the identified intensification areas and extend approximately 200-400 metres from the intersection.

Halton Region's access spacing guidelines are further refined by providing spacing for cross-section type as identified in the Region's Right-of-Way guidelines. Please see Appendix B for the individual cross-sections by Rural/NHS (R), Corridor (C) and Node (N).

Table 1 outlines the minimum spacing requirements for access and road connections to Regional roads. As speed limits increase, greater minimum distance is required between access locations. Access spacing can be reduced to a minimum of 250 metres within the intensification areas (Node) identified within the Regional Official Plan that can be substantiated through the submission of a comprehensive corridor analysis and Transportation Impact Study analyzing all possible alternatives and taking into consideration land use and community factors. Access spacing is measured stop bar to stop bar.

Table 1 – Minimum Access Spacing

TYPE*	Full Movement Access (m)	Right in/out Access (m)
R1	400	115
R2	400	115
C1	400	115
C2	300	115
C3	300	115
C4	300	115
C5	300	115
N1	250	115
N2	250	115

3.4 SIGHT DISTANCE REQUIREMENTS

A safe sight distance is the distance needed by a driver on a Major Arterial, or a driver exiting a driveway or street to verify that the road is clear and to avoid conflicts with other vehicles.

Adequate sight distance must be provided for both movements into and out of an access with a minimum of hazard and disruption to traffic. Sight distance requirements must be considered both for vehicles approaching the access and departing from the stopped position at the access.

The sight distances should be designed to enable existing vehicles:

- Upon turning left or right, to accelerate to the operating speed of the street without causing approaching vehicles to reduce speed by more than 15km/h; and
- Upon turning left, to clear the near half of the street without conflicting with vehicles approaching from the left.

The operating characteristics (driver eye elevation, visibility of the vehicle, and vehicle acceleration characteristics) of both trucks and passenger vehicles should be considered if both vehicle types are anticipated to utilize the access.

3.5 DRIVEWAY AND SITE CONFIGURATION

Driveway location and design affects the ability of a driver to safely and easily enter and exit a site. Road classification, right-of-way, design speed, design hour volumes, and land use influence driveway location and design. For driveways to be permitted along major Regional roads, the design of the proposed driveways should be feasible to minimize interference with the mobility of the through traffic by designing the driveway to provide desirable:

- Driveway width – See Section 5.4 -Table 2
- Driveway radii – See Section 5.4 – Table 3
- Clear throat conditions

-
- Turning characteristics

3.6 ACTIVE TRANSPORTATION

In the design of a access, it is also important to integrate the pedestrian and cyclist needs into the vehicular operational requirements of the driveway. Separate accommodation for vehicles, pedestrians and cyclists is required for sites with significant pedestrian/cyclist flows.

Pedestrians and cyclists should be provided the right of way at sidewalk/driveway crossings by way of pedestrian and cycling facilities that should adhere to the standards set out in Ontario Traffic Manual Book 18. Generally, the sidewalk should extend through the driveway to illustrate to the driver that the pedestrian has the right of way. However, at driveways with very high vehicle volumes or vehicle speed and pedestrian safety concerns, consideration may be given to not carrying the sidewalk through the driveway to alert pedestrians to the vehicle/pedestrian conflict.

As development occurs within the Nodes and Corridors, Halton Region will work with the local Municipality to ensure that there is proper integration between pedestrian walkways, cycling paths and transit routes and vehicular access to development. Halton Region will also support any Municipal initiatives to encourage and increase safety for pedestrians and cyclists.

Accesses within Nodes and Corridors of high pedestrian volumes should be designed to maximize visibility of pedestrians and for reducing speeds of entering and exiting vehicles and drivers by using smaller driveway radii. All efforts will be made to consolidate access points within these areas in order to maximize pedestrian and cyclist comfort.

3.8 SUMMARY OF OPERATIONAL IMPLICATIONS

The evaluation of access locations will be assessed for the following specific operational criteria to determine whether a full movement or partial movement access is feasible. In most instances a Transportation Impact Study will be required.

- Insufficient gaps in the traffic flow
- Turn movements that conflict or overlap
- Conflicting movements with traffic signal operations
- Sight distance constraints (vertical or horizontal geometry, parking, etc.)
- Traffic flow queues that block the access
- Inadequate on-site traffic conditions (circulation, clear throat, or grades)
- Access impact on trees or other boulevard elements
- Impacts on downstream traffic operations
- Conflicts resulting from lane transitions (i.e. lanes beginning or terminating)
- Undesirable pick-up / drop-off or service vehicle activity

-
- Contributing to existing safety / operational problems
 - Encourages unsafe maneuvers (weaving, U-turns, wrong way, reversing)
 - Insufficient right of way for adequate road geometrics (radii, width)
 - Impact on pedestrian and cycling environment (excessive width or combination of access)
 - Impacts on adjacent properties
 - Transit operations and bus stop locations

4.0 MITIGATION OF ACCESS IMPACTS

Road and/or traffic signal control improvements are often required to mitigate the traffic demands of a proposed development. Where deemed appropriate by the Region, traffic control signals or other measures may be required to accommodate an access.

4.1 ROAD IMPROVEMENTS

Road improvements include any physical changes that must be made to the Regional Road network to mitigate the transportation impacts of a proposed access. These improvements can be modest or extensive, such as making changes to existing pavement markings or physically constructing a turn lane.

All costs associated with the design and construction of the access and roadworks to facilitate the development are 100% paid by the Developer/Owner.

4.2 TRAFFIC CONTROL IMPROVEMENTS

Traffic signal control improvements include any physical changes that must be made to the existing traffic control signal network to mitigate the impacts of a proposed development or access. These improvements may require the provision of additional signal hardware at an existing location and/or the installation of traffic control signals at a new location.

Any proposed and approved full movement accesses to a Regional Road will require the proponent to assess the need for traffic control signals by using the technical warrants established by the Ministry of Transportation Ontario (MTO) and Transportation Association of Canada (TAC), as well as engineering judgment. Traffic control signals will be required to be installed if warrants are met through the previous listed evaluation criteria. The traffic signals should be designed in reference with section 3 in the “Design Guidelines for Proposed Road and Traffic Signal Works on Region of Halton Roads”.

4.3 TRAFFIC CONTROL DEVICES

When the construction and alteration of an access takes place, the Region may require traffic control devices to be installed, relocated, or removed on a Regional Road. The traffic control devices include regulatory or warning signs, traffic control signals, delineators, pavement markings, etc. The Region or its designate shall be the only party, which undertakes the installation, relocation, removal, and maintenance of such devices on the Regional road right-of-way. An easement may be required on private property for traffic control devices that are part of the Regional system in order for the Region to undertake ongoing or future maintenance.

The Region may require traffic control devices to be installed, relocated, or removed on private property. All such work including the installation, maintenance, relocation, and removal of such devices shall be the responsibility of the applicant and must be completed to the satisfaction of the Region. Where identified by the Transportation Impact Study, traffic signals shall be designed and installed in accordance with the specifications and requirements detailed in the “Uniform Traffic Signal Specifications for Operating Authorities with the Region of Halton” manual.

All traffic control devices, including pavement markings and traffic signs, are to be designed and installed in accordance with the latest version of the Ontario Traffic Manual

5.0 ACCESS DESIGN

Access Design Guidelines identify the design thresholds from which desirable operations can be maintained. Access Design Guidelines are an important part of the access design process. These Guidelines help the slower turning traffic move off the arterial road more quickly and help the traffic leaving the driveway turn and enter the stream of traffic more efficiently.

The traffic activities that occur on a site in the vicinity of the proposed driveway can also affect the traffic operations of the Regional Road. Key objectives for the effective management of traffic operations are: to reduce delay, remove the need for undesirable movements such as backing out onto the Regional Road, minimize turning conflicts, and maintain an acceptable level of safety for pedestrians, cyclists and motorists.

There are a number of site configuration issues that affect the physical design of the driveway and traffic operations into and out of the driveway. The access design should consider the following:

- Sight Distance Requirements – Section 5.1;
- Driveway Design – Section 5.2
- Auxiliary Lanes – Section 5.3;
- Angle of Intersection – Section 5.4;
- Illumination – Section 5.5;
- Drainage – Section 5.6; and
- Drive-Thru – Section 5.7

5.1 SIGHT DISTANCE REQUIREMENTS

Decision Sight Distance

Decision sight distance is the distance required for a driver to detect an unexpected or otherwise difficult to perceive information source or hazard in a roadway environment that may be visually cluttered, recognize the hazard or its potential threat, select an appropriate speed and path, and initiate and complete the movement safely and efficiently. Under some conditions the added complexity of traffic and local conditions and driver expectancy may require longer times and distances to accommodate normal vehicle manoeuvres of lane changing, speed changes and path changes.

Decision sight distance is applied where pedestrians, vehicles or design features and topographic conditions must be addressed by the driver. Stopping sight distance is applied where only one obstacle must be seen in the roadway and dealt with. The difference between stopping sight distance in the context of decision sight distance is that the vehicle is forced to stop for some traffic condition, such as a queue of vehicles, rather than an object in the roadway.

In view of the complexity and variations in drivers' expectancy regarding situations associated with access management, **decision sight distance is the preferred criteria when reviewing**

access management. It allows drivers additional margin for error and affords them sufficient length and time to adjust their vehicle movements at the same or reduced speed of approaching traffic.

Turning Sight Distance

Turning sight distance is the distance a driver entering a roadway from an access can see such that the vehicle can accelerate to a speed without significantly interfering with the vehicles approaching from the right. According to TAC, it is assumed that the main line vehicle will slow down to a speed 85% of the design speed and there should always be a gap of at least 2.0 seconds between the turning vehicle and the main-line vehicle.

Drivers may require longer sight distances at critical locations, such as intersections where several sources of information compete, where the intersection is on or beyond a crest of a vertical curve, or, where there is substantial horizontal curvature on the approach to the intersection area. Treatments such as landscaping and public art may contribute to the corridor aesthetically and possibly operationally (in terms of speed management). However treatments should not affect sight lines for entering or exiting vehicles or pedestrian/cyclist visibility approaching access points.

The provision of adequate sight distance is required to accommodate a driveway on a Regional Road. Inadequate sight distances will affect traffic flows and contribute to reduce safety. Direct access from a property to a Regional Road will be denied when the turning and/or decision sight distance cannot be attained and when restrictions on turning movements to and from a proposed development would not be practical.

5.2 DRIVEWAY DESIGN

Driveway design affects the ability of a driver to safely and easily enter and exit a site. The following criteria should be considered when designing accesses to Regional Roads.

The traffic activities that occur on a site in the vicinity of the proposed driveway can affect the traffic operations of the Regional Road. Key objectives for the effective management of traffic operations are: to reduce delay, remove the need for undesirable movements such as backing out onto the Regional Road, minimize turning conflicts, and maintain an acceptable level of safety for pedestrians and motorists.

There are a number of site configuration issues that affect the physical design of the driveway and traffic operations into and out of the driveway.

Driveway Dimensions

Appropriate driveway width and radii are important design elements in ensuring safe and efficient flow of traffic to/from the Regional Road. The driveway dimensions should reflect:

- The proposed land use;
- The operational type (1-way or 2-way traffic flow);

- The traffic volume; and
- The design vehicles the driveway will serve.

Generally, larger radii are applied with narrower driveway widths and vice versa.

Driveway Width

Driveway width should accommodate the appropriate design vehicles, control the location and angle of conflict points, and limit entry/exit to the intended number of lanes of operation. Whether a driveway will operate with one-way or two-way traffic flow should also be considered. **Table 2** illustrates recommended dimensions.

Table 2 – Driveway Dimensions (Width)

Width (m)	Urban			Rural		
	Residential	Commercial	Industrial	Residential	Commercial	Industrial
Minimum	3.5	4.5	6.0	4.9	5.5	6.0
Maximum	7.5	9.0	12.0	9.0	9.0	12.0

Source: Transportation Association of Canada – 1999 Geometric Design Guide for Canadian Roads

It should be noted that the minimum width of commercial and industrial driveways is intended to apply to one-way operation. In high pedestrian activity areas such as in urban growth areas as identified within the Regional Official Plan or in the same block with community centre, school or library, the maximum basic width should be 9 m. The width is measured perpendicular to the centreline of the driveway, in most instances, at the inner limit of a curbed radius or between the line of the radius and the near edge of a curbed island at least 4.6 m in area.

Driveway Radii

The radius of the curb return or amount of flare/taper of the curb connecting the edge of throat of a driveway with the edge of the nearest traveled lane affects the ease and speed of vehicles entering or exiting the roadway. **Table 3** illustrates recommended dimensions.

The following factors should be considered in designing the radius of the curb:

- The vehicle turning path making a right turn to or from the site;
- The width and nature of the driveway; and
- The width of the adjacent traffic lanes.

Table 3 – Driveway Dimensions (Radii)

Right Turn Radius (m)	Urban			Rural		
	Res.	Com.	Ind.	Res.	Com.	Ind.
Minimum	1.5	3.0	4.5	3.0	4.5	7.5
Maximum	4.5	16.0	16.0	7.5	24.0	24.0

Source: Transportation Association of Canada – 1999 Geometric Design Guide for Canadian Roads

Driveway Alignment

The introduction of a new driveway impacts directly on the existing traffic operations to and from the Regional Road. Careful integration of a new driveway into the existing operating character of the Regional Road is required to minimize turning conflicts and disruption to through traffic. A centreline of a new driveway to the Regional Road should align with the centreline of any opposing existing driveway or roadway.

Clear Throat Distance

Clear throat distance is the area provided on a driveway to store vehicles waiting to circulate into the site, usually a parking area. Failure to provide an adequate clear throat distance can create congestion and queuing of vehicles onto the Regional Road. This can result in as well as safety concerns for pedestrians/cyclists attempting to cross the driveway and on-road vehicle-vehicle conflicts.

The driveway throat should be designed to enable traffic leaving the site efficiently.

A queuing study may be required to determine that an appropriate amount of storage space is provided.

Adequate parking is required for all new developments. All parking areas should be designed to minimize conflict with traffic on Regional Roads. The amount of storage space required is directly related to the peak hour traffic demands of the parking lot, and should be assessed on a site-by-site basis.

5.3 AUXILIARY LANES

Auxiliary lanes can minimize the conflict between turning vehicles and following through traffic by providing storage space, which results in smoother traffic flow, and increased capacity and safety. Turning lanes should be utilized to minimize the difference in speed between turning and through vehicles and used to eliminate encroachment of turning vehicles into adjacent lands. Considerations will be made for transportation infrastructure, environmental and utility conflicts. All auxiliary lane designs must be in accordance with the TAC Manual – 1999 Geometric Design Guide for Canadian Roads.

Right Turn Lanes

Right turn tapers are required on Regional Roads at accesses that:

- Have sufficient volume of decelerating vehicles;
- Have right turn volume at signalized intersections of 10% of the through traffic volume;
or
- Have constrained sight distance approaching the access

The storage length required can be calculated by the following formula

$$S = NL/30$$

Where S = storage length

N = design volume (vph)

L = length of an average vehicle

The recommended taper is a function of design speed as summarized in **Table 4**.

Table 4 – Right Turn Lane Tapers

Design Speed	Design Domain for Taper Ratio	Parallel Lane Length (m)
50	11:1 – 17:1	35 – 75
60	14:1 – 17:1	40 – 90
70	17:1 – 20:1	50 – 110
80	17:1 – 24:1	60 – 130

Source: Transportation Association of Canada – 1999 Geometric Design Guide for Canadian Roads

Left Turn Lanes

When the number of left-turning vehicles entering an access is sufficient to create a hazard, and reduces capacity, consideration should be given to the provision of a separate left-turn lane. Left turn lane requirements are based on:

- Volume warrants;
- Roadway environment conditions such as sight distance constraints (there should be sufficient decision sight distance approaching a turning point);
- Safety considerations.

The minimum design length (parallel) of left turn lanes is 15 metres, however the storage plus the taper should at least equal the deceleration length. The recommended taper is a function of design speed as summarized in **Table 5**. The lower end of the range should only be used in constrained urban conditions.

Table 5 – Left Turn Lane Tapers

Design Speed	Design Domain for Taper Ratio	Horizontal Curve Taper R (m)
50	8:1 – 30:1	500
60	15:1 – 36:1	750
70	15:1 – 42:1	1000
80	15:1 – 48:1	1200
90	27:1 – 54:1	1500
100	30:1 – 60:1	2000

Source: Transportation Association of Canada – 1999 Geometric Design Guide for Canadian Roads

5.4 ANGLE OF INTERSECTION

The angle of intersection is the degree at which a driveway or road intersects with the Regional Road. It is desirable that the centerline of the new driveway and the centerline of the Regional Road meet at or nearly at right angles (90°) to ensure safe sight visibility when maneuvering to and from the site.

The angle of intersection at which a new driveway intersects with the Halton Regional Road should be 90°. A minimum acute angle of 70°, as measured from the driveway curb line, may be acceptable. An angle of intersection less than 70° or greater than 110° will not be permitted. For one-way driveways, a skewed intersection in the range of 45° to 60° may be appropriate in certain situations and should be discussed with staff to confirm.

5.5 ILLUMINATION

All roadway illumination design and installation shall be carried out in accordance with the latest version of the following or applicable standards:

- Guide of the Design of Roadway Lighting – Transportation Association of Canada
- Illumination of Isolated Rural Intersection – Transportation Association of Canada
- Design Manual for Traffic Barriers, Energy Attenuators, Light Poles – Ontario Ministry of Transportation

At-Grade Illumination

Roadway illumination is required for at-grade intersections and entrances to commercial/industrial/institutional/high density development where:

- Separate left-turn or right-turn lanes are provided;
- Traffic is channeled by means of raised median islands on one or more approaches;
- Traffic signal is installed;
- Multi-lane undivided approaches are present and warrants for traffic signals are at least 80 percent met; and
- Multi-lane undivided approaches are present and through traffic volumes is in excess of 6,000 AADT.

Provision for future illumination is required for at-grade intersections where the requirement to illuminate the intersection is not immediately met but increase in traffic and other factors in the future will warrant illumination. The provision of the underground ductwork underneath pavement structures will be required.

Structures

Roadway illumination is required on all structures where:

-
- Approaches to the structure are illuminated;
 - Traffic travels under a structure for a distance greater than 25 metres and where regular night time pedestrian traffic is expected; and
 - All pedestrian tunnels/over passes.

Provisions for future illumination is required for all structures. These requirements shall include duct bank along the structure and concrete plinths for future street light pole installations.

5.6 DRAINAGE

Accesses must be constructed so that they do not adversely affect the roadway drainage, drainage on adjacent properties or the drainage or stability of the roadway subgrade. The construction of an access must not cause water to flow across the road pavement, to pond on the shoulders or in the ditch or cause erosion within the highway.

Existing Gutter

Where drainage is carried along the curb, the access must be sloped to prevent run-off spillage into private property and the flow line of the gutter through the access shall be maintained.

Existing Ditch

A culvert pipe shall be installed where construction of an access necessitates crossing an open ditch. The culvert shall be installed such that the invert elevation is compatible with the profile of the existing ditch line. No ditch or gutter shall be filled without adequate alternate provision for drainage being made.

Culvert pipes shall be of a size adequate to carry the anticipated flow in the ditch as determined by the Region. In most instances, the size of the culvert pipe shall not be less than 400mm inside diameter.

The structural material and gauge of the culvert pipe shall be adequate to withstand the anticipated vehicular traffic across the access and shall meet the Regional specifications as identified in Halton Region's Geometric Design Guidelines.

5.7 DRIVE-THRU

Drive-thru facilities at car washes, banks, service station kiosks, and fast food restaurants can result in significant vehicle queues. Parking and circulation activities on these sites should be accommodated simultaneously without creating internal conflicts that may result in congestion or queuing on the Regional Road.

The provisions of a drive-thru facility should meet the following criteria:

- Drive-thru traffic is separate from other site traffic and parking facilities;
- The drive-thru area and the remaining parking lot circulation is clearly defined and delineated with appropriate pavement markings and signage;
- Adequate vehicle queue storage is provided to meet the peak design traffic demands so as not to interfere with pedestrian and/or vehicular movements and must be maintained within the site to prevent vehicles queuing on the roadway; and
- Discourage drive-thru facilities within the identified urban growth areas.

The amount of stacking space provided on a site with a drive-through must be sufficient to accommodate the range of potential tenants. This demand should be sensitive to the changes in potential uses and current surveys of demand. Sites with access to a Regional Road should provide sufficient storage for vehicles without impeding site circulation or blocking parking or site loading.

A queuing study may be required to determine that an appropriate amount of storage space will be provided.

6.0 REFERENCES

Association of State Highway & Transportation Officials (AASHTO) – Geometric Design of Highways and Streets.

iTRANS Consulting Inc., Literature Review for Durham Region RR 5 Corridor Study: Economic Impacts of Raised Medians. May 2000.

McCormick Rankin Corporation, Halton Functional Road Network and North Halton Transportation Study. May 1999.

Metropolitan Toronto, Access Management Guidelines for Development in Metro Toronto. April 1995.

NHI Course No. 15255, Access Management Location and Design. June 1998.

Oregon Department of Transportation. Right-in Right-out Channelization Discussion Paper No.13. October 1998.

The Traffic Engineering Handbook, 5th Edition October 1999

The Regional Municipality of Halton, Regional Official Plan. [2009]

The Regional Municipality of Halton, Access Management Plan for Regional Roads 5. November 1999

The Regional Municipality of Durham, Durham Mobility Study Working Papers #1-4. May 2000.

The Regional Municipality of Peel, Access to Regional Roads Report. April 27, 1993.

The Regional Municipality of Halton, Transportation Master Plan – Regional Roads 5 and 25 Corridor. November 1999.

The Regional Municipality of Waterloo, Policy and Procedures for Access onto Regional Roads. September 27, 1984.

Transportation Association of Canada (TAC), Geometric Design Guide for Canadian Roads. September 1999.

Transportation Research Board (TRB) Report – “Access Management Guidelines for Activity Centres” 2003, Federal Highway Administration

7.0 APPENDIX A

Region of Halton Transportation Facility Classification

Facility Type	Function	General Design Guidelines
Provincial Freeways	<ul style="list-style-type: none"> ▪ Serve mainly inter-regional travel demands ▪ Accommodate all truck traffic ▪ Accommodate higher order transit services and high-occupancy-vehicle lanes ▪ Carry high volumes of traffic ▪ Connect Urban Areas in different regions 	<ul style="list-style-type: none"> ▪ Full access control ▪ Minimum 4 travel lanes ▪ Noise-sensitive land uses to be discouraged along right-of-way
Provincial Highways	<ul style="list-style-type: none"> ▪ Serve mainly inter-regional travel demands ▪ Accommodate all truck traffic ▪ Accommodate higher order transit services and high occupancy vehicle lanes ▪ Carry high volumes of traffic ▪ Connect Urban Areas in different regions 	<ul style="list-style-type: none"> ▪ High degree of access control ▪ Transit-supportive, high density, mixed use development to be encouraged along right-of-way within urban areas ▪ Right-of-way requirements vary
Major Arterials	<ul style="list-style-type: none"> ▪ Serve mainly inter-regional and regional travel demands ▪ May serve an Intensification Corridor ▪ Accommodate all truck traffic ▪ Accommodate higher order transit services and high occupancy vehicle lanes ▪ Connect Urban Areas or Nodes different municipalities ▪ Carry high volumes of traffic ▪ Distribute traffic to and from Provincial Freeways and Highways 	<ul style="list-style-type: none"> ▪ High degree of access control ▪ Transit-supportive, high density, mixed use development to be encouraged along right-of-way within urban areas ▪ Right-of-way requirements up to 50m
Multi-Purpose Arterials	<ul style="list-style-type: none"> ▪ Serve a mix of functions of Major Arterials and Minor Arterials 	<ul style="list-style-type: none"> ▪ Intermediate degree of access control ▪ Transit-supportive, high density, mixed use development to be encouraged along right-of-way within urban areas ▪ Right-of-way requirements up to 50m
Minor Arterials	<ul style="list-style-type: none"> ▪ Serve mainly local travel demands ▪ May serve an Intensification Corridor ▪ Accommodate local truck traffic ▪ Accommodate local transit services ▪ Connect Urban Areas within the same municipalities ▪ Carry moderate to high volumes of traffic ▪ Distribute traffic to and from Major and Multi-Purpose Arterials 	<ul style="list-style-type: none"> ▪ Intermediate degree of access control ▪ Right-of-way requirement generally up to 35m unless specifically identified in a Local Municipal Official Plan

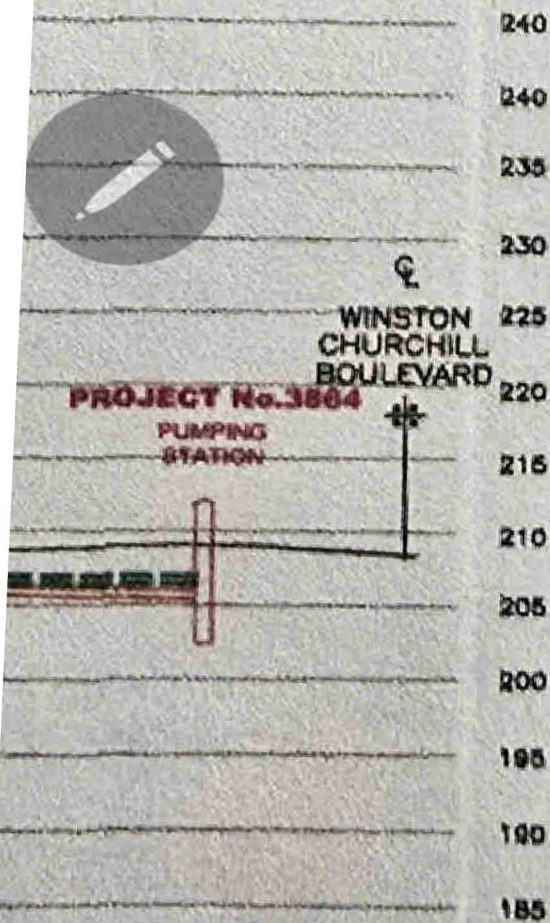
Higher Order Transit Corridors	<ul style="list-style-type: none"> ▪ Serve inter-municipal and inter-regional travel demands by public transit ▪ Serve an Intensification Corridor ▪ Accommodate higher order transit services ▪ Connect Urban Growth Centres and Mixed Use Nodes 	<ul style="list-style-type: none"> ▪ Exclusive right-of-way for transit vehicles where possible ▪ Transit-supportive, high density, mixed use development to be encouraged around stations
Major Transit Stations	<ul style="list-style-type: none"> ▪ Part of high order transit network ▪ Serve inter-municipal and inter-regional travel demands by public transit ▪ Complements Intensification Areas 	<ul style="list-style-type: none"> ▪ Connecting the higher order transit service with other transportation modes ▪ Facilities for mode transfers such as bicycle parking, automobile drop-off/pick up ▪ Surrounded by transit-supportive, high density, mixed use development to ensure the viability of high level of transit services
Rail Lines	<ul style="list-style-type: none"> ▪ Serve all types of people and goods movements by rail at the regional or national scale ▪ Accommodate commuter rail movement to Urban Growth Centres in the Greater Toronto and Hamilton Area and surrounding region 	<ul style="list-style-type: none"> ▪ Grade separations at intersections with other major transportation facilities where warranted ▪ Noise- or vibration-sensitive land uses to be discouraged along right-of-way ▪ Transit-supportive, high density, mixed use development encouraged around Major Transit Stations

Source: Halton Region Official Plan, 2009





HH#4 WWPS
Location



South Halton Master Plan Update

Steeles Avenue

Projects 3863, 3864, 3865,
4648, 5902, 5903 & 5904



Figure PP-15

JUNE 2008
H-1:5000
V-1:500
2863-D-55

14.3.1 Wastewater Servicing Strategy Review

14.3.2 Wastewater Treatment Plants

As noted the general servicing concept from the 2002 Master Plan remains valid and has been updated to reflect updated information on servicing requirements, new legislative requirements, capacity, scheduling and costing.

Mid-Halton Treatment Plant

The servicing strategy is based on providing all of the servicing for new growth areas in Oakville, Milton and the Halton Hills 401 Corridor at the Mid-Halton WWTP, which will be expanded to provide the needed capacity. Servicing of North Oakville is via a new east-west wastewater collection system on Dundas Street, which will convey flow to the Mid-Halton WWTP.

Milton Wastewater Treatment Plant

The Milton WWTP is maintained to provide capacity for servicing central and north west areas in Milton. Increased flows can be diverted to the Mid-Halton WWTP.

Skyway Wastewater Treatment Plant

The Skyway WWTP is expanded to service growth and intensification in Burlington.

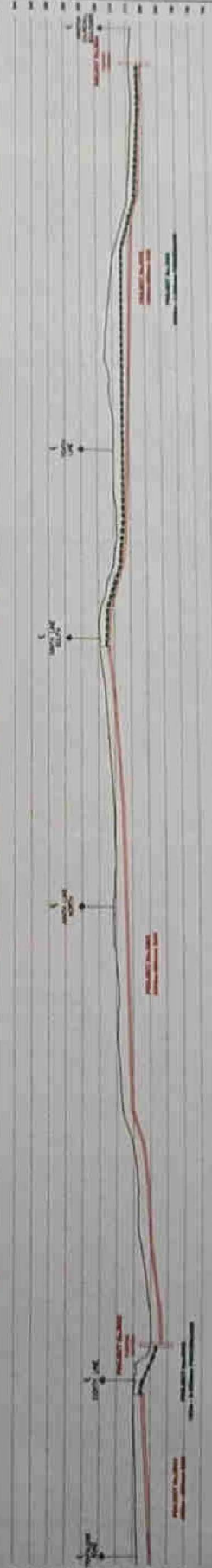
14.3.3 Pumping Stations

Based on the existing systems and investment in the existing infrastructure, ongoing use of pumping stations and building on the 2002 Master Plan strategy to convey flows to the Mid-Halton WWTP in general for the growth areas in Milton and North Oakville remains valid.

New sewage pumping stations have been added to the plan for Milton and the Halton Hills 401 Corridor to enable improved staged implementation and to minimize sewer depth.

Site locations and pumping capacity requirements have been updated based on updated planning data. Accelerated wastewater servicing needs have triggered acceleration of future expansions and updated planning information have provided greater detail to enable refinement of siting options. There may be requirement for refinement of the final locations for new pumping stations based on coordination with the completion of the area servicing plans for new development.

New facilities such as pumping stations as well as expansions to existing facilities will need to consider property requirements and related potential impacts. The site requirements for the new facilities will be located outside of the Conservation Halton's regulated areas, with final locations for new pumping stations located within the urban expansion areas coordinated where applicable through the respective subdivision development plans.

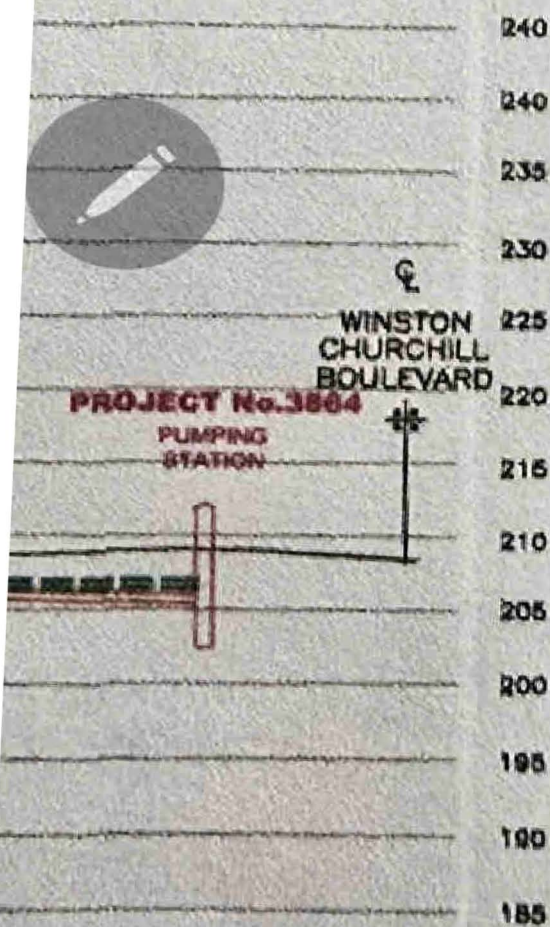


Steeles Avenue
Projects 2004, 2004, 2002
Phase 2, 2002, 2002 & 2004

Scale: 1" = 100'



HH#4 WWS
Location



South Halton Master Plan Update

Steeles Avenue

Projects 3863, 3864, 3865,
4648, 5902, 5903 & 5904



Figure PP-15

JUNE 2008
H-1:5000
Y-1:500
2863-D-55

Public Works Department
MEMORANDUM
Water Services

TO: Chris Campbell and Tom Ferguson, AECOM

FROM: Jacek Pawlus, Project Manager

DATE: September 9, 2011

RE: Sustainable Halton - Bulk Water Servicing Strategy

1.1 Introduction

Halton Region has identified the need to take an integrated approach to the short and long term servicing to manage its bulk water stations program. As part of the 2011 Sustainable Halton Water and Wastewater Master Servicing Plan (**Master Servicing Plan**), the anticipated bulk water demand, bulk water station improvements and expansions, preferred bulk water locations and cost estimates (see Capital Plan) for any necessary capital infrastructure were identified.

The overall goals of the Bulk Water Servicing Strategy within the context of the Master Servicing Plan are outlined below:

1. Develop criteria to evaluate existing and new sites.
2. Evaluate the existing bulk water station sites and units.
3. Assess the need for relocating existing bulk water station sites.
4. Identify and evaluate new bulk water station sites.
5. Assess the need for additional new bulk water station sites.
6. Identify a preferred Bulk Water Stations Servicing Strategy to benefit existing users and projected growth.
7. Determine Class Environmental Assessment Schedule types for each new bulk water station site.
8. Provide documentation to satisfy any schedule B projects in the Master Servicing Plan.

English literature

[illegible]

Haltom

CH - CONSERVATION ACTION
ICI - IND DEV.

March 12/13

Re: Pump Stn. No. 4 – (Public Infor. Centre March 13/13)

As long-standing residents, business owners and local employer, (we have been located in Whaley's Corner for more than 40 years) we have invested a significant amount of time, energy and resources in our home, business, and the community including contributing significant tax revenues to both the local and regional governments. We have built a growing local business whose model was founded in the rural character of the community, but as part of a planned future commercial gateway we have been investing significant resources into growing and modernizing the brand.

To date, the Region has failed to demonstrate the *necessity* of the proposed expropriation or that of the utility siting. We have reviewed the South Halton Water and Wastewater Master Plan and note that the proposed Site Plan location (Dwg. C900) is not the preferred utility location, as it is outside of the study area, outside the 2021 urban expansion limits, and refinement to the final location is not based on *coordination with the completion of the area servicing plans* or through an addendum. The consultation process and statutory offer have not been meaningful.

Requested mitigation summarised as follows:

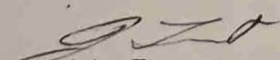
- Easement – (Contract R-2044B-2012, Dwg. 46 of 60) re-locate hydro pole (4+700) southerly, CSP as required, to maintain the easterly service alignment to the extent necessary to prevent encumbrance (<1.0m). New pylon sign at the southwest corner of the affected property (1.5m set back) to be coordinated.
- Lot Creation and Site Plan – proposed lot creation and site plan do not conform to the Town's design guidelines or official plan for the 401 Gateway Node. Provide local road access (S.1.2 Site Access) to site, residential and commercial interests within >20m buffer (S.3.4 Required Landscape Buffers). <http://www.haltonhills.ca/planning/pdf/401urbanDesignGuidelines.pdf>
- Services – leakage and spill of sewage, and hydraulic disruption from construction or use, or both, of the works, can only be acceptably mitigated by potable, sanitary, and well-decommissioning services.

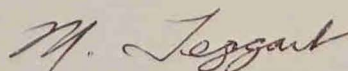
Further to our registered letter (March 7, 2013) regarding our objection to the Application to Expropriate Land – Part 9, 20R-19400, we are confirming our objection to the Lot Creation and siting of the Halton Hills Pumping Station NO.4 and confirming our intent to request a Hearing of Necessity and pursue a claim for damages should a reasonable solution not be negotiated.

Also see comments in attached 401urbanDesignGuidelines: pgs.3 Steeles Ave., 4 gateway node, 5 flexibility of development, 9 site access, 28 signage, 34 required landscape buffers, 40 gateways
Vii Site Plan Review

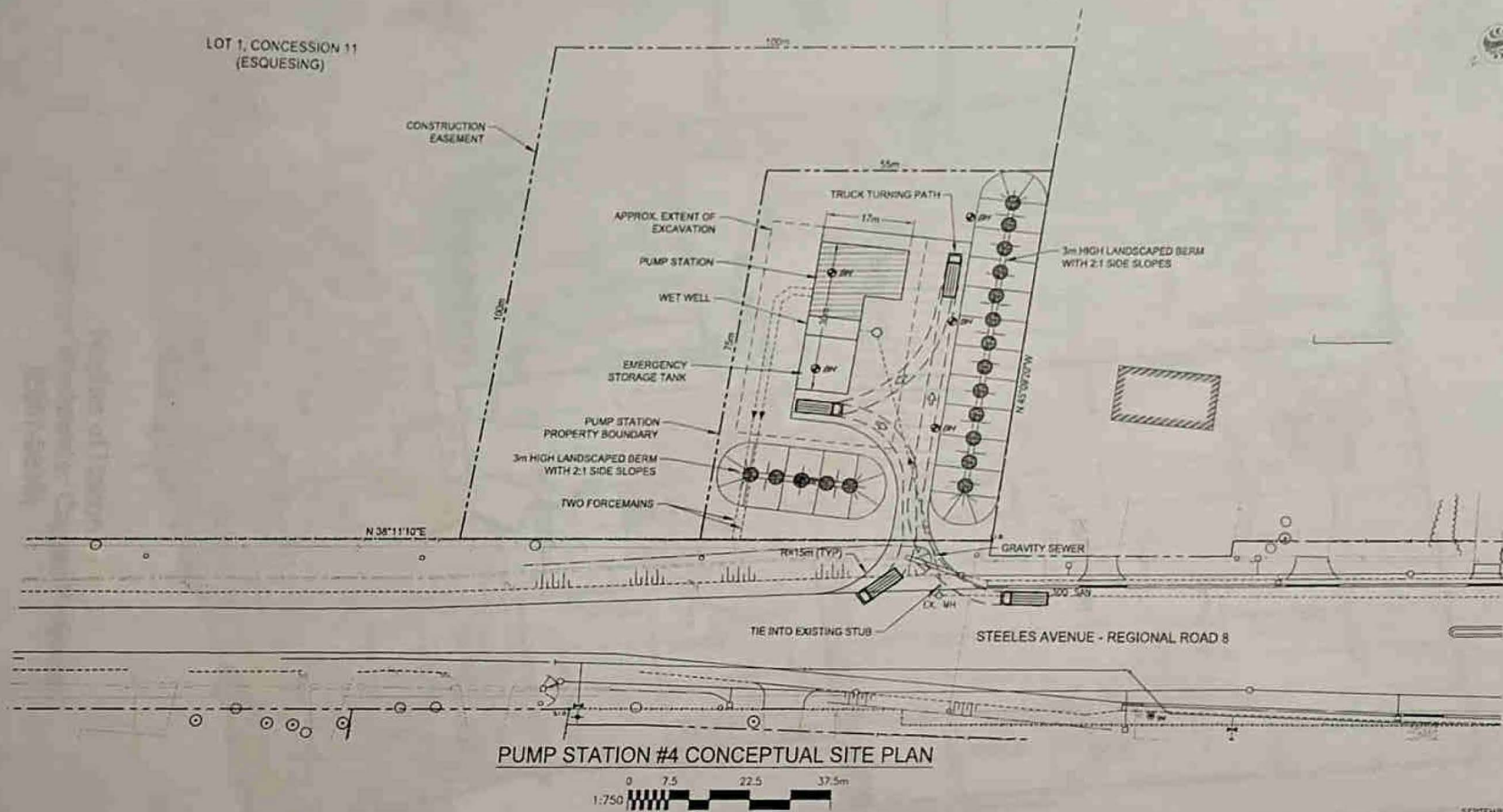
Attachments: 1. Drawing Mark-up to Rectify Easement

2. Drawing Mark-up Site Plan


John Teggart


Margaret Teggart

Public Information Meeting - March 6th (tentative, TBC)



Legend

PROPOSED BH/MONITORING WELL LOCATION

Notes



Stantec Consulting Ltd.
49 Frederick Street
Kitchener, ON Canada
N2H 6M7
Tel. 519.579.4410
Fax. 519.579.6733
www.stantec.com

Client/Project
REGIONAL MUNICIPALITY OF HALTON
HALTON HILLS PUMPING STATION #4

Figure No.
1.0
Title
CONCEPTUAL
SITE PLAN

SEPTEMBER 2012
16111062



April 9, 2013

Mr and Mrs Teggart
16863 Steeles Avenue
Halton Hills, ON L0P 1E0

Public Works
Wastewater Services
1151 Bronte Road
Oakville ON L6M 3L1

Dear Mr. and Mrs. Teggart:

RE: Proposed Steeles Avenue Pumping Station (Halton Hills No. 4)

Halton Region is designing a new Halton Hills No. 4 Pumping Station to be constructed in the vicinity of Steeles Avenue, west of Winston Churchill Boulevard.

We received your letter dated March 12, 2013 and wish to advise you that staff is actively considering the matters raised in your letter. Since the Public Open House on March 13, 2013 and since we received your letter, staff has revisited plans to design and construct the new station in the location shown at the meeting.

During the course of Halton Region's review of this pumping station you will receive additional opportunities to discuss this matter directly with staff and/or to respond to additional information provided to you by staff. In the coming weeks, we will send you a letter to update you on the many activities that have occurred since the Public Open House. In that letter, we will set out the next steps in this process for your information.

Inquiries or requests for additional information may be directed to Kenny Ching ext. 7426, or email to kenny.ching@halton.ca.

Sincerely,

David Ohashi, P.Eng.
Manager, Wastewater Design and Construction
905-825-6000 ext. 7074
david.ohashi@halton.ca

location was not
adjusted

cc. Councillor Clark Somerville, Ward 1 & 2, Town of Halton Hills
Magda Bielawski, P. Eng., Project Manager, Wastewater Planning, Halton Region
Kenny Ching, P.Eng., Project Manager, Wastewater Design and Construction, Halton Region
Janet Amos, Amos Environment and Planning

The Regional Municipality of Halton



Legislative and Planning Services
1151 Bronte Road
Oakville, ON L6M 3L1
Tel: (905) 825-6000 Ext. 7203
Fax: 905-825-3879
Toll free: 1-866-442-5866

April 2, 2013

REGULAR MAIL

16863 Steeles Ave RR2
Hornby, ON
L0P 1E0

Attention: Mr. John Teggart

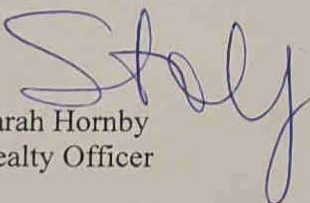
Dear Mr. Teggart,

**RE: Reconstruction of Steeles Avenue from 8th Line North to Winston Churchill
Boulevard, Town of Halton Hills (R2044B)
16863 Steeles Avenue, Town of Halton Hills**

As a result of our recent discussions and in preparation for the reconstruction of Steeles Avenue, please be aware that the placement of hydro poles/wires in the vicinity of your property has been reviewed. As a result, this will confirm that the Region of Halton no longer requires a permanent easement, described as Part 9 on Reference Plan 20R-19400, from your property.

Thank you for your cooperation and if you require additional information or clarification, I would be pleased to assist.

Yours very truly,


Sarah Hornby
Realty Officer

Encls.

cc Don Williams, Manager of Realty Services

unnecessary easement
was sought at
insulting valuation

The Regional Municipality of Halton

NOTICE OF APPLICATION FOR APPROVAL TO EXPROPRIATE LAND

IN THE MATTER OF an application by The Regional Municipality of Halton for approval to expropriate a fee simple, permanent easement and a 30 month temporary easement interest commencing on August 1, 2013, in the lands described in Schedule A set out below, registered in the Land Registry Office for the Land Titles Division of Halton Region No. (20), for the purpose of facilitating the reconstruction, improvement, extension and widening of Steeles Avenue (Regional Road No. 8) from Eighth Line North to Winston Churchill Boulevard (Regional Road No. 19) ("Steeles Avenue Reconstruction") in the Town of Halton Hills. In the case of the permanent easement interest, for entering on the lands with all necessary vehicles, machinery, workmen and other material for municipal purposes including but not limited to the construction, installation, maintenance and repair of associated public utilities including aerial and/or subsurface hydro lines, tiebacks, poles, anchors, guy wires and associated materials and equipment and any works ancillary thereto including utility relocation and site grading, and in the case of a 30 month temporary easement interest commencing on August 1, 2013 to enter with all necessary vehicles, machinery, equipment and material required to facilitate the reconstruction and maintenance of the Steeles Avenue Reconstruction, including, but not limited to, grading, placement and storage of soil and other material, installation of a storm drainage system, construction staging and construction ingress and egress associated with the Steeles Avenue Reconstruction, in the Town of Halton Hills and works ancillary thereto.

NOTICE IS HEREBY GIVEN that application has been made for approval to expropriate the lands described in Schedule A set out below, in the Town of Halton Hills.

Any owner of lands in respect of which notice is given who desires an inquiry into whether the taking of such land is fair, sound and reasonably necessary in the achievement of the objectives of the expropriating authority shall so notify the approving authority in writing

- (a) in the case of a registered owner, served personally or by registered mail within thirty (30) days after the registered owner is served with the notice, or, when the registered owner is served by publication, within thirty (30) days after the first publication of the notice;
- (b) in the case of an owner who is not a registered owner, within thirty (30) days after the first publication of the notice.

The approving authority is:

The Council of The Regional Municipality of Halton
1151 Bronte Road, Oakville, Ontario L6M 3L1

The expropriating authority is:

The Regional Municipality of Halton
1151 Bronte Road, Oakville, Ontario L6M 3L1

SCHEDULE A

Those lands described in the Town of Halton Hills, Regional Municipality of Halton, described as follows:

Fee Simple

Part of Lot 15, Concession 9, New Survey (Trafalgar) designated as Parts 6 and 7 on Plan 20R-18904, Parts 2, 3 and 4, on Plan 20R-18422;

Part of Lot 15, Concession 10, New Survey (Trafalgar) designated as Parts 3 and 4 on Plan 20R-18423, Parts 1, 2, 3, 4 and 5, on Plan 20R-18426;

Part of Lot 15, Concession 11, New Survey (Trafalgar) designated as Parts 1 and 2, on Plan 20R-18898;

Part of Lot 1, Concession 9, (Esquesing) designated as Parts 3, 5 and 6 on Plan 20R-18409;

Part of Lot 1, Concession 10, (Esquesing) designated as Parts 1 and 2 on Plan 20R-18905, Parts 1 and 2, on Plan 20R-18423;

Part of Lot 1, Concession 11, (Esquesing) designated as Parts 7 and 8, on Plan 20R-18426, Part 1, on Plan 20R-18427, Parts 1 and 2, on Plan 20R-18900

Permanent Easement

Part of Lot 1, Concession 9, (Esquesing) designated as Part 1, on Plan 20R-19400;

Part of Lot 15, Concession 9, New Survey (Trafalgar) designated as Parts 2 and 3, on Plan 20R-19400;

Part of Lot 1, Concession 10, (Esquesing) designated as Parts 4 and 5 on Plan 20R-19400;

Part of Lot 15, Concession 10, New Survey (Trafalgar) designated as Part 7, on Plan 20R-19400;

Part of Lot 1, Concession 11, (Esquesing) designated as Parts 8 and 9, on Plan 20R-19400.

Temporary Easement

Part of Lot 1, Concession 9, (Esquesing) designated as Part 4, on Plan 20R-18409.

Stephen F. Waque
T (416) 367-6275
F (416) 361-2708
swaque@blg.com

Borden Ladner Gervais LLP
Scotiabank Place, 40 King St W
Toronto, ON, Canada M5H 3J4
T 416.367.6000
F 416.367.6749
blg.com

BLG
Borden Ladner Gervais

File No. 013773/000177

February 11, 2013

Delivered by Registered Mail

John Teggart
16863 Steeles Avenue
RR#2, Hornby, ON
L0P 1E0

Dear Sir/Mesdames:

**Re: Regional Municipality of Halton – Steeles Avenue Expropriation
Owners: John Teggart
Part 9, 20R-19400
Your Interest: Owner**

This Notice of Application for Approval to Expropriate Land is being served upon you because it appears that you are a “registered owner” or “owner”, as defined by the *Expropriations Act*, R.S.O. 1990, c. E.26, of the land required by The Regional Municipality of Halton.

The Region of Halton requires a permanent easement interest in the lands more particularly described in the attached Notice for the purpose of facilitating the reconstruction, improvement and widening of Steeles Avenue (Regional Road No. 8) from Eighth Line North to Winston Churchill Boulevard (Regional Road No. 19) (“Steeles Avenue Reconstruction”) in the Town of Halton Hills for entering on the lands with all necessary vehicles, machinery, workmen and other material for municipal purposes including but not limited to the construction, installation, maintenance and repair of associated public utilities including aerial and/or subsurface hydro lines, tiebacks, poles, anchors, guywires and associated materials and equipment and any works ancillary thereto including utility relocation and site grading. A copy of the Plan associated with the property for which you have an interest is enclosed for your convenience.

Although expropriation proceedings have started, the Region of Halton is prepared to discuss a voluntary purchase and entitlement to compensation with you, and wishes to reach an agreement with you, if possible.

If you have any questions with respect to this matter, please contact Mr. Don Williams, Manager of Realty Services, at the Region of Halton at their office at 1151 Bronte Road in Oakville or at (905) 825 – 6000, ext. 7238, or you may contact our clerk, Rey Velasco at (416) 367-6187.

THE REGIONAL MUNICIPALITY OF HALTON
PROPERTY PURCHASE AGREEMENT – PERMANENT EASEMENT

BETWEEN:

JOHN TEGGART (the "Vendor")

- and -

THE REGIONAL MUNICIPALITY OF HALTON (the "Purchaser")

WHEREAS the Vendor is the owner of lands in the Town of Halton Hills, in the Regional Municipality of Halton;

AND WHEREAS the Vendor hereby offers to sell to the Purchaser a permanent easement over the lands described as Part 9 on Reference Plan 20R-19400 (the "Easement Lands") and the Purchaser proposes to purchase a permanent easement over the Easement Lands;

NOW THEREFORE in consideration of the mutual covenants and agreements herein contained, the parties hereto agree as follows:

1. (a) The Vendor hereby agrees to sell and convey to the Purchaser the Easement Lands for the total sum of One Thousand Dollars (\$1,000.00) (the "Purchase Price"). The Purchase Price shall be paid on closing, subject to adjustments, if any, provided the title is good and free from all encumbrances, except for any registered restrictions or covenants that run with the Easement Lands, providing that such are complied with.

(b) If this transaction is subject to the Harmonized Sales Tax then such tax will be in addition to the Purchase Price and added to the Purchaser's account.

(c) This transaction will be completed by electronic registration pursuant to Part III of the *Land Registration Reform Act*, R.S.O. 1990, c. L.4, as amended.
2. The Vendor warrants that he is not a non-resident within the meaning of Section 116 of the *Income Tax Act*, R.S.C. 1970, c.L. 5 (Canada), as amended. In the event that the Vendor is a non-resident, all necessary applications shall be made to the Canada Revenue Agency required by a non-resident of Canada concerning disposition or proposed disposition of Canadian property and pay any and all applicable taxes to the Receiver General of Canada, or provide security acceptable to the Canada Revenue Agency with respect to the proposed disposition of the Easement Lands.
3. This Agreement is subject to the express condition that if the provisions of Section 50 of the *Planning Act*, R.S.O. 1990, c. P.13, as amended, apply to this transaction, the said agreement shall be effective to create an interest in lands only if such provisions are complied with.
4. The Purchaser accepts the Easement Lands subject to the restrictions and covenants that may run with the Easement Lands, providing that same have been complied with and do not restrict



Public Works
Office of the Commissioner
1151 Bronte Road
Oakville, ON L6M 3L1

July 24, 2025

Lisa Teggart
16863 Steeles Avenue
Hornby, ON L0P 1E0
haltonhomestead@hotmail.com

Hello Ms. Teggart:

Re: Halton Hills #4 Wastewater Pumping Station Municipal Class Environmental Assessment Study

Thank you for meeting with the Region on May 15, 2025, and your follow-up information provided on June 6 and June 10, 2025. We appreciate your feedback on the Halton Hills #4 Wastewater Pumping Station (WWPS) Municipal Class Environmental Assessment (MCEA) Study and sharing the history of your business over the last 40 years in Whaley's Corners. We appreciate that you are generally in support of the overall development plan and have prepared the following responses to your specific concerns and requests below.

Municipal Class Environmental Assessment (MCEA) Process

As it relates to the MCEA process, the Region considered several alternatives to service the proposed development. For site location, the province's plans related to Highway 413 restricted us to looking at options on the north side of Steeles Avenue only. In addition, the Region also assessed the option of installing a gravity sewer to support the proposed development; however, it was determined that it would not meet the required in-service timelines and was therefore not preferred. Nonetheless, we are committed to minimizing the impact of this infrastructure on the community, and further in this letter we have outlined additional efforts the Region can take to achieve this.

New Regional and Local Road Access

The concerns raised were safety, separation, and other technical issues of establishing a new entrance from Steeles Avenue to the Halton Hills #4 WWPS. This site will be an unmanned facility and is projected to generate a negligible amount of traffic as Region staff are expected to attend the site intermittently, for operational and maintenance purposes. Additionally, a bulk water station is not planned for this site.

To address the question about access being provided from a local road right-of-way, the approved Premier Gateway Phase 2B Secondary Plan does not consider a local road being provided in the vicinity of the property. However, as development proceeds, we will continue to coordinate with the Town for any opportunities to provide access from a local road right-of-way.

In terms of building citing, the Region is prepared to locate the facility on the west side of the property, subject to final design decisions and can include landscaping screens between the facility and your property. For odour management, an emission dispersion model will be completed during detailed design to determine odour control requirements.

The Regional Municipality of Halton

Water Service Disruption

With respect to concerns raised of a potential contamination pathway between the sewage works and your existing dug well, as per the Ontario Water Resources Act and Ontario Regulation 903: Wells, the Region is required to construct the WWPS in a manner that does not impact your water supply.

To achieve this the Region will undertake pre and post-construction surveys on your well, as well as all private wells in the vicinity of the project to monitor for any impact. Should it be determined that construction has impacted water supplies, then replacement supplies would be provided for your current use. This could include replacement wells or connection to a municipal supply, like the existing watermain servicing the Conestoga development that is currently supplied from Peel Region under the Halton-Peel Cross-Boundary Water Servicing Agreement.

We would like to thank you again for meeting with staff and sharing your feedback on the ongoing Halton Hills #4 WWPS MCEA Study.

Please feel free to contact me at 905-825-6000, extension 7547 if you have any further questions or comments.

Sincerely,

A handwritten signature in blue ink, appearing to read 'Lee Anne Jones', with a stylized flourish at the end.

Lee Anne Jones, P. Eng.
Commissioner, Public Works

From: Minkin, Dan (MCM) <Dan.Minkin@ontario.ca>
Sent: Wednesday, October 16, 2024 5:33 PM
To: Freya Wu
Cc: Alonzo Moya, Carlos; Jonathan Rudyk
Subject: RE: R236814-20240724-Halton Hills #4 WWPS - Premier Gateway Employment Area Class EA Notice of Commencement [MCM File 0013436]

[CAUTION EXTERNAL EMAIL] Make Sure that it is legitimate **before** Replying or Clicking on any links

Hi Freya, my apologies for letting this one drop in September.

I'm not sure I understand the question with respect to the archaeological assessment. The Stage 1 assessment recommended in Karla Barboza's email of February 28 is in progress, right? Are you asking about carrying out invasive geotechnical and hydrogeological investigations (boreholes etc) as part of the EA study without waiting for the results of the Stage 1? If so I don't see a problem with that, if the locations in question were found in the Stage 2 to be clear of archaeological concern; the ongoing Stage 1 work is really for purposes of EA documentation.

With respect to the built heritage checklist, from a quick look at the document you linked it screens for both known and potential built heritage resources and cultural heritage landscapes. So if it found the present study area not to contain any, you could answer "yes" to question 2 of the checklist ("Has the property (or project area) been evaluated before and found **not** to be of cultural heritage value?") and not complete the rest.

I hope that helps.

Dan Minkin

Heritage Planner | Heritage Operations Branch
Ministry of Citizenship and Multiculturalism | Ontario Public Service
416-786-7553 | dan.minkin@ontario.ca



Taking pride in strengthening Ontario, its places and its people

From: Freya Wu <mwu@rvanderson.com>
Sent: Tuesday, October 15, 2024 9:48 AM
To: Minkin, Dan (MCM) <Dan.Minkin@ontario.ca>
Cc: Alonzo Moya, Carlos <Carlos.AlonzoMoya@halton.ca>; Jonathan Rudyk <JRudyk@rvanderson.com>
Subject: RE: R236814-20240724-Halton Hills #4 WWPS - Premier Gateway Employment Area Class EA Notice of Commencement [MCM File 0013436]

CAUTION -- EXTERNAL E-MAIL - Do not click links or open attachments unless you recognize the sender.

Good morning Dan,

Hope this email finds you well.

Just following up regarding the email sent below. We have reviewed your letter and wish to clarify a few things.

Thank you!

Freya (Menghong) Wu, M.A.Sc. (she/her)

Process Designer



R.V. Anderson Associates Limited
2001 Sheppard Avenue East, Suite 300, Toronto ON M2J 4Z8
t 416 497 8600 ext. 1377

[LinkedIn](#) | [Facebook](#) | [Website](#)



From: Freya Wu

Sent: September 11, 2024 2:55 PM

To: Minkin, Dan (MCM) <Dan.Minkin@ontario.ca>

Cc: Alonzo Moya, Carlos <Carlos.AlonzoMoya@halton.ca>; Jonathan Rudyk <JRudyk@rvanderson.com>

Subject: RE: R236814-20240724-Halton Hills #4 WWPS - Premier Gateway Employment Area Class EA Notice of Commencement [MCM File 0013436]

Good afternoon Dan,

Thank you very much for the response. We have reviewed your letter and wish to clarify a few things. The first is regarding the prior AAs done on the site, and the second is regarding the Cultural Heritage Checklist mentioned on page 3.

For quick reference, I have attached previous email chain between us and MCM and summarized below the project background/discussion up to date:

- Halton Region is initiating a Schedule B Class EA study for a Wastewater Pumping Station (WWPS) and the study area consists of two sites, Alternative 2 (East side) and Alternative 3 (West side).
- A prior Stage 1&2 archaeological assessment (PIF P052-1083-2021) for Part of East Half of Lot 1, Concession 2, Town of Halton Hills was undertaken to support a development application under the *Planning Act*. The report has been entered into the Ontario Public Register of Archaeological Reports and cleared Alternative 2 (East side) and Alternative 3 (West side) from additional archaeological assessments (AA). However, when preparing the report indigenous communities were not contacted and Halton Region was not the proponent of the AA. Other complexities were summarized in a previous email sent by my colleague Dania Chehab to Karla Barboza on January 31st, 2024.
- We communicated with MCM and MECP regarding the complexities and it was recommended that a Stage 1 archaeological assessment be completed for Halton Region's WWPS (MCM's recommendation was emailed to us by Karla Barboza on February 28th, 2024). The PIF for the current Stage 1 AA is P051-0292-2023 and P051-0322-2024. Given that the study area (Alternative 2 and Alternative 3) is clear from additional archaeological assessments, we are planning to proceed with the geotechnical and hydrogeological scopes of work in the study area.

The second piece is regarding the cultural heritage checklist. A larger area-wide cultural heritage report was completed as part of the larger Secondary Plan (Link here: <https://www.haltonhills.ca/en/residents/premier->

gateway-phase-2b.aspx#Background-information) and does not note any cultural heritage resources or landscapes near the study area, so is the Cultural Heritage Checklist still required?

Regards,

Freya (Menghong) Wu, M.A.Sc. (she/her)

Process Designer



R.V. Anderson Associates Limited
2001 Sheppard Avenue East, Suite 300, Toronto ON M2J 4Z8
t 416 497 8600 ext. 1377

[LinkedIn](#) | [Facebook](#) | [Website](#)



From: Minkin, Dan (MCM) <Dan.Minkin@ontario.ca>

Sent: August 26, 2024 5:37 PM

To: Alonzo Moya, Carlos <Carlos.AlonzoMoya@halton.ca>; Dania Chehab <dchehab@rvanderson.com>

Cc: Freya Wu <mwu@rvanderson.com>; Carol Derrick <cderrick@rvanderson.com>

Subject: RE: R236814-20240724-Halton Hills #4 WWPS - Premier Gateway Employment Area Class EA Notice of Commencement [MCM File 0013436]

[CAUTION EXTERNAL EMAIL] Make Sure that it is legitimate **before** Replying or Clicking on any links

Good afternoon,
Please see attached.

Dan Minkin

Heritage Planner | Heritage Operations Branch
Ministry of Citizenship and Multiculturalism | Ontario Public Service
416-786-7553 | dan.minkin@ontario.ca



Taking pride in strengthening Ontario, its places and its people

From: Carol Derrick <cderrick@rvanderson.com>

Sent: Wednesday, July 24, 2024 10:18 AM

Cc: Freya Wu <mwu@rvanderson.com>

Subject: R236814-20240724-Halton Hills #4 WWPS - Premier Gateway Employment Area Class EA Notice of Commencement

CAUTION -- EXTERNAL E-MAIL - Do not click links or open attachments unless you recognize the sender.

Dear Sir/Madam:

Please see attached the Notice of Commencement for the Halton Hills #4 Wastewater Pumping Station – Premier Gateway Employment Area Municipal Class Environmental Assessment (Class EA).

The Regional Municipality of Halton is undertaking this Class EA to identify a preferred site for the Halton Hills #4 Wastewater Pumping Station based on current industry standards and due to growing development flows in the Premier Gateway Employment Area.

This notice is sent to your attention as it was deemed that you may be an interested stakeholder.

If you would like more information about the Halton Hills #4 Wastewater Pumping Station – Premier Gateway Employment Area or would like to be removed from the study's contact list, please contact the undersigned.

Yours very truly,

Dania Chehab
Consultant Project Manager
R.V. Anderson Associates Limited
2001 Sheppard Avenue East, Suite 300
Toronto, ON M2J 4Z8

Phone: 416-497-8600 ext. 1456

Email: dchehab@rvanderson.com

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[REDACTED]

From: Jonathan Rudyk
Sent: Wednesday, October 23, 2024 9:45 AM
To: Freya Wu
Subject: FW: R236814-20241023-Notice of PIC Halton Hills #4 Wastewater Pumping Station – Premier Gateway Employment Area Municipal Class Environmental Assessment (MCEA)

Jonathan Rudyk, P. Eng (he/him)

Associate, Project Engineer, Water



R.V. Anderson Associates Limited
2001 Sheppard Avenue East, Suite 300, Toronto ON M2J 4Z8
t 416 497 8600 ext. 1497 | m 416 802 5048

[LinkedIn](#) | [Facebook](#) | [Website](#)



[REDACTED]

Sent: October 23, 2024 9:38 AM
To: Jonathan Rudyk <JRudyk@rvanderson.com>
Subject: RE: R236814-20241023-Notice of PIC Halton Hills #4 Wastewater Pumping Station – Premier Gateway Employment Area Municipal Class Environmental Assessment (MCEA)

[CAUTION EXTERNAL EMAIL] Make Sure that it is legitimate **before** Replying or Clicking on any links

Hi Jonathan,

After the minimum 30 days of the public and agency review period, and if approved, what is the anticipated timeline to complete the wastewater pumping station? (in respect to development timeline for the lands between Ninth Line and Winston Churchill, north of Steeles.)

Kind regards,

[REDACTED]



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From: Carol Derrick <cderrick@rvanderson.com>

Sent: Wednesday, October 23, 2024 8:52 AM

Cc: Freya Wu <mwu@rvanderson.com>

Subject: R236814-20241023-Notice of PIC Halton Hills #4 Wastewater Pumping Station – Premier Gateway Employment Area Municipal Class Environmental Assessment (MCEA)

You don't often get email from cderrick@rvanderson.com. [Learn why this is important](#)

Caution: Message from external sender

Dear Sir/Madam:

Please see enclosed the Notice of Public Information Centre for the Halton Hills #4 Wastewater Pumping Station – Premier Gateway Employment Area Municipal Class Environmental Assessment (MCEA).

The Regional Municipality of Halton is undertaking this Class EA to identify a preferred site for the Halton Hills #4 Wastewater Pumping Station based on current industry standards and due to growing development flows in the Premier Gateway Employment Area.

This notice is sent to your attention as it was deemed that you may be an interested stakeholder.

If you would like more information about the Halton Hills #4 Wastewater Pumping Station – Premier Gateway Employment Area or would like to be removed from the study's contact list, please contact the undersigned.

Yours very truly,

Jonathan Rudyk

Consultant Project Manager

R.V. Anderson Associates Limited

2001 Sheppard Avenue East, Suite 300

Toronto, ON M2J 4Z8

Phone: 416-497-8600 ext. 1497

Email: jrudyk@rvanderson.com

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From: Carol Derrick
Sent: Wednesday, October 23, 2024 12:11 PM
To: Freya Wu
Subject: FW: R236814-20241023-Notice of PIC Halton Hills #4 Wastewater Pumping Station – Premier Gateway Employment Area Municipal Class Environmental Assessment (MCEA)

FYI.

From: Laureen Choi [Staff] <choil@hdsb.ca>
Sent: October 23, 2024 11:46 AM
To: Carol Derrick <cderrick@rvanderson.com>
Cc: Frederick Thibeault [Staff] <thibeaultf@hdsb.ca>; carlos.alonzomoya@halton.ca; Jonathan Rudyk <JRudyk@rvanderson.com>
Subject: Re: R236814-20241023-Notice of PIC Halton Hills #4 Wastewater Pumping Station – Premier Gateway Employment Area Municipal Class Environmental Assessment (MCEA)

[CAUTION EXTERNAL EMAIL] Make Sure that it is legitimate **before** Replying or Clicking on any links

Hi Carol. Thank you for the circulation.

This email is to acknowledge receipt of your email. Note that the closest Halton District School Board existing facility is Pineview Public School located at 13074 5 Side Rd (SE corner of Trafalgar Road and 5 Side Road).

Please continue to circulate us on future updates of this project. We will review and provide comments on any future submissions.

Laureen Choi | Specialist - Planning
Halton District School Board
J.W. Singleton Education Centre
2050 Guelph Line, Burlington, ON L7R 3Z2
choil@hdsb.ca | ext. 2201
905-749-2184

On Wed, Oct 23, 2024 at 8:52 AM Carol Derrick <cderrick@rvanderson.com> wrote:

Dear Sir/Madam:

Please see enclosed the Notice of Public Information Centre for the Halton Hills #4 Wastewater Pumping Station – Premier Gateway Employment Area Municipal Class Environmental Assessment (MCEA).

The Regional Municipality of Halton is undertaking this Class EA to identify a preferred site for the Halton Hills #4 Wastewater Pumping Station based on current industry standards and due to growing development flows in the Premier Gateway Employment Area.

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If you would like more information about the Halton Hills #4 Wastewater Pumping Station – Premier Gateway Employment Area or would like to be removed from the study's contact list, please contact the undersigned.

Yours very truly,

Jonathan Rudyk

Consultant Project Manager

R.V. Anderson Associates Limited

2001 Sheppard Avenue East, Suite 300

Toronto, ON M2J 4Z8

Phone: 416-497-8600 ext. 1497

Email: jrudyk@rvanderson.com

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From: Jonathan Rudyk
Sent: Wednesday, October 23, 2024 10:18 AM
To: Freya Wu
Subject: FW: R236814-20241023-Notice of PIC Halton Hills #4 Wastewater Pumping Station – Premier Gateway Employment Area Municipal Class Environmental Assessment (MCEA)

Jonathan Rudyk, P. Eng (he/him)

Associate, Project Engineer, Water



R.V. Anderson Associates Limited
2001 Sheppard Avenue East, Suite 300, Toronto ON M2J 4Z8
t 416 497 8600 ext. 1497 | m 416 802 5048

[LinkedIn](#) | [Facebook](#) | [Website](#)



From: Jason Blackmere <jblackmere@haltonhills.ca>
Sent: October 23, 2024 9:59 AM
To: Jonathan Rudyk <JRudyk@rvanderson.com>
Subject: FW: R236814-20241023-Notice of PIC Halton Hills #4 Wastewater Pumping Station – Premier Gateway Employment Area Municipal Class Environmental Assessment (MCEA)

[CAUTION EXTERNAL EMAIL] Make Sure that it is legitimate **before** Replying or Clicking on any links

Good morning [REDACTED],

Our Fire Chief would like more information regarding the study and if you require any fire department input.

Thank you,

Jason Blackmere
Fire Prevention Officer, CFEI
Halton Hills Fire Department
14007 10 Side Road
Halton Hills, ON L7G 4S5
Cell: 416-523-8923
Desk: 905 873-2601 X2139
jblackmere@haltonhills.ca

From: Carol Derrick <cderrick@rvanderson.com>

Sent: Wednesday, October 23, 2024 8:52 AM

Cc: Freya Wu <mwu@rvanderson.com>

Subject: R236814-20241023-Notice of PIC Halton Hills #4 Wastewater Pumping Station – Premier Gateway Employment Area Municipal Class Environmental Assessment (MCEA)

[EXTERNAL EMAIL]

Dear Sir/Madam:

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Yours very truly,

Jonathan Rudyk

Consultant Project Manager

R.V. Anderson Associates Limited

2001 Sheppard Avenue East, Suite 300

Toronto, ON M2J 4Z8

Phone: 416-497-8600 ext. 1497

Email: jrudyk@rvanderson.com

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Jonathan Rudyk

From: Jonathan Rudyk
Sent: February 11, 2025 11:33 AM
To: Marc Denardis
Cc: Pasquale, Christopher; Freya Wu
Subject: RE: Halton Hills #4 Wastewater Pumping Station MCEA

Hi Marc,

Thank you for your question.

My name is Jonathan Rudyk, I am the Class EA Lead for R.V. Anderson, the consultant assisting the Region with this Class EA. The Notice of Completion has not yet been issued and will be sometime this spring. You are on our stakeholder list so you will receive this correspondence.

Additionally, please note that Christopher Pasquale (cc'ed on this email) is the new Halton Project Manager, so feel free to reach out to either of us with any further comments or questions.

Thanks,

Jonathan Rudyk, P. Eng (he/him)

Associate, Project Engineer, Water



R.V. Anderson Associates Limited
2001 Sheppard Avenue East, Suite 300, Toronto ON M2J 4Z8
t 416 497 8600 ext. 1497 | m 416 802 5048

[LinkedIn](#) | [Facebook](#) | [Website](#)



From: Marc Denardis <mdenardis@gwdplanners.com>
Sent: February 3, 2025 2:35 PM
To: Alonzo Moya, Carlos <Carlos.AlonzoMoya@halton.ca>
Cc: Jonathan Rudyk <JRudyk@rvanderson.com>; Rees, Alexandria <Alexandria.Rees@halton.ca>
Subject: RE: Halton Hills #4 Wastewater Pumping Station MCEA

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Hello Alonzo,

I am taking a moment to check in on the HH#4 Wastewater Public Station EA.

Has the Notice of Completion has been issued? I recall reading that the Report was targeted for completion in early 2025.

Thank you in advance. Take care.

Marc De Nardis
GWD Ltd.
(905) 796-5790 x257

From: Alonzo Moya, Carlos <Carlos.AlonzoMoya@halton.ca>
Sent: November 13, 2024 11:08 AM
To: Marc Denardis <mdenardis@gwdplanners.com>
Cc: jrudyk@rvanderson.com; Rees, Alexandria <Alexandria.Rees@halton.ca>
Subject: RE: Halton Hills #4 Wastewater Pumping Station MCEA

Good morning Marc,

Thank you very much for your email.

We will add you to the project distribution list. You will now be informed of project updates. Additionally, please feel free to review the [Project MCEA Webpage](#) and the [Project Consultation Webpage](#). The Public Information Centre is currently ongoing virtually, and you can submit questions about the materials until November 23, 2024.

Please do not hesitate to contact the project team if you have any questions.

Best regards,
Carlos Alonzo Moya

Carlos Alonzo Moya (he/him/his), M.A.Sc., P.Eng.
Project Manager II
Infrastructure Planning & Policy
Public Works
Halton Region
905-825-6000, ext. 7426 | 1-866-442-5866



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From: Marc DeNardis <mdenardis@gwdplanners.com>
Sent: Thursday, November 7, 2024 11:31 AM
To: Alonzo Moya, Carlos <Carlos.AlonzoMoya@halton.ca>
Cc: jrudyk@rvanderson.com
Subject: Halton Hills #4 Wastewater Pumping Station MCEA

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Good morning Carlos,

I writing to you with respect to the HH #4 Waste Water Pumping Station MCEA.

Our office is Planning Consultant to Maple Lodge Farms Ltd. (MLF) and its affiliated companies, who own land located on the east and west sides of Winston Churchill Boulevard, north of Steeles Avenue. Of their landholdings within the Halton Hills, 20 hectares (50 acres) northeast of Steeles Avenue and Tenth Line are located within the limits of the Premier Gateway Phase 2B (PGP2B) Secondary Plan Study Area (see attached).

Given MLF's interest, may I ask that my email address be added to the notification/distribution list. Should you require additional information do not hesitate to contact me at the number below.

Thank you for your consideration. Have a great day.

Marc De Nardis, B.U.R.Pl., M.C.I.P., R.P.P.
Planning Associate



****NEW GWD OFFICE ALERT****: 7685 Hurontario Street, Suite 501, Brampton, ON, L6W 0B4

E: mdenardis@gwdplanners.com W: www.gwdplanners.com P: 905-796-5790 ext. 257 TF: 1-855-771-7266

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Jonathan Rudyk

From: Pasquale, Christopher <Christopher.Pasquale@halton.ca>
Sent: April 24, 2025 10:00 AM
To: [REDACTED]
Cc: Jonathan Rudyk; Passera Lavoie, Alexander
Subject: Halton Hills #4 Wastewater Pumping Station Schedule B Municipal Class Environmental Assessment

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Hi [REDACTED]

My name is Christopher Pasquale, and I'm the project manager for the Halton Hills #4 Wastewater Pumping Station Schedule B Municipal Class Environmental Assessment Study. I'm reaching out because you shared a comment and expressed interest in being contacted by the project team.

Thank you for your comment regarding the study area and location of the wastewater pumping station to support the Premier Gateway Employment Area lands. The study area is situated near the lowest points of the entire Secondary Plan Area, which naturally facilitates the gravity-fed drainage system required to support development of the Premier Gateway Employment Area lands and the wastewater pumping station. A gravity-based drainage system is the most efficient option for conveying wastewater to a wastewater pumping station and topography is a key consideration.

If you have any further questions or comments, please don't hesitate to reach out.

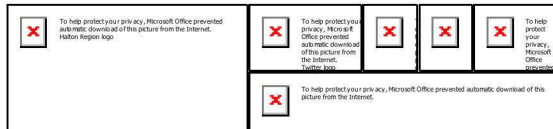
As part of the Municipal Class Environmental Assessment Study process, a Project File Report is being prepared to document the decision-making process and commitments for detailed design. A notice of Study Completion will be issued when the Project File Report is available for public review on the [project webpage](#) at halton.ca. You have been added to our project mailing list.

Thank you very much for your participation.

Best regards,

Christopher Pasquale

Christopher Pasquale, P.Eng., PMP
Project Manager II
Infrastructure and Environmental Services
Public Works
Halton Region
289-834-4731 x7134 | 1-866-442-5866



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Jonathan Rudyk

From: Jonathan Rudyk
Sent: May 12, 2025 4:42 PM
To: Freya Wu
Subject: FW: Halton Hills #4 Wastewater Pumping Station Schedule B Municipal Class Environmental Assessment

Jonathan Rudyk, P.Eng.

Associate, Project Engineer Water



R.V. Anderson Associates Limited
2001 Sheppard Avenue East, Suite 300
Toronto ON M2J 4Z8
t 416 497 8600 x1497
c 416 802 5048

[LinkedIn](#) | [Facebook](#) | [Website](#)



From: Pasquale, Christopher <Christopher.Pasquale@halton.ca>

Sent: May 12, 2025 3:41 PM

To: [REDACTED]

Cc: Jonathan Rudyk <JRudyk@rvanderson.com>; Passera Lavoie, Alexander <Alexander.PasseraLavoie@halton.ca>

Subject: Halton Hills #4 Wastewater Pumping Station Schedule B Municipal Class Environmental Assessment

[CAUTION EXTERNAL EMAIL] Make Sure that it is legitimate **before** Replying or Clicking on any links

Hi [REDACTED]

My name is Christopher Pasquale, and I'm the project manager for the Halton Hills #4 Wastewater Pumping Station Schedule B Municipal Class Environmental Assessment Study. I'm reaching out because you provided comments and expressed interest in being contacted by the project team.

Thank you for your comments regarding operation of the new wastewater pumping station. The station will be powered by Halton Hills Hydro, the local hydroelectric utility. Building heating will be supplied through natural gas, which is available along Steeles Avenue. The station will also have a standby generator to provide backup power in the event of utility outages, as required by provincial regulations for wastewater pumping stations of this size.

Regarding climate change mitigation, the project includes several initiatives aligned with the Region's goals of reducing greenhouse gas emissions and enhancing energy efficiency. These include:

- **Remote Monitoring Systems:** Implementing advanced remote monitoring technologies to minimize the need for on-site staff presence, thereby reducing transportation-related emissions.
- **Efficient Building Design and Insulation:** Adopting energy-efficient building designs and high-quality insulation materials to minimize heat loss and reduce energy consumption.

- **Energy-Efficient Equipment:** Utilizing energy-efficient motors for pumps and other equipment to decrease electricity usage.

The Region's broader strategy to address climate change and transition to a resilient net-zero future can be found at [Halton - Climate Change and the Environment](#) on halton.ca.

You also inquired about the Monarch butterfly habitat. This habitat will be impacted as part of construction, therefore landscaping measures will be developed during detailed design and implemented after construction which will include re-planting appropriate host plants, nectar-producing flowers and other native species.

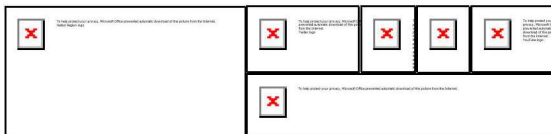
As part of the Municipal Class Environmental Assessment Study process, a Project File Report is being prepared to document the decision-making process and commitments for detailed design. A notice of Study Completion will be issued when the Project File Report is available for public review on the [project webpage](#) at halton.ca. You have been added to our project mailing list.

Thank you very much for your participation.

Best regards,

Christopher Pasquale

Christopher Pasquale, P.Eng., PMP
Project Manager II
 Infrastructure and Environmental Services
 Public Works
Halton Region
 289-834-4731 x7134 | 1-866-442-5866



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Jonathan Rudyk

From: Pasquale, Christopher <Christopher.Pasquale@halton.ca>
Sent: April 24, 2025 10:02 AM
To: [REDACTED]
Cc: Jonathan Rudyk; Passera Lavoie, Alexander
Subject: Halton Hills #4 Wastewater Pumping Station Schedule B Municipal Class Environmental Assessment

[CAUTION EXTERNAL EMAIL] Make Sure that it is legitimate **before** Replying or Clicking on any links

Hi [REDACTED]

My name is Christopher Pasquale, and I'm the project manager for the Halton Hills #4 Wastewater Pumping Station Schedule B Municipal Class Environmental Assessment Study. I'm reaching out because you provided comments and expressed interest in being contacted by the project team.

Thank you for your comments regarding operation of the new wastewater pumping station. The new wastewater pumping station will direct wastewater through a series of sewers to the Mid-Halton Wastewater Treatment Plant, located in the Town of Oakville. At the plant, wastewater is treated to remove organic and inorganic materials, disinfected, and then released into Lake Ontario. The treated water is carefully monitored to ensure it meets the plant's discharge regulatory limits set by the Province of Ontario.

You also inquired whether the Region has contacted nearby property owners to inform them about the project. As part of the Municipal Class Environmental Assessment Study process, property owners within the vicinity of the study area were initially engaged through a mailed Notice of Commencement and would have received a Notice of Public Information Centre as well. They will also receive a Notice of Completion when the project concludes.

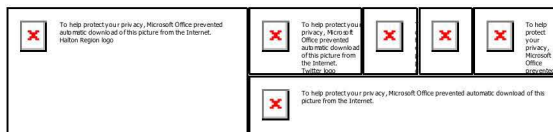
As part of the Municipal Class Environmental Assessment Study process, a Project File Report is being prepared to document the decision-making process and commitments for detailed design. A Notice of Completion will be issued when the Project File Report is available for public review on the [project webpage](#) at halton.ca. You have been added to our project mailing list.

Thank you very much for your participation.

Best regards,

Christopher Pasquale

Christopher Pasquale, P.Eng., PMP
Project Manager II
Infrastructure and Environmental Services
Public Works
Halton Region
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Jonathan Rudyk

From: Pasquale, Christopher <Christopher.Pasquale@halton.ca>
Sent: April 24, 2025 10:03 AM
To: [REDACTED]
Cc: Jonathan Rudyk; Passera Lavoie, Alexander
Subject: Halton Hills #4 Wastewater Pumping Station Schedule B Municipal Class Environmental Assessment

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Hi [REDACTED]

My name is Christopher Pasquale, and I'm the project manager for the Halton Hills #4 Wastewater Pumping Station Schedule B Municipal Class Environmental Assessment Study. I'm reaching out because you shared a comment and expressed interest in being contacted by the project team.

You inquired whether the Region has contacted nearby property owners to inform them about the project. As part of the Municipal Class Environmental Assessment Study process, property owners within the vicinity of the study area were initially engaged through a mailed Notice of Commencement and would have received a Notice of Public Information Centre as well. They will also receive a Notice of Completion when the project concludes.

As part of the Municipal Class Environmental Assessment Study process, a Project File Report is being prepared to document the decision-making process and commitments for detailed design. A Notice of Completion will be issued when the Project File Report is available for public review on the [project webpage](#) at halton.ca. You have been added to our project mailing list.

Thank you very much for your participation.

Best regards,

Christopher Pasquale

Christopher Pasquale, P.Eng., PMP

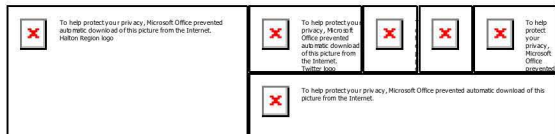
Project Manager II

Infrastructure and Environmental Services

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Jonathan Rudyk

From: Pasquale, Christopher <Christopher.Pasquale@halton.ca>
Sent: April 24, 2025 10:04 AM
To: [REDACTED]
Cc: Jonathan Rudyk; Passera Lavoie, Alexander
Subject: Halton Hills #4 Wastewater Pumping Station Schedule B Municipal Class Environmental Assessment

[CAUTION EXTERNAL EMAIL] Make Sure that it is legitimate **before** Replying or Clicking on any links

Hi [REDACTED]

My name is Christopher Pasquale, and I'm the project manager for the Halton Hills #4 Wastewater Pumping Station Schedule B Municipal Class Environmental Assessment Study. I'm reaching out because you shared a comment and expressed interest in being contacted by the project team.

To follow-up on your question regarding the location of the Public Information Centre video, it is posted on the project's public consultation webpage: <https://www.halton.ca/For-Residents/Opportunities-to-Participate/Online-Consultation-Premier-Gateway-Municipal-Cla>.

As part of the Municipal Class Environmental Assessment Study process, a Project File Report is being prepared to document the decision-making process and commitments for detailed design. A Notice of Completion will be issued when the Project File Report is available for public review on the [project webpage](#) at halton.ca. You have been added to our project mailing list.

Thank you very much for your participation.

Best regards,

Christopher Pasquale

Christopher Pasquale, P.Eng., PMP

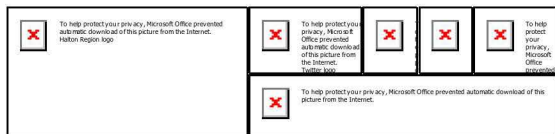
Project Manager II

Infrastructure and Environmental Services

Public Works

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Jonathan Rudyk

From: Pasquale, Christopher <Christopher.Pasquale@halton.ca>
Sent: April 29, 2025 1:32 PM
To: [REDACTED]
Cc: Jonathan Rudyk; Passera Lavoie, Alexander
Subject: Halton Hills #4 Wastewater Pumping Station Schedule B Municipal Class Environmental Assessment

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Hi [REDACTED]

My name is Christopher Pasquale, and I'm the project manager for the Halton Hills #4 Wastewater Pumping Station Schedule B Municipal Class Environmental Assessment Study. I'm reaching out because you shared a comment and expressed interest in being contacted by the project team.

Thank you for your inquiry regarding whether homes on Tenth Line will be connected to the new Halton Hills #4 Wastewater Pumping Station. Currently, design work is underway to construct a new sanitary sewer along Steeles Avenue from Winston Churchill Boulevard to Ninth Line South. This new sewer will support the future development of the Premier Gateway Employment Area. The Town of Halton Hills Phase 2B Secondary Plan outlines how this area is meant to develop in the future. One of the studies associated with that Secondary Plan is the Water and Wastewater Area Servicing Plan for the Phase 2B Premier Gateway Employment Area. That study proposes that a local sanitary sewer will be required in the future on Tenth Line to support the anticipated employment uses in that area. While the Region of Halton constructs the trunk/transmission mains, it is the responsibility of developers to construct local mains. The local main that is identified for Tenth Line will need to be constructed in the future by developers and is not part of the ongoing servicing work for Steeles Avenue. Once the local main on Tenth Line is constructed and operational, residents that front on to that main will have the opportunity to connect to that service at their own expense.

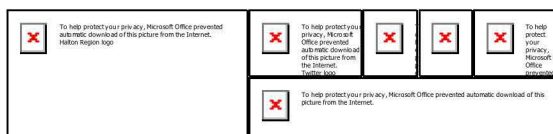
As part of the Municipal Class Environmental Assessment Study process, a Project File Report is being prepared to document the decision-making process and commitments for detailed design for the new Halton Hills #4 Wastewater Pumping Station. A Notice of Completion will be issued when the Project File Report is available for public review on the [project webpage](#) at halton.ca. You have been added to our project mailing list.

Thank you very much for your participation.

Best regards,

Christopher Pasquale

Christopher Pasquale, P.Eng., PMP
Project Manager II
Infrastructure and Environmental Services
Public Works
Halton Region
289-834-4731 x7134 | 1-866-442-5866



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Jonathan Rudyk

From: Bell, Trevor (MECP) <Trevor.Bell@ontario.ca>
Sent: June 13, 2025 3:14 PM
To: Pasquale, Christopher
Cc: Jonathan Rudyk; Freya Wu; Mazzuca, Marco (MECP); Hannington, Neil (MECP)
Subject: Draft Project File Report - Halton Hills #4 WWPS - Schedule B Municipal Class EA
Attachments: MECP Comments_Draft PFR_Halton Hills #4 WWPS.pdf

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Good afternoon,

Please find attached a letter from the Ministry of the Environment, Conservation and Parks, Environmental Assessment Branch, regarding the above-mentioned project. Please feel free to contact me directly with any questions you may have.

Sincerely,

Trevor Bell | Regional Environmental Planner
Project Review Unit, Environmental Assessment Branch
Ministry of the Environment, Conservation and Parks
New Phone: 437-213-1014 | trevor.bell@ontario.ca

**Ministry of the Environment,
Conservation and Parks**

**Ministère de l'Environnement,
de la Protection de la nature
et des Parcs**

Environmental Assessment
Branch

Direction des évaluations
environnementales

1st Floor
135 St. Clair Avenue W
Toronto ON M4V 1P5
Tel.: 416 314-8001
Fax.: 416 314-8452

Rez-de-chaussée
135, avenue St. Clair Ouest
Toronto ON M4V 1P5
Tél. : 416 314-8001
Télec. : 416 314-8452

June 13, 2025

Christopher Pasquale, P.Eng., PMP
Infrastructure & Environmental Services
Halton Region
christopher.pasquale@halton.ca

BY EMAIL ONLY

**Re: Halton Hills #4 Wastewater Pumping Station
Regional Municipality of Halton
Schedule B Municipal Class EA
Draft Project File Report**

Dear Mr. Pasquale,

On behalf of the Ministry of the Environment, Conservation and Parks (ministry), I have reviewed the draft Project File Report (PFR) prepared by R.V. Anderson Associates Ltd, dated May 12, 2025, for the Halton Hills #4 Wastewater Pumping Station (WWPS) Schedule B Municipal Class EA.

The need for a new WWPS was identified in Halton Region's 2008 and 2011 Water and Wastewater Master Plans to service Phase 2B of the Premier Gateway Employment Area for future commercial and industrial growth.

This Class EA was initiated to identify the preferred location of the new WWPS. The PFR also identifies permits and approvals, and additional technical studies that will be required for the detailed design, construction, and operation of the new WWPS itself.

I understand that the preferred alternative is Alternative #2, which is to locate the new WWPS on the east side of the study area.

I am generally satisfied with the report, and offer the following comments:

1. The new provincial planning and policy direction document is called the Provincial Planning Statement (2024). Please update references to the Provincial Policy Statement (2024).
2. Section 3.2 of the PFR contains a reference error, which should be fixed.
3. The PFR states that an Emissions Summary Dispersion Model will be completed for the site as part of the design stage to quantify the impacts. As there will be a standby generator on the site, NOX emissions will also be reviewed as part of the modelling. An Environmental Compliance Approval (ECA) from the ministry may be required for any air emissions associated with the project (in addition to ECA for sewage works). It is recommended you consult with Air Approvals in the ministry's Environmental Permissions Branch to discuss ECA requirements for air emissions.
4. The PFR states that Noise studies up to and including an Acoustic Assessment Report will be completed for the site to confirm the new equipment and site meets the Environmental Noise Guideline – Stationary and Transportation Sources (NPC-300). It is recommended that the noise studies be reviewed by a ministry noise engineer when the reports become available.

Thank you for the opportunity to review the draft PFR. Please ensure a copy of the Notice of Completion is sent to the ministry's Central Region EA notification email address (eanotification.cregion@ontario.ca) when it is issued.

If you have any questions about any of the above, feel free to contact me directly.

Sincerely,



Trevor Bell
Regional Environmental Planner – Central Region
Project Review Unit, Environmental Assessment Branch

Cc: Jonathan Rudyk, Project Manager, R.V Anderson Associates Ltd.
Freya Wu, Project Designer, R.V Anderson Associates Ltd.
Marco Mazzuca, Supervisor, Project Review Unit, EAB, MECP
Neil Hannington, Manager, Halton-Peel District Office, MECP

RVA 236814

June 26, 2025

Ministry of the Environment, Conservation and Parks
Environmental Assessment Branch
1st Floor
135 St. Clair Avenue W
Toronto, ON
M4V 1P5

Attention: Trevor Bell, Regional Environmental Planner – Central Region

Dear Mr. Bell,

Re: Halton Hills #4 Wastewater Pumping Station – Premier Gateway Employment Area
Schedule B Municipal Class Environmental Assessment – Draft Project File Report MECP Comments

Thank you for providing the Ministry of the Environment, Conservation and Parks' comments on the draft Project File Report for the above-mentioned project, in your letter dated June 13, 2025. The project team has reviewed the comments and has responded to them in the table attached. Please do not hesitate to contact the undersigned if there are any additional comments or questions.

Yours very truly,

R.V. ANDERSON ASSOCIATES LIMITED



Jonathan Rudyk, P.Eng.
Project Manager

Copied to: Christopher Pasquale, Project Manager II, Infrastructure and Environmental Services, Halton Region
Freya Wu, Project Coordinator, R.V. Anderson Associates Limited

Encls.

- Response Table

Halton Hills #4 WWPS Schedule B MCEA – MECP Response Table

Item No.	MECP Comment	Project Team Response
1	<p>General</p> <p>The new provincial planning and policy direction document is called the Provincial Planning Statement (2024). Please update references to the Provincial Policy Statement (2024).</p>	<p>This reference has been updated in the Project File Report.</p>
2	<p>General</p> <p>Please revise the reference error in Section 3.2.</p>	<p>This reference has been corrected in the Project File Report.</p>
3	<p>Air Emissions</p> <p>It is recommended to consult with Air Approvals in the ministry's Environmental Permissions Branch to discuss ECA requirements for air emissions.</p>	<p>Air Approvals in the ministry's Environmental Permissions Branch will be consulted during the subsequent detailed design phase to discuss ECA requirements for air emissions.</p>
4	<p>Noise Studies</p> <p>It is recommended that the noise studies be reviewed by a ministry noise engineer when reports become available.</p>	<p>A ministry noise engineer will be consulted during the subsequent detailed design phase to discuss noise studies and assessments.</p>

Jonathan Rudyk

From: Minkin, Dan (MHSTCI)
Sent: June 13, 2025 5:44 PM
To: Jonathan Rudyk
Cc: Pasquale, Christopher; Freya Wu; Passera Lavoie, Alexander
Subject: RE: 236814 - Halton Hills #4 WWPS Class EA PFR for MCM Review
Attachments: 2025-06-13_PremierGateway_MCMcomments.pdf

[CAUTION EXTERNAL EMAIL] Make Sure that it is legitimate **before** Replying or Clicking on any links

Hi Jonathan,
Please see our comments attached.

Dan Minkin

Heritage Planner | Heritage Operations Branch
Ministry of Citizenship and Multiculturalism | Ontario Public Service
416-786-7553 | dan.minkin@ontario.ca



Taking pride in strengthening Ontario, its places and its people

From: Jonathan Rudyk <JRudyk@rvanderson.com>
Sent: Monday, May 12, 2025 6:36 PM
To: Minkin, Dan (MCM) <Dan.Minkin@ontario.ca>
Cc: Pasquale, Christopher <christopher.pasquale@halton.ca>; Freya Wu <mwu@rvanderson.com>; Passera Lavoie, Alexander <alexander.passeralavoie@halton.ca>
Subject: RE: 236814 - Halton Hills #4 WWPS Class EA PFR for MCM Review

CAUTION -- EXTERNAL E-MAIL - Do not click links or open attachments unless you recognize the sender.

Hi Dan,

That's strange that you cannot access that, I apologize. I have re-sent the file through OneDrive, this should work: [20250512 - Draft PFR to MECP and MCM](#). Let me know, it may prompt you to put in your email, and then Microsoft should send you a verification code.

Regarding the MCM file number, I have attached the MCM's response to the Notice of Commencement, file number 0013436.

Thanks,

Jonathan Rudyk, P.Eng.

Associate, Project Engineer Water



R.V. Anderson Associates Limited
2001 Sheppard Avenue East, Suite 300
Toronto ON M2J 4Z8
t 416 497 8600 x1497
c 416 802 5048

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From: Minkin, Dan (MCM) <Dan.Minkin@ontario.ca>

Sent: May 12, 2025 5:06 PM

To: Jonathan Rudyk <JRudyk@rvanderson.com>

Cc: Pasquale, Christopher <christopher.pasquale@halton.ca>; Freya Wu <mwu@rvanderson.com>; Passera Lavoie, Alexander <alexander.passeralavoie@halton.ca>

Subject: RE: 236814 - Halton Hills #4 WWPS Class EA PFR for MCM Review

[CAUTION EXTERNAL EMAIL] Make Sure that it is legitimate **before** Replying or Clicking on any links

Hi Jonathan,

I received the file transfer email, but I get the below error screen when I click the download links. By the way, do you have an MCM file number from any of our previous correspondence on this study?



ERROR: The requested item was not found. It may have been deleted by a Newforma user. For more information.

From: Jonathan Rudyk <JRudyk@rvanderson.com>

Sent: Monday, May 12, 2025 3:55 PM

To: Minkin, Dan (MCM) <Dan.Minkin@ontario.ca>

Cc: Pasquale, Christopher <christopher.pasquale@halton.ca>; Freya Wu <mwu@rvanderson.com>; Passera Lavoie, Alexander <alexander.passeralavoie@halton.ca>

Subject: 236814 - Halton Hills #4 WWPS Class EA PFR for MCM Review

CAUTION -- EXTERNAL E-MAIL - Do not click links or open attachments unless you recognize the sender.

Hi Dan,

My name is Jonathan Rudyk, I am the project manager at RVA for an ongoing Schedule 'B' Municipal Class Environmental Assessment called the Halton Hills #4 Wastewater Pumping Station MCEA. The Notice of Commencement for this MCEA was issued last August, with a PIC occurring in October 2024. The project team has also been reached out to the MCM on the past in this project to discuss some prior Stage 1-2 AAs on the site, to jog your memory on the project further.

In keeping with the MCM's request at the Notice of Commencement stage, we have prepared our draft Project File Report and would like to circulate it to the MCM for your review. In a few moments you should receive a Newforma File Transfer with a PDF of the draft PFR. If you could please confirm receipt of the file transfer that would be appreciated, and we would be asking for the MCM to supply their comments for June 13th, 2025. Please confirm this timeline is feasible.

Thanks, and let me know if there were any issues with the file transfer.

Jonathan Rudyk, P.Eng.

Associate, Project Engineer Water



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**Ministry of Citizenship
and Multiculturalism**

Heritage Planning Unit
Heritage Operations Branch
Citizenship, Inclusion and
Heritage Division
5th Flr, 400 University Ave
Tel.: 416.786.7553

**Ministère des Affaires civiques
et du Multiculturalisme**

Planification relative au patrimoine
Opérations relatives au patrimoine
Division des affaires civiques, de
l'inclusion et du patrimoine
5e étage, 400, av. University
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June 13, 2025

EMAIL ONLY

Jonathan Rudyk, P.Eng.
Associate, Project Engineer Water
R.V. Anderson Associates Limited
2001 Sheppard Avenue East, Suite 300
Toronto ON M2J 4Z8
JRudyk@rvanderson.com

MCM File : 0013436
Proponent : Halton Region
Subject : Municipal Class EA – Schedule B – Draft Project File Report
Project : Halton Hills #4 Wastewater Pumping Station – Premier Gateway
Employment Area
Location : Town of Halton Hills, Ontario

Dear Jonathan Rudyk:

Thank you for providing the Ministry of Citizenship and Multiculturalism (MCM) with the draft Project File Report (PFR) for this project dated May 12, 2025, prepared by RVA.

MCM's interest in this project relates to its mandate of conserving Ontario's cultural heritage, which includes:

- archaeological resources, including land and marine;
- built heritage resources, including bridges and monuments; and
- cultural heritage landscapes.

We have reviewed the draft PFR and offer the following comments.

Project Summary

Halton Region is initiating a Municipal Class Environmental Assessment study to identify the preferred site for the Halton Hills #4 Wastewater Pumping Station to support growth in the Premier Gateway Employment Area in the Town of Halton Hills. The new wastewater pumping station will service the area generally located north of Steeles Avenue between Ninth Line and Winston Churchill Boulevard. This project was identified in the 2008 South Halton Water and Wastewater Master Plan Update and confirmed in the 2011 Sustainable Halton Water and Wastewater Master Plan.

Comments

We have no substantive concerns about with this draft PFR. On a terminological note, cultural heritage resources is a term that encompasses archaeological resources, built heritage resources, and cultural heritage landscapes. We recommend that Section 4.1 be revised to the following: *“The social and cultural environment in the existing study area may include cultural heritage resources, including archaeological resources, built heritage resources, and cultural heritage landscapes.”* Similarly, we recommend that Section 4.1.2 be renamed “Built Heritage Resources and Cultural Heritage Landscapes – Cultural Heritage Resources Assessment from PGEA Phase 2B Secondary Plan”.

Thank you for consulting MCM on this project and please continue to do so throughout the EA process. If you have any questions or require clarification, please do not hesitate to contact me.

Sincerely,

Dan Minkin
Heritage Planner
dan.minkin@ontario.ca

It is the sole responsibility of proponents to ensure that any information and documentation submitted as part of their EA report or file is accurate. The Ministry of Citizenship and Multiculturalism (MCM) makes no representation or warranty as to the completeness, accuracy or quality of the any checklists, reports or supporting documentation submitted as part of the EA process, and in no way shall MCM be liable for any harm, damages, costs, expenses, losses, claims or actions that may result if any checklists, reports or supporting documents are discovered to be inaccurate, incomplete, misleading or fraudulent.

Should previously undocumented archaeological resources be discovered, they may be a new archaeological site and therefore subject to Section 48(1) of the *Ontario Heritage Act*. The proponent or person discovering the archaeological resources must cease alteration of the site immediately and engage a licensed consultant archaeologist to carry out an archaeological assessment, in compliance with Section 48(1) of the *Ontario Heritage Act*.

The *Funeral, Burial and Cremation Services Act, 2002, S.O. 2002, c.33* requires that any person discovering human remains must cease all activities immediately and notify the police or coroner. If the coroner does not suspect foul play in the disposition of the remains, in accordance with *Ontario Regulation 30/11* the coroner shall notify the Registrar, Ontario Ministry of Public and Business Service Delivery, which administers provisions of that Act related to burial sites. In situations where human remains are associated with archaeological resources, the Ministry of Citizenship and Multiculturalism should also be notified (at archaeology@ontario.ca) to ensure that the archaeological site is not subject to unlicensed alterations which would be a contravention of the *Ontario Heritage Act*.

RVA 236814

June 26, 2025

Ministry of Citizenship and Multiculturalism
Heritage Planning Unit
Heritage Operations Branch
Citizenship, Inclusion and Heritage Division
5th Flr, 400 University Ave
Toronto, ON
M5G 1S5

Attention: Dan Minkin, Heritage Planner

Dear Mr. Minkin,

Re: Halton Hills #4 Wastewater Pumping Station – Premier Gateway Employment Area
Schedule B Municipal Class Environmental Assessment – Draft Project File Report MCM Comments

Thank you for providing the Ministry of Citizenship and Multiculturalism's comments on the draft Project File Report for the above-mentioned project, in your letter dated June 13, 2025. The project team has reviewed the comments and has responded to them in the table attached. Please do not hesitate to contact the undersigned if there are any additional comments or questions.

Yours very truly,

R.V. ANDERSON ASSOCIATES LIMITED



Jonathan Rudyk, P.Eng.
Project Manager

Copied to: Christopher Pasquale, Project Manager II, Infrastructure and Environmental Services, Halton Region
Freya Wu, Project Coordinator, R.V. Anderson Associates Limited

Encls.

- Response Table

Halton Hills #4 WWPS Schedule B MCEA – MCM Response Table

Item No.	MCM Comment	Project Team Response
1	<p>General</p> <p>Please revise Section 4.1 to the following: “The social and cultural environment in the existing study area may include cultural heritage resources, including archaeological resources, built heritage resources, and cultural heritage landscapes.”</p>	<p>This revision has been reflected in the Project File Report.</p>
2	<p>General</p> <p>Please rename Section 4.1.2 to “Built Heritage Resources and Cultural Heritage Landscapes – Cultural Heritage Resources Assessment from PGEA Phase 2B Secondary Plan.”</p>	<p>This revision has been reflected in the Project File Report.</p>